



FY2025 Annual Inspection and Underwater Inspection

FY2025 Facility Annual Inspection Report

1 | 1 December 30, 2024

Chesapeake Bay Bridge Tunnel (CBBT) District

Task Orders 5 & 6

Distribution of copies

Revision	Issue approve	Date issued	Issued to	Comments
Draft		12/8/2024	CBBT	Draft report for review and comment
Final		12/28/2024	CBBT	





FY2025 Annual Inspection and Underwater Inspection

Project No: 222309/05 & 222309/06

Document Title: FY2025 Facility Annual Inspection Report

Document No.: 1 Revision: 1

Document Status: Final

Date: December 30, 2024

Client Name: Chesapeake Bay Bridge and Tunnel District

Client No: Task Orders 5 & 6
Project Manager: Jousha Hill, PE

Author: Jousha Hill, PE & Shannon Turner, PE

File Name: CBBT_FY2025_Annual Inspection and Underwater Inspection

Moffatt & Nichol

3000 World Trade Center 101 West Main Street, Suite 3000 Norfolk, VA 23510

Limitation: This document has been prepared on behalf of, and for the exclusive use of Moffatt & Nichol's client, and is subject to, and issued in accordance with, the provisions of the contract between Moffatt & Nichol and the client. Moffatt & Nichol accepts no liability or responsibility whatsoever for, or in respect of, any use of, or reliance upon, this document by any third party.



Table of Contents

1.	Executive Summary	1-1
2.	Inspection Schedule	2-1
3.	Ratings for GASB 34	3-1
4.	Repair Recommendations	4-1
4.1	Trestles, North Channel Bridges, and Fisherman Inlet Bridges	4.1.1-1
4.2	Tunnels	4.1.12-1
4.3	General Facility	4.3-1
5.	VDOT Structure Inventory and Appraisal (SI&A) Records (B-6 and B-7 Forms)	5-1
5.1	Trestle ANB SI&A Forms	5.1-1
5.2	Trestle ASB SI&A Forms	5.2-1
5.3	Trestle BNB SI&A Forms	5.3-1
5.4	Trestle BSB SI&A Forms	5.4-1
5.5	Trestle CNB SI&A Forms	5.5-1
5.6	Trestle CSB SI&A Forms	5.6-1
5.7	NCB-NB SI&A Forms	5.7-1
5.8	NCB-SB SI&A Forms	5.8-1
5.9	Trestle DNB SI&A Forms	5.9-1
5.10	Trestle DSB SI&A Forms	5.10-1
5.11	Trestle ENB, FIB-NB, & FNB SI&A Forms	5.11-1
5.12	Trestle ESB, FIB-SB, & FSB SI&A Forms	5.12-1
6.	Bridge Element Level Data	6-1
6.1	Trestle ANB Element Level Data	6.1-1
6.2	Trestle ASB Element Level Data	6.2-1
6.3	Trestle BNB Element Level Data	6.3-1
6.4	Trestle BSB Element Level Data	6.4-1
6.5	Trestle CNB Element Level Data	6.5-1
6.6	Trestle CSB Element Level Data	6.6-1
6.7	NCB-NB Element Level Data	6.7-1
6.8	NCB-SB Element Level Data	6.8-1
6.9	Trestle DNB Element Level Data	6.9-1
6.10	Trestle DSB Element Level Data	6.10-1
6.11	Trestle ENB, FIB-NB, & FNB Element Level Data	6.11-1
6.12	Trestle ESB, FIB-SB, & FSB Element Level Data	6.12-1
7.	Tunnel Inspection	7-1
7.1	Tunnel Baseline Data	7-1
7.2	NTIS Inspection Team	7-2





1.3	Summary of Findings	
8.	Tunnel Inventory Items	7.3-1
8.1	TSCT Inventory Items	8.1-1
8.2	CCT Inventory Items	8.2-1
9.	Tunnel Element Level Data	9-1
9.1	Thimble Shoal Channel Tunnel - Element Level Data	9.1-1
9.2	Chesapeake Channel Tunnel - Element Level Data	9.2-1
10.	Photographs	9.2-1
10.1	Trestle ANB Photographs	10.1-1
10.2	Trestle ASB Photographs	10.2-1
10.3	Trestle BNB Photographs	10.3-1
10.4	Trestle BSB Photographs	10.4-1
10.5	Trestle CNB Photographs	10.5-3
10.6	Trestle CSB Photographs	10.6-1
10.7	NCB-NB Photographs	10.7-1
10.8	NCB-SB Photographs	10.8-1
10.9	Trestle DNB Photographs	10.9-1
10.10	Trestle DSB Photographs	10.10-1
10.11	Trestle ENB, FIB-NB, & FNB Photographs	10.11-1
10.12	Trestle ESB, FIB-SB, & FSB Photographs	10.12-1
10.13	TSCT Photographs	10.13-1
10.14	CCT Photographs	10.14-1
10.15	General Facility	10-0
11.	Bay Bottom Profiles	11-0
12.	Tunnel Inspection Back Up Data	12-1
13.	Tunnel Major Preventative Maintenance Records	13-2
14.	Special Testing Documents	14-3
15.	Electronic Submittals	15-1





List of Tables

Table 1: GASB 34 Cluster Item Ratings		
Table 2: CBBT Facility Component Inspection Schedule		
Table 3: CBBT Trestles & North Channel Bridge Projected Hands-on Inspection Schedule		
Table 4: GASB 34 Components with Condition Rating Below 7		3-1
Table 5: GASB 34 Condition Ratings		
Table 6: ANB Critical Findings	4.1	1.1-1
Table 7: ANB Priority Repairs	4.	1.1-
Table 8: ANB Routine Repairs	4.	1.1-
Table 9: ASB Critical Findings	4.	1.2-
Table 10: ASB Priority Repairs	4.	1.2-
Table 11: ASB Routine Repairs	4.	1.2-
Table 12: BNB Critical Findings		
Table 13: BNB Priority Repairs	4.	1.3-1
Table 14: BNB Routine Repairs	4.	1.3-1
Table 15: BSB Critical Findings	4.	1.4-
Table 16: BSB Priority Repairs	4.	1.4-
Table 17: BSB Routine Repairs	4.	1.4-
Table 18: CNB Critical Findings	4.	1.5-1
Table 19: CNB Priority Repairs	4.	1.5-1
Table 20: CNB Routine Repairs	4.	1.5-1
Table 21: CSB Critical Findings	4.	1.6-
Table 22: CSB Priority Repairs	4.	1.6-
Table 23: CSB Routine Repairs	4.	1.6-
Table 24: NCB-NB Critical Findings	4.	1.7-
Table 25: NCB-NB Priority Repairs	4.	1.7-
Table 26: NCB-NB Routine Repairs	4.	1.7-
Table 27: NCB-SB Critical Findings	4.	1.8-
Table 28: NCB-SB Priority Repairs		
Table 29: NCB-SB Routine Repairs		
Table 30: Trestle DNB Critical Findings	4.1	1.9-
Table 31: Trestle DNB Priority Repairs	4.	1.9-
Table 32: Trestle DNB Routine Repairs	4.	1.9-
Table 33: Trestle DSB Critical Findings	4.1.	10-1
Table 34: Trestle DSB Priority Repairs	4.1.	10-1
Table 35: Trestle DSB Routine Repairs		
Table 36: ENB, FIB-NB, and FNB Critical Findings	4.1.	11-1
Table 37: ENB, FIB-NB, and FNB Priority Repairs	4.1.	11-1
Table 38: ENB, FIB-NB, and FNB Routine Repairs	4.1.	11-1
Table 39: ESB, FIB-SB, and FSB Critical Findings	4.1.	12-1
Table 40: ESB, FIB-SB, and FSB Priority Repairs	4.1.	12-1
Table 41: ESB, FIB-SB, and FSB Routine Repairs	4.1.	12-1
Table 42: TSCT Critical Findings	4.2	2.1-
Table 43: TSCT Priority Repairs	4.2	2.1-1
Table 44: TSCT Routine Repairs		
Table 45: CCT Critical Findings		
Table 46: CCT Priority Repairs	4.2	2.2-
Table 47: CCT Routine Repairs		
Table 48: General Facility Critical Findings		
Table 49: General Facility Priority Repairs	2	4.3-





Table 50: General Facility Routine Repairs	4.3-1
Table 51: ANB Element Level Data	6.1-1
Table 52: ASB Element Level Data	6.2-1
Table 53: BNB Element Level Data	6.3-1
Table 54: BSB Element Level Data	6.4-1
Table 55: CNB Element Level Data	6.5-1
Table 56: CSB Element Level Data	6.6-1
Table 57: NCB-NB Element Level Data	6.7-1
Table 58: NCB-SB Element Level Data	6.8-1
Table 59: DNB Element Level Data	6.9-1
Table 60: DSB Element Level Data	
Table 61: ENB, FIB-NB, & FNB Element Level Data	6.11-1
Table 62: ESB, FIB-SB, & FSB Element Level Data	6.12-1
Table 63: TSCT Identification Items	8.1-1
Table 64: TSCT Age and Service Items	8.1-1
Table 65: TSCT Classification Items	8.1-2
Table 66: TSCT Geometric Data Items	8.1-2
Table 67: TSCT Inspection Items	8.1-2
Table 68: TSCT Load Rating and Posting Items	8.1-3
Table 69: TSCT Navigation Items	8.1-3
Table 70: TSCT Structure Type and Materials Items	8.1-3
Table 71: CCT Identification Items	8.2-1
Table 72: CCT Age and Service Items	8.2-1
Table 73: CCT Classification Items	8.2-2
Table 74: CCT Geometric Data Items	8.2-2
Table 75: CCT Inspection Items	8.2-2
Table 76: CCT Load Rating and Posting Items	
Table 77: CCT Navigation Items	8.2-3
Table 78: CCT Structure Type and Material Items	8.2-3
Table 79: TSCT Element Level Data	9.1-1
Table 80: CCT Flement Level Data	9 2-1





1. Executive Summary

Enclosed is a combination of the GASB 34 with the Maintenance Rating Program (MRP) Condition Scale and ratings for the trestles along with repair recommendations, and recommended insurance coverages for the facilities that make up the Chesapeake Bay Bridge Tunnel District (the District) for Fiscal Year 2025 (FY2025, July 1, 2024 – June 30, 2025). The ratings of the GASB 34 Cluster Item Ratings are summarized below in Table 1, and can be observed in detail in Section 2: Ratings for GASB 34 of the report.

It should be noted that items identified as "bold" and in "blue" font have changed from the previous report.

Table 1: GASB 34 Cluster Item Ratings

Cluster Item	Rating
A. Approach Roads	7
B. Fisherman Island Causeway	7
C. Tunnels	7
D. Portal Islands	7
E. Toll Plaza Infrastructure	8
F. Site-Wide Utilities	8

No critical findings were reported during the inspection. A few specific facility components were rated less than Good Condition (numeric rating of 7 or above) at the time of the inspection, with some of these items being typical components that are regularly monitored and repaired or replaced as needed. The components rated less than 7 make up a minority percentage of the overall cluster items, such that the overall ratings were all 7 or greater as shown in Table 1.

The trestle components were found to be in Good Condition or better (numeric rating of 7 or above). A detailed breakdown of the components can be observed in Section 4: Virginia Department of Transportation (VDOT Structure Inventory and Appraisal (SI&A) Records (B-6 and B-7 Forms) of the report. Based on the historical structural analysis and review of the 2023 Hydrographic Survey for Acceptable Scour Depth (ASD), approximately 35% of piles along Trestle CNB are coded as Condition State 3 (Poor) due to bat bottom elevations and scour remediation in place. There are approximately 64 piles total coded as Condition State 4 (Severe) due to bay bottom elevations found to be below the Allowable Scour Depth along Trestle ANB, BNB and CSB. Otherwise, most of the piles would be coded as based on their Condition State 2 (Fair) based on the physical condition of the pile.

A list of supplemental information used to develop the ratings provided in the report has been submitted to the District electronically. A list of these supplemental items is displayed in Section 15: Electronic Submittals.

Repair recommendations are separated into two categories: Priority Repairs and Routine Repairs and have been provided in Section 3: Repair Recommendations. It is understood by Moffatt & Nichol that the District is in the process of performing or developing contract documents for the repair or engineering analysis of several of these components such as:

• Repairs to spalled concrete areas on concrete columns, walls, roof beams, and ceiling of the supply fan room of all ventilation buildings.





- Little Creek Pier Fender System will be replaced as part of the lease agreement that the District has with Chesapeake Tunnel Joint Venture (CTJV) prior to the expiration of necessary permits on July 31, 2025.
- Demonstration / Research of facility wide repairs to concrete pile substructure units to extend service life.
- Facility wide steel bridge paint project for all existing structural steel as required to extend the services life. Project is to begin in 2025.

Inspection Personnel:

The inspection team consisted of the following personnel:

Bridge (Visual & Hands-on):

```
Shannon Turner, PE - NHI Team Leader (Visual & Hands-on) - Moffatt & Nichol

Jonathan Chapman, PE - NHI Team Leader (Hands-on) - Moffatt & Nichol

Ethan Stivers - NHI Inspector - Moffatt & Nichol

Jon Krites - NHI Inspector - Moffatt & Nichol
```

Tunnel:

```
William (Billy) Bolt, PE - NTIS Structural Team Leader – Modjeski and Masters, Inc.

Alexander (Lex) Waardenburg, PE - NTIS Mechanical Team Leader – Modjeski and Masters, Inc.
```

Kyle Gable, PE - NTIS Electrical Team Leader - Modjeski and Masters, Inc.

Bridge (Underwater):

```
Callan – NHI UW Inspector – Crofton Diving

Daniels – NHI UW Inspector – Crofton Diving

Crutchfield – NHI UW Inspector – Crofton Diving

Minozzi – NHI UW Inspector – Crofton Diving

Bolyard – NHI UW Inspector – Crofton Diving
```

Support Facilities:

```
Jousha Hill, PE - Structural & Civil - Moffatt & Nichol

Zach Adams, PE - Electrical - Moffatt & Nichol
```

Drone Survey:

Stephen Sirotko, EIT - NHI Inspector - Moffatt & Nichol





2. Inspection Schedule

Portions of the CBBT facility covered during the FY2025, FY2024, FY2023, FY2022, FY2021 were inspected and are noted in Table 2. An underwater inspection was performed on Trestle BNB, DSB, ENB, FNB, FIB-NB, NCB-SB and Finger Pier at Little Creek during the FY2025 inspection.

Table 2: CBBT Facility Component Inspection Schedule

Facili	ty Component	Portion Inspected
1	Trestle A - Northbound (ANB)	Completed in FY2025: Hands-on: Spans ANB177 to Span ANB226
		Completed in FY2024: Hands-on: Spans ANB132 to Span ANB178
		Underwater Inspection of Trestle ANB (including ANB')
		Visual: Spans A'NB1 to Abutment ANB226
		Completed in FY2023:
		Hands-on: Spans ANB83 to Span ANB132
		Completed in FY2022:
		Hands-on: Spans ANB33 to Span ANB81
		Visual: Spans A'NB1 to Span ANB33 and ANB82 to Abutment ANB226
		Completed in FY2021:
		Hands-on: Abutment A'NB1 to Span ANB32
		Completed in FY2020:
		Hands-on: Spans ANB177 to Abutment ANB226
		Visual: Spans A'NB1 to ANB176
		Completed in FY2019:
		Hands-on: Spans ANB129 to ANB176
		Underwater Inspection of Trestle ANB (including ANB')
2	Trestle A - Southbound (ASB)	Completed in FY2025: Hands-on: Spans ASB39 to Span ASB80
		Visual: Spans All – ASB spans
		Completed in FY2024: Hands-on: Spans ASB81 to ASB 120
		Underwater Inspection of Trestle ASB
		Completed in FY2023:
		Hands-on: Spans ASB121 to ASB164
		Completed in FY2022:
		Hands-on: Spans ASB167 to ASB186 and ASB194 to
		Abutment ASB205
		Completed in FY2021:
		Hands-on: Spans ASB1 to ASB38
		Visual: Spans ASB39 to Abutment ASB205



		Completed in FY2020:
		Hands-on: Spans ASB39 to ASB80
		Visual: Spans ASB1 to ASB18 from Chic's Beach and ASB204
		and Abutment ASB205
		Completed in FY2019:
		Hands-on: Spans ASB81 to ASB122
		Visual: Spans ASB18 to ASB204
		Underwater Inspection of Trestle ASB
3	Trestle B - Northbound (BNB)	Completed in FY2025: Hands-on: Spans BNB159 to BNB212
		Underwater Inspection of Trestle BNB
		Completed in FY2024: Hands-on: Spans BNB105 to BNB158
		Visual: Spans BNB1 to BNB264
		Completed in FY2023:
		Hands-on: Spans BNB55 to Abutment BNB104
		Completed in FY2022:
		Hands-on: Spans Abutment BNB1 to BNB44
		Visual: Spans BNB45 to BNB264
		Completed in FY2021:
		Hands-on: Spans BNB213 to Abutment BNB266
		Visual: Spans BNB1 to Abutment BNB212
		Completed in FY2020:
		Hands-on: Spans BNB159 to BNB212
		Visual: Spans BNB1 to 158 and 213 to 265
		Underwater Inspection of Trestle BNB
		Completed in FY2019:
		Hands-on: Spans BNB105 to BNB158
4	Trestle B - Southbound (BSB)	Completed in FY2025: Hands-on: Spans BSB57 to BSB100
		Visual: Spans All – BSB spans
		Completed in FY2024: Hands-on: Spans BSB101 to BSB142
		Completed in FY2023:
		Hands-on: Spans BSB143 to BSB178
		Completed in FY2022:
		Hands-on: Spans BSB1 to BSB16, BSB179-BSB202
		Underwater Inspection of Trestle BSB
		Completed in FY2021:
		Hands-on: Spans BSB17 to BSB56
		Visual: Spans Abutment BSB1 to Span BSB16 and BSB57 to
		Abutment BSB203
		Completed in FY2020:
		Hands-on: Spans BSB57 to BSB100
		Visual: Spans BSB1 and BSB202
		Completed in FY2019:
		Hands-on: Spans BSB101 to BSB142



		Visual: Spans BSB1 to BSB202
5	Trestle C - Northbound (CNB)	Completed in FY2025: Hands-on: Spans CNB197 to CNB263
		Completed in FY2024: Hands-on: Spans CNB132 to Span
		CNB196 Visual: Spans CNB1 to CNB322
		Completed in FY2023:
		Hands-on: Spans CNB67 to Span CNB131
		Completed in FY2022:
		Hands-on: Spans CNB1 to Span CNB45
		Visual: Spans CNB46 to CNB322
		Completed in FY2021:
		Hands-on: Spans CNB264 to Span CNB322 and Pier NCB-NB1
		Visual: Spans CNB1 to 263 and 264 to 322
		Underwater Inspection of Trestle CNB
		Completed in FY2020:
		Hands-on: Spans CNB198 to CNB263
		Visual: Spans CNB1 to 197 and 264 to 322
		Completed in FY2019:
		Hands-on: Spans CNB132 to CNB197
6	Trestle C - Southbound (CSB)	Completed in FY2025: Hands-on: Spans CSB53 to CSB97
	,	Visual: Spans All – CSB spans
		Completed in FY2024: Hands-on: Spans CSB97 to CSB154
		Completed in FY2023:
		Hands-on: Abutment CSB149 to Spans CSB200
		Completed in FY2022:
		Hands-on: Abutment CSB200 to Spans CSB246
		Completed in FY2021:
		Hands-on: Abutment CSB1 to Spans CSB52
		Visual: Spans CSB53 to CSB246
		Completed in FY2020:
		Hands-on: Spans CSB53 to CSB104
		Visual: CSB1
		Completed in FY2019:
		Hands-on: Spans CSB105 to CSB154
		Visual: Spans CSB1 to CSB246
7	North Channel Bridge - Northbound	Fracture Critical: Spans 1 thru 18 completed in FY2024
	(NCB-NB)	Above Water: Piers 1 thru 18 all completed in FY2024
		Underwater Inspection of NCB-NB completed in FY2022
8	North Channel Bridge - Southbound	Hands-on: All spans. Completed in FY2025
	(NCB-SB)	Underwater Inspection of NCB-SB completed in FY2025
9	Trestle D - Northbound (DNB)	Hands-on: All spans. Completed in FY2025
		Underwater Inspection of DNB completed in FY2022





10	Trestle D - Southbound (DSB)	Hands-on: Entire component completed in FY2025
		Underwater Inspection of DSB completed in FY2025
11	Trestle E - Northbound,	Hands-on: All spans. Completed in FY2025
	Fisherman Inlet Bridge - Northbound, Trestle F - Northbound (ENB, FIB-NB, FNB)	Underwater Inspection of ENB, FIB-NB, FNB completed in FY2025
12	Trestle E - Southbound, Fisherman Inlet Bridge - Southbound,	Hands-on: All spans. Completed in FY2025
	Trestle F - Southbound (ESB, FIB-SB, FSB)	Underwater Inspection of ESB, FIB-SB, FSB completed in FY2022
13	Thimble Shoals Channel Tunnel (TSCT)	Routine inspection of the entire Tunnel, including Open Approaches on Islands 1 and 2 (completed in FY2024)
14	Chesapeake Channel Tunnel (CCT)	Routine inspection of the entire Tunnel, including Open Approaches on Islands 3 and 4 (completed in FY2025)
15	Approach Roads	Main components and spot check minor components (support GASB 34)
16	Fisherman Island Causeway	Main components and spot check minor components (support GASB 34)
17	Portal Islands No. 1, 2, 3, and 4	Main components, spot check minor components (support GASB 34). Portal Islands No. 3 & 4 (completed in FY2025) and Portal Islands No. 1 & 2 (completed in FY2024).
18	South Toll Plaza	Main components and spot check minor components
		Overhead Sign Structures (S-1, S-2, and S-3) Inspection completed in FY2023
19	North Toll Plaza	Main components and spot check minor components
		Overhead Sign Structures (N-2 & N-3) and Butterfly Sign Structure (N-1) Inspections completed in FY2023
20	Toll Plaza Infrastructure	Main components and spot check minor components
21	Site-Wide Utilities	Main components and spot check minor components

The projected hands-on inspection schedule for all trestles is noted in Table 3. Southbound portions of the facility are approximated as the crew aims to hit the quota (approximately 1/5 of trestle) while looking to stop at or beyond the quota at light poles that will require the bridge inspection platform to be stowed and redeployed. Visual inspections will be performed on Trestles ANB, BNB, and CNB during even FYs (FY2024, FY2026, etc.) and Trestles ASB, BSB, and CSB during odd FYs (FY2025, FY2027, etc.). The rest of the structures on the facility receive hands-on inspections at intervals not to exceed 24-months.





Table 3: CBBT Trestles & North Channel Bridge Projected Hands-on Inspection Schedule

Facility Component		Project Spans to be Inspected					
		FY2025	FY2026	FY2027	FY2028	FY2028	
1	Trestle ANB (48 spans/year)	177 - Abutment 236	1' - 32	33 - 81	83-132	133-176	
2	Trestle ASB (41 spans/year)	39 - 80	1 - 38	165 - Abutment 205	121-164	81-120	
3	Trestle BNB (54 spans/year)	159 - 212	213 - Abutment 266	Abutment 1 - 55	56-104	105-158	
4	Trestle BSB (41 spans/year)	57 - 100	17 - 56	Abutment 1- 16 & 179-202	143-178	101-142	
5	Trestle CNB (65 spans/year)	198 - 263	264 - 322	Abutment 1 - 45	67-131	132-197	
6	Trestle CSB (41 spans/year)	53 - 104	1 - 52	200-246	149-199	97-154	
7	North Channel Bridge – NB	-	All	-	All	-	
8	North Channel Bridge - SB	All	-	All	-	All	
9	Trestle DNB	All	-	All	-	All	
10	Trestle DSB	All	-	All	-	All	
11	ENB, FIB-NB, FNB	All	-	All	-	All	
12	ESB, FIB-SB, FSB	All	-	All	-	All	
13	TSCT	-	All	-	All	-	
14	ССТ	All	-	All	_	All	



1 | 1



3. Ratings for GASB 34

The following tables represent condition ratings to support the requirements of Statement No. 34 of the Governmental Accounting Standards Board: Basic Financial Statements - and Management's Discussion and Analysis - for State and Local Governments (GASB 34). The overall Cluster Item Ratings were found to be in Good Condition or better (numeric rating of 7 or above). Table 4 identifies specific components with a condition rating below 7.

Table 4: GASB 34 Components with Condition Rating Below 7

Item	Component	Rating	Explanation of Rating	
1	Tunnel Ventilation Buildings - Structural Framing	6	Spalls on columns and roof beams. Some repaired. However, new and existing patches are deteriorating. Programmed for repair.	
2	2 Tunnel Ventilation Building - Elevator		Elevator will not level out in Ventilation Building Nos. 1 or 2 when the doors open. Programmed for replacement.	
3	Portal Island No. 1 - Splash Walls	6	Based on tipping panels at southeast corner, appears to have stabilized in recent years although continue to monitor. To be replaced as part of the Parallel Thimble Shoal Tunnel (PTST) Project.	
4	Portal Islands – Approach Walls	6	Several locations of spalled/delaminated concrete, some with exposed reinforcement.	
5	Portal Island – Other Islands – Splash Walls	6	Based on sinkholes and rotation of splash wall panels on Portal Island No. 4.	

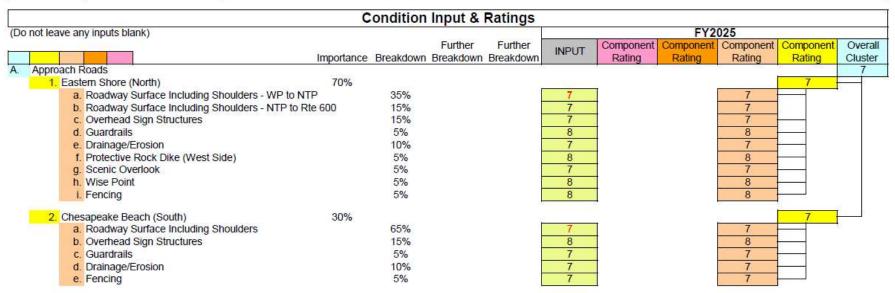
Section 3: Rating for GASB 34 3-1



Table 5: GASB 34 Condition Ratings

Overall Ratings Summary	FY2025
A. Approach Roads	7
B. Fisherman Island Causeway	7
C. Tunnels	7
D. Portal Islands	7
E. Toll Plaza Infrastructure	8
F. Site-Wide Utilities	8

Numeric Code	Narrative Code	Definition
9	Excellent	Component/Element has been recently put in service or remains in new condition
8	Very Good	No problems noted, potential exists for minor preventative maintenance
7	Good	Potential exists for minor maintenance
6	Satisfactory	Potential exists for major maintenance
5	Fair	Potential exists for minor repair or rehabilitation
4	Poor	Potential exists for major repair or rehabilitation
3	Serious	Major repair or rehabilitation is required
2	Critical	The need for repair or rehabilitation is urgent, Component/Element should be taken out of service until indicated repair is complete
1	Imminent Failure	Component/Element is out of service: study feasibility for repair or rehabilitation
0	Failed	Component/Element is out of service and beyond repair, replacement required



1 | 1 | Section 3: Rating for GASB 34 3-2





Table 5: GASB 34 Condition Ratings (continued)

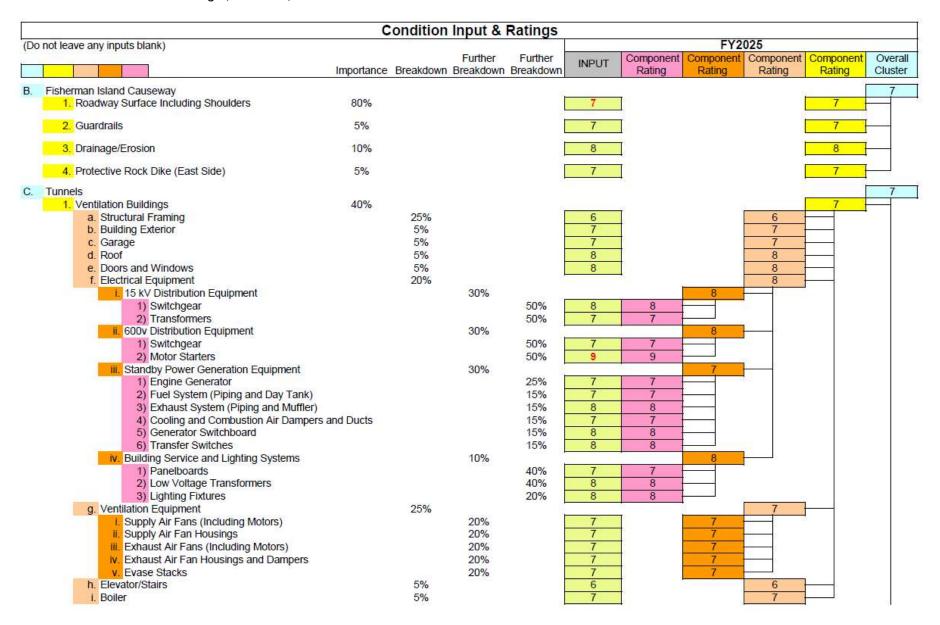






Table 5: GASB 34 Condition Ratings (continued)

not leave any inputs blank)	C	onunun	Input &	Kaunys			EVO	025		
lot leave any inputs blank)			Further	Further	AND DESCRIPTION	Component			Component	Overa
	Importance	Breakdown		Breakdown	INPUT	Component Rating	Rating	Component Rating	Component Rating	Cluste
unnels (continued)										
Tunnel Structure	60%								7	34.5
a. Tunnel Roadway Slab		20%			7			7		
b. Tunnel Walls		15%			7			7	10	
c. Tunnel Ceiling		10%			7			7	8	
d. Lighting		10%				→ .8		8	8	
i. Fixtures			20%	1	8		8		× .	
ii. Panelboards			20%		8	r.	8			
iii. Transformers			20%		8	2	8			
 Uninterruptible Power Supply (UPS) 			20%		8	G I	8	-		
v. Controls Panel			20%		7	le l	7	2 3	99	
e. Supply Air Duct		10%			7		ė-	7	2	
f. Exhaust Air Duct		10%			7	4		7		
 g. Emergency Crew Walkway (Sidewalk) 		5%			7			7	8	
h. Portal Drains and Gutters		5%			7	L ^S		7	6	
 Mid-Channel Pump Room Equipment 		5%		8.				8	8	
i. Lighting Fixtures			20%		8		8			
ii. Motor Control Equipment			40%		8	7	8			
iii. Pumps and Valves			20%		8	r.	8			
iv. Piping			20%		8	2	8	(i)	8	
j. Fire Emergency Equipment		5%			7			7	8	
k. Portal Pump Rooms		5%			7	1		8	8	
i. Lighting Fixtures			20%		8	[2]	8			
ii. Motor Control Equipment			40%		8	2	8			
ili. Pumps and Valves			20%		7	i.	7			
iv. Piping			20%		8	. S	8	(5)		

1 | 1 Section 3: Rating for GASB 34 3-4





Table 5: GASB 34 Condition Ratings (continued)

	C	ondition	Input &	Ratings						
not leave any inputs blank)						FY2025				0.5
	Importance	Breakdown	Further Breakdown	Further Breakdown	INPUT	Component Rating	Component Rating	Component Rating	Component Rating	Overal Cluster
Portal Islands										7
1. Island No. 1	25%					_			7	
a. Splash Walls		20%			6			6	9 9	85
 b. Open Approach Walls 		25%			6			6	 	
 c. Open Approach Roadway 		10%			8			8	2 - 4	
 d. Portal Island Surface 		10%			7			7	* *	
e. Guardrails		5%			7			7		
f. Armor Stone		20%			8			8 -	* *	
g. Perimeter Fencing	To be Replaced Post PTST	0%			6	2		6		
h. Fishing Pier	Fishing Pier Not Inspected.	5%		2	6	20 0		7	3 9	
i. Deck/Railing	Ratings carried forward from		20%		7		7	1		
ii. Superstructure	last inspection until completion		40%	1	7		7			
iii, Substructure	of Rehabilitation.		40%	1	7		7	<u> </u>		
2. Other Islands	75%								7	
a. Splash Walls		25%			6	T		6	4 9	
b. Open Approach Walls		25%			7			7		
c. Open Approach Roadway		15%		-	8			8		
d. Portal Island Surface		5%		1	6			6	3 H	
e. Guardrails		5%			6			6		
f. Armor Stone		25%		1	8			8		

1 | 1 Section 3: Rating for GASB 34 3-5





Table 5: GASB 34 Condition Ratings (continued)

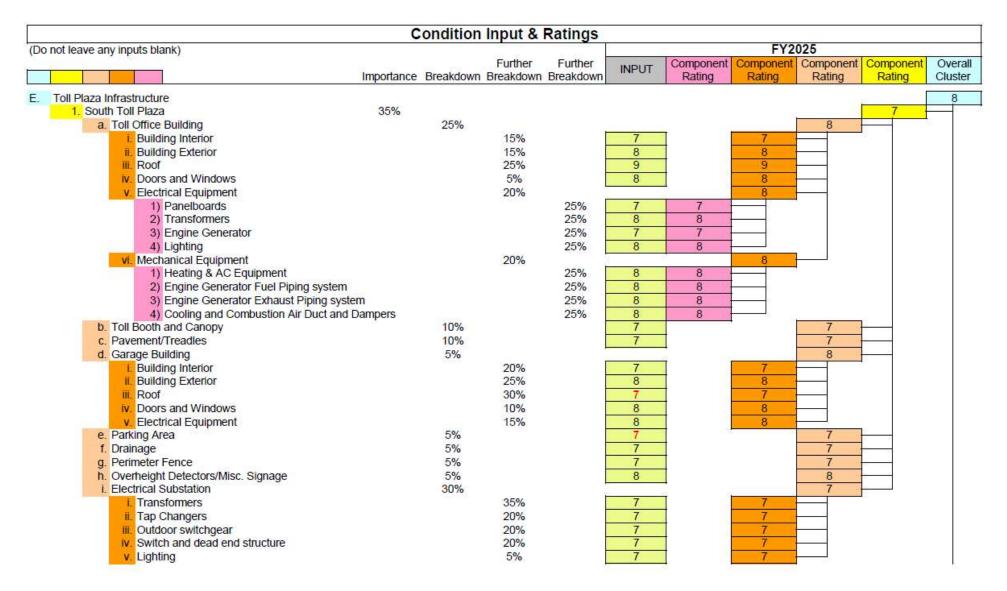


Table 5: GASB 34 Condition Ratings (continued)

1 | 1 | Section 3: Rating for GASB 34 3-6





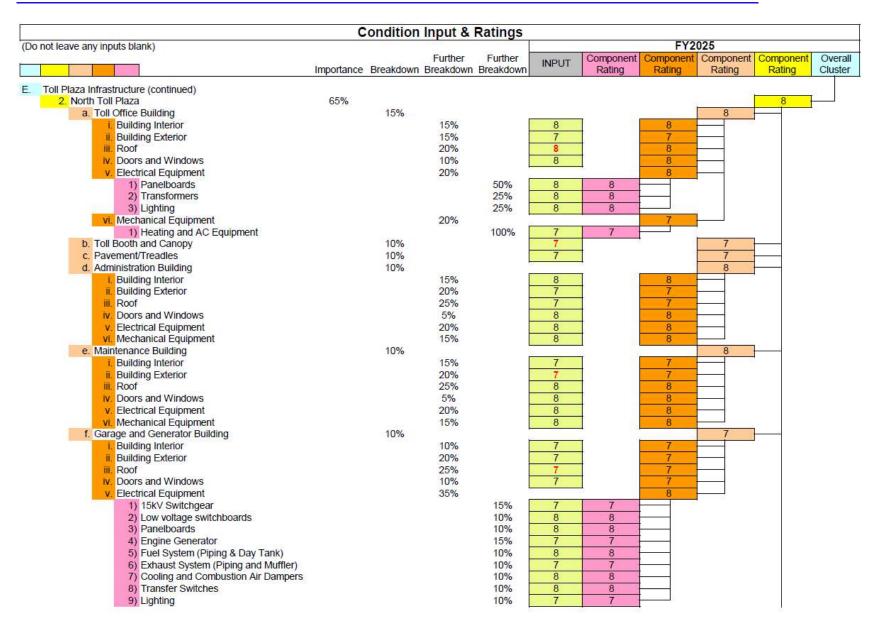
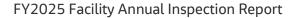


Table 5: GASB 34 Condition Ratings (continued)

1 | 1 Section 3: Rating for GASB 34 3-7







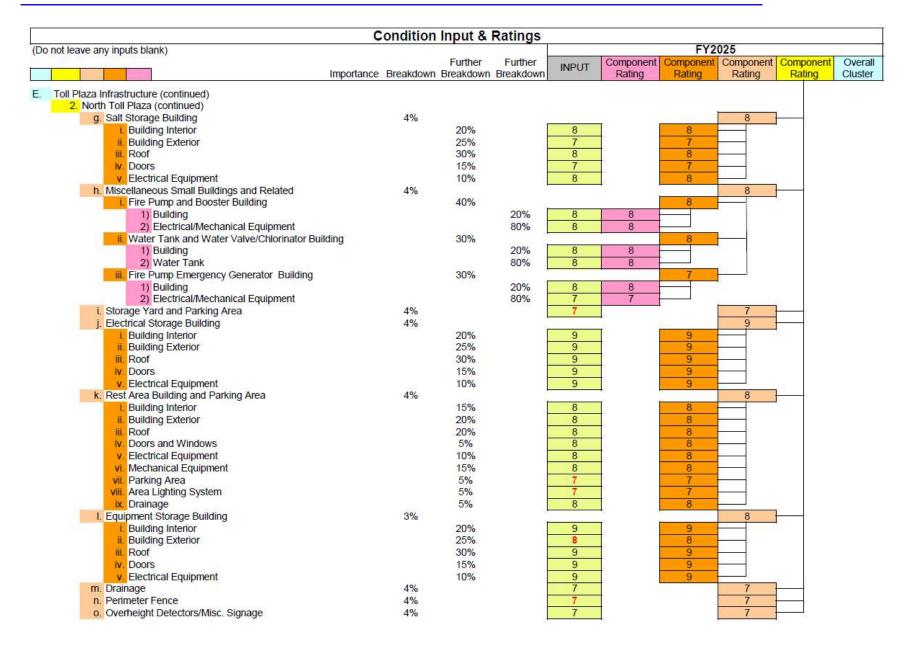






Table 5: GASB 34 Condition Ratings (continued)

Condition Input & Ratings						
Do not leave any inputs blank)	0 1111 1110 1100 1100 1101 1101 1101 1	FY2025				
	Further Further Importance Breakdown Breakdown Breakdown	NPUT Component Rating Component Rating Component Rating Component Component Rating Component Component Component Rating Component Component Component Rating Component				
Site-Wide Utilities 1. Water supply from South Shore to Island #1	5%	7				
Sewer From South Shore to Island #1	5%	7				
3. Roadway Lighting System a. Fixtures b. Poles c. Substations d. Panelboards e. Cable and Cable Tray System	15% 20% 20% 20% 20% 20%	8 7 9 9 9 9				
Emergency Telephone system a. Telephones b. Cabling/wiring	15% 50% 50%	9 9				
SCADA System a. CNC Cabinets and Equipment b. Standby Engine-Generators c. Panelboards d. Traffic Control signals	20% 25% 25% 25% 25%	9 8 8 9 9				
6. 15K Distribution System a. 15Kv Cable b. Cable Tray System	30% 70% 30%	8 7				
7. Toll Collection Systems	10%	8 8				





4. Repair Recommendations

Repair recommendations are separated into two categories, **Priority Repairs** and **Routine Repairs**, and are subject to change at each annual report based on progression of degradation observed during each inspection cycle. For this facility, **Priority Repairs** are for defects that likely will become problematic, or repair costs could escalate significantly if not addressed within the next 24 months. **Routine Repairs** are for defects that will likely not become problematic or see a significant increase in repair costs if not addressed within 60 months and should be completed as the opportunities present themselves during other maintenance and capital improvement operations. All Critical Findings require "immediate" action, and each Critical Finding is listed with the date it was reported to the District, and the recommended action or follow-up.





4.1 Trestles, North Channel Bridges, and Fisherman Inlet Bridges

4.1.1 Trestle A – Northbound (ANB)

Table 6: ANB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

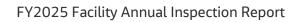
Table 7: ANB Priority Repairs

Element	Item Description	Quantity	Unit
Prestressed	Determine effectiveness of Lifejacket system at ANB57 Pile A	1	EA
Concrete Piles	where anode does not have a cable connecting it to the junction		
	box at the top of the pile and repair accordingly		
Prestressed	Install Scour Countermeasures at 10 pile locations where scour	10	EA
Concrete Piles	exceeds the Allowable Scour Depth (ASD).		

Table 8: ANB Routine Repairs

Element	Item Description	Quantity	Unit
Metal Bridge Railing	Repair spalled concrete curb with exposed reinforcement.	15	LF
Metal Bridge Railing	Repair delaminated failed concrete curb repair.	3	LF
Metal Bridge Railing	Repair bent or damaged railing.	18	LF
Metal Bridge Railing / Light Pole	Reinstall/Repair ground wire that are broken at railing connections or light pole connections.	7	EA
Reinforced Concrete Slab (Underside)	Repair area of delaminated concrete within Spans.	6	SF
Concrete Top Flange (Underside)	Repair spalling with exposed longitudinal and/or transverse reinforcement or where previous repairs have failed.	12	SF
Prestressed Concrete Girder	Repair spalling with exposed prestressing strands or exposed reinforcement.	52	LF
Prestressed Concrete Piles	Provide a repair to piles and pile caps with failed protective concrete coatings at locations where cracks, delamination, or spalling is present.	1,859	SF
Prestressed Concrete Piles	Monitor damaged Lifejacket system and repair as needed.	14	EA







Prestressed Concrete Piles	Replace anodes on piles with Life Jacket Systems where anode section loss was reported to be greater than 50%.	10	EA
Light Pole	Replace light poles that exhibit cracking at baseplate.	5	EA





4.1.2 Trestle A – Southbound (ASB)

Table 9: ASB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 10: ASB Priority Repairs

Element	Item Description	Quantity	Unit
Metal Bridge	Repair spalled concrete near railing post.	3	LF
Railing			

Table 11: ASB Routine Repairs

Element	Item Description	Quantity	Unit
Concrete Top	Repair spalling with exposed longitudinal and/or transverse	3	SF
Flange	reinforcement or where previous repairs have failed.		
(Underside)			
Prestressed	Repair failed repairs of delamination with rust staining or spalling	7	LF
Concrete Girder	with exposed prestressing strands / reinforcement.		
Prestressed	Repair top portion of pile exhibiting minor spall with exposed	3	EA
Concrete Piles	reinforcement at Bent-Pile.		
Prestressed	Repair failed repairs or areas of delamination with rust staining or	4	EA
Concrete Piles	spalling with exposed prestressing strands or reinforcement.		
Light Poles	Monitor light poles that exhibit cracking or delamination/spalling	16	EA
	near the baseplate and replace if vertically oriented steel		
	reinforcement becomes exposed and degraded.		





4.1.3 Trestle B – Northbound (BNB)

Table 12: BNB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 13: BNB Priority Repairs

Element	Item Description	Quantity	Unit
Prestressed	Install Scour Countermeasures at 18 pile locations where scour	18	EA
Concrete Piles	exceeds the Allowable Scour Depth (ASD).		

Table 14: BNB Routine Repairs

Element	Item Description	Quantity	Unit
Metal Bridge Railing	Repair/replace dented railing.	82	LF
Metal Bridge Railing	Replace corroded anchor bolt connections with section loss at 51 locations.	51	EA
Metal Bridge Railing / Light Pole	Reinstall/Repair ground wire that are broken at railing connections or light pole connections.	4	EA
Reinforced Concrete Slab (Underside)	Repair areas of delaminated and spalled concrete on deck underside.	6	SF
Concrete Top Flange (Underside)	Repair spalling with exposed longitudinal and/or transverse reinforcement or where previous repairs have failed.	45	SF
Prestressed Concrete Girder	Repair spalling with exposed prestressing strands or exposed reinforcement.	1	LF
Prestressed Concrete Piles	Repair piles with wide cracks, exposed prestressing strand on top portion, or exhibit rust staining.	100	EA
Prestressed Concrete Piles	Repair top portion of piles exhibiting minor spalls with exposed reinforcement.	51	EA
Prestressed Concrete Piles	Perform a repair to the steel built-up pile at BNB78 Pile B to prevent additional degradation or corrosion.	1	EA
Light Pole	Replace light poles that exhibit cracking at baseplate.	10	EA





4.1.4 Trestle B – Southbound (BSB)

Table 15: BSB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 16: BSB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 17: BSB Routine Repairs

Element	Item Description	Quantity	Unit
Metal Bridge	Repair/replace dented railing.	26	LF
Railing			
Steel Girders	Repair paint system at scattered locations where overcoat is peeling.	1	LS
Prestressed	Repair minor spalling with exposed strands.	1	LF
Concrete Girder			
Prestressed	Repair portion of piles exhibiting minor spalls with exposed	8	EA
Concrete Piles	reinforcement.		
Light Poles	Monitor light poles that exhibit cracking or delamination/spalling	10	EA
	near the baseplate and replace if vertically oriented steel		
	reinforcement becomes exposed.		





4.1.5 Trestle C – Northbound (CNB)

Table 18: CNB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 19: CNB Priority Repairs

Element	Item Description	Quantity	Unit
Prestressed	Fill void in spalled pile joint on CNB267 Pile C with Epoxy	1	EA
Concrete Piles			

Table 20: CNB Routine Repairs

Element	Item Description	Quantity	Unit
Metal Bridge	Repair/replace dented railing.	38	EA
Railing			
Metal Bridge	Repair spalled concrete curb with exposed reinforcement.	6	LF
Railing			
Metal Bridge	Repair failed concrete curb repairs.	13	LF
Railing			
Metal Bridge	Reinstall/Repair ground wire that are broken at railing	2	EA
Railing / Light	connections or light pole connections.		
Pole			
Concrete Top	Repair spalling with exposed longitudinal and transverse	20	SF
Flange	reinforcement.		
(Underside)			
Prestressed	Repair spalling with exposed prestressing strands or exposed	280	LF
Concrete Girder	reinforcement.		
Prestressed	Repair top portion of piles exhibiting minor spalls with exposed	55	EA
Concrete Piles	reinforcement.		
Prestressed	Replace junction box cover on Lifejacket System at Bent-Pile:	2	EA
Concrete Piles	CNB183-B and CNB191-C		
Light Pole	Replace light pole as required.	15	EA
Light Pole	Tighten loose anchor nut at light pole.	1	EA





4.1.6 Trestle C – Southbound (CSB)

Table 21: CSB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 22: CSB Priority Repairs

Eleme	ent	Item Description	Quantity	Unit
Prest	ressed	Install Scour Countermeasures at 39 pile locations where scour	39	EA
Conci	ete Piles	exceeds the Allowable Scour Depth (ASD).		

Table 23: CSB Routine Repairs

Element	Item Description	Quantity	Unit
Metal Bridge	Repair bent or damaged railing.	10	EA
Railing			
Metal Bridge	Repair spalling with exposed reinforcement or failed repairs.	64	SF
Railing (Curb)			
Concrete Top	Repair failed repairs of delamination with rust staining or spalling	7	SF
Flange	with exposed reinforcement.		
(Underside)			
Prestressed	Patch spalls on prestressed concrete girders, giving priority to	30	LF
Concrete Girders	girders exhibiting two or more exposed strands per location.		
Steel Diaphragms	Replace top protective coating on steel girders at locations where	1	LS
and Bracing	coating has bubbled and exhibits freckled rust.		
Concrete Pier	Repair detached conduit at CSB25 north face and replace broken	2	EA
Caps	conduit near light pole at CSB57		
Light Poles	Monitor light poles at that exhibit cracking or	10	EA
	delamination/spalling near the baseplate and replace if vertically		
	oriented steel reinforcement becomes exposed and degraded.		





4.1.7 North Channel Bridge – Northbound (NCB-NB)

This section was last inspected in FY2024 and will be inspected again in FY2026.

Table 24: NCB-NB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 25: NCB-NB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 26: NCB-NB Routine Repairs

Element	Item Description	Quantity	Unit
Steel	Repair/retrofit floor beam webs with isolated moderate to severe	3	EA
Floorbeams	section loss below stringer connections throughout the structure.		
Steel	Repair/retrofit areas of bottom flanges with isolated areas of minor	4	LF
Floorbeams	section loss throughout the structure.		
Steel	Repair/retrofit floor beam stiffeners over the longitudinal girders	15	EA
Floorbeams	with minor to moderate section loss throughout the structure.		
Steel	Retrofit or replace areas with greater than 25% section loss	1	LS
Superstructure	throughout the facility.		
Steel	Clean and repaint areas exhibiting coating system failure.	1	LS
Superstructure			
Reinforced	Seal cracks in splash zone at Piers.	9	EA
Concrete			
Columns			
Steel Truss	Repair/retrofit truss members with moderate section loss	1	LS
	throughout the structure.		
Light Pole	Repair cracks in light poles as required.	4	EA





4.1.8 North Channel Bridge – Southbound (NCB-SB)

This section was last inspected in FY2025 and will be inspected again in FY2027.

Table 27: NCB-SB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 28: NCB-SB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 29: NCB-SB Routine Repairs

Item Description	Quantity	Unit
Clean and recoat steel girders, cross frames, and diaphragms at	1	LS
locations of pack rust, where zinc primer is exposed, or coating is starting to fail.		
Reseat bearing pins that are not fully seated.	7	EA
Replace anchor bolts throughout catwalk and ladder system as	1	LS
required where hardware is exhibiting over 50% section loss.		
Clean and recoat ladder and catwalk.		
Update the 2023 Hydrographic Survey to indicate scour remediation (scour blanket) has been installed at below ASD Pier NCB-SB13 and verify if same remediation has been installed around near ASD piers.	1	LS
	Clean and recoat steel girders, cross frames, and diaphragms at locations of pack rust, where zinc primer is exposed, or coating is starting to fail. Reseat bearing pins that are not fully seated. Replace anchor bolts throughout catwalk and ladder system as required where hardware is exhibiting over 50% section loss. Clean and recoat ladder and catwalk. Update the 2023 Hydrographic Survey to indicate scour remediation (scour blanket) has been installed at below ASD Pier NCB-SB13 and verify if same remediation has been installed	Clean and recoat steel girders, cross frames, and diaphragms at locations of pack rust, where zinc primer is exposed, or coating is starting to fail. Reseat bearing pins that are not fully seated. 7 Replace anchor bolts throughout catwalk and ladder system as required where hardware is exhibiting over 50% section loss. Clean and recoat ladder and catwalk. Update the 2023 Hydrographic Survey to indicate scour remediation (scour blanket) has been installed at below ASD Pier NCB-SB13 and verify if same remediation has been installed





4.1.9 Trestle D – Northbound (DNB)

This section last received a hands-on inspection in FY2025 and will be inspected again in FY2027. An underwater inspection was performed in FY2022.

Table 30: Trestle DNB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 31: Trestle DNB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 32: Trestle DNB Routine Repairs

Element	Item Description	Quantity	Unit
Prestressed	Repair piles exhibiting minor spalls with exposed reinforcement at	13	EA
Concrete Piles	and wide cracks or rust staining.		
Prestress	Patch spalls on prestressed concrete girders, giving priority to	15	LF
Concrete Girders	girders exhibiting two or more exposed strands per location.		
Wingwall	Fill in undermining of west wingwall.	1	LS
Steel	Clean and paint steel superstructure/substructure adjacent to	1	Bent
Superstructure &	NCB-N.		
Substructure			





4.1.10 Trestle D – Southbound (DSB)

This section last received a hands-on inspection in FY2025 and will be inspected again in FY2027.

Table 33: Trestle DSB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 34: Trestle DSB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 35: Trestle DSB Routine Repairs

Element	Item Description	Quantity	Unit
Prestressed	Verify scour remediation was completed in 2002 on DSB1 and	1	EA
Concrete Piles	update the Hydrographic Survey - Acceptable Scour Depth to		
	include those limits.		





4.1.11 Trestle E, Fisherman Inlet Bridge, Trestle F – Northbound (ENB, FIB-NB, and FNB)

A hands-on inspection was performed in FY2025, and an underwater inspection was performed in FY2022, and element level data is presented in this section.

Table 36: ENB, FIB-NB, and FNB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 37: ENB, FIB-NB, and FNB Priority Repairs

Element	Item Description	Quantity	Unit
N/A	N/A	N/A	N/A

Table 38: ENB, FIB-NB, and FNB Routine Repairs

Element	Item Description	Quantity	Unit
Steel	Clean and recoat steel girders, cross frames, and diaphragms at	1	LS
Superstructure	locations of pack rust, where zinc primer is exposed, or coating is starting to fail.		
Fender System	Repair/reinstall top wale and portions of lower wale in fender system in FIBSB / FIBNB navigation channel.	1	LS





4.1.12 Trestle E, Fisherman Inlet Bridge, Trestle F – Southbound (ESB, FIB-SB, and FSB)

This section last received a hands-on inspection in FY2025 and will be inspected again in FY2027. An underwater inspection was performed in FY2022.

Table 39: ESB, FIB-SB, and FSB Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 40: ESB, FIB-SB, and FSB Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 41: ESB, FIB-SB, and FSB Routine Repairs

Element	Item Description	Quantity	Unit
Prestressed	Repair piles exhibiting minor spalls with exposed reinforcement.	2	EA
Concrete Piles			
Prestressed	Monitor damaged Lifejacket system and repair as needed.	3	EA
Concrete Piles			
Prestressed	Repair spalls with exposed prestressing strands or exposed	30	LF
Concrete Girder	reinforcement at beams.		
Drainage Swale -	Repair drainage swale in slope protection on west end of FSB.	1	LS
Roadway			
Approach			
Fender System	Repair/reinstall top wale and portions of lower wale in fender	1	LS
	system in FIBSB / FIBNB navigation channel.		



4.2 Tunnels

4.2.1 Thimble Shoal Channel Tunnel (TSCT)

A hands-on inspection was performed in FY2024. Maintenance records were reviewed as part of the NTIS inspection.

Table 42: TSCT Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 43: TSCT Priority Repairs

Element	Item Description	Quantity	Unit
Fan System	Repair fan system as needed to ensure 100% operation in	1	LS
	emergency mode.		

Table 44: TSCT Routine Repairs

Element	Item Description	Quantity	Unit
Concrete Invert	Repair spalled concrete along roadway edge of Sidewalk Side at	50	SF
Slab	several locations		
Steel Pedestrian	Clean and repaints steel railing along the Roadway.	15,840	SF
Railing			
Concrete Tunnel	Repair spalls with exposed reinforcement and conduits in exhaust	741	SF
Liner	duct at several locations		
Concrete Tunnel	Repair spalls with exposed reinforcement in supply duct at several	40	SF
Liner	locations		
Invert Slab	Repair spalls with exposed reinforcement in supply duct at several	275	SF
	locations		





4.2.2 Chesapeake Channel Tunnel (CCT)

A hands-on inspection was performed in FY2025 and will be inspected again in FY2027. Maintenance records were reviewed as part of the NTIS inspection.

Table 45: CCT Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 46: CCT Priority Repairs

Element	Item Description	Quantity	Unit
None	N/A	N/A	N/A

Table 47: CCT Routine Repairs

Element	Item Description	Quantity	Unit
Concrete Tunnel Liner	Repair spalls with exposed	675	SF
	reinforcement and conduits in exhaust		
	duct at several locations.		
Concrete Tunnel Liner	Repair spalls with exposed	236	SF
	reinforcement in supply duct at several locations.		
Concrete Portal	Repair minor large spalls throughout	20	SF
	Island 3 portal.		
Concrete Ceiling Slab	Repair areas of corrosion staining and or	222	SF
_	spalls in the ceiling slab.		
Concrete Invert Slab	Repair delaminations with corrosion	610	SF
	staining and spalls with exposed		
	reinforcement in supply duct at several		
	locations.		
Steel Pedestrian Railing	Clean and repaint steel pedestrian	6,070	LF
	railing along the roadway.		
Fire Protection System	Replace the non-functional height	N/A	N/A
	transducer for ground water storage		
	tank #2. (There are two total)		
Ventilation System	Replace opaque oil level sight glasses	1	EA
	on all twelve fan damper reducers.		
Pumps	Rehabilitate the packing seal at the rear	1	EA
	of pump BND-1.		
Pumps	Replace pumps BSD-1 and BND-1 with	1	EA
	appropriately sized main sump pumps.		
Tunnel Operations and Security	Repair phones at 490+47 Chesapeake	1	EA
System	Tunnel (7324) and 519+47 Chesapeake		
	Tunnel (7312).		



4.3 General Facility

Table 48: General Facility Critical Findings

Element	Item Description	Date Reported	Recommended Action/Follow-Up
None	N/A	N/A	N/A

Table 49: General Facility Priority Repairs

Element	Item Description	Quantity	Unit
Portal Island #4 -	Replace light poles exhibiting cracking and significant concrete	8	EA
Light Poles	spalling with exposed & corroded reinforcement		

Table 50: General Facility Routine Repairs

Element	Item Description	Quantity	Unit
Portal Island #3 & #4 – Approach Walls	Repair spalled and delaminated concrete along top and face of wall.	60	LF
Portal Island #3 – Light Pole	Repair/replace light poles exhibiting cracking with random rust staining	7	EA
Portal Island #3 & #4 – Grading	Continue to monitor and fill washouts and sinkholes near splash walls as required to prevent erosion.	1	LS
Portal Island #3 & #4 – Splash Wall	Repair spalled and delaminated concrete along the splash walls.	1	LS
Roadway Approach - North	Repair pothole/concrete pavement settlement along rigid pavement approach. Located approx. 675' south of toll plaza.	40	LF
Item (f) - Generator / Generator Building	Update tags on batteries to current expiration dates / maintenance needs.	1	LS
Item (i) - Storage Yard & Parking	Seal pavement cracks greater than 1/8" wide.	1	LS
Item (n) - Fence	Repair/Reinstall areas of damaged fence and broken barb arms/wire	1	LS





5. VDOT Structure Inventory and Appraisal (SI&A) Records (B-6 and B-7 Forms)





5.1 Trestle ANB SI&A Forms

B-6 ANB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID:1002Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:9/9/2024 & 9/10/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 46.62 Location: Over Chesapeake Bay
Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance

CONDITION OF STRUCTURE: Fair to Generally Good

- 1. Bent or damaged railing on spans ANB10, ANB12, ANB23, ANB27, ANB31, ANB192, ANB195 and ANB217.
- 2. Scattered minor transverse cracking and spalling or failed repairs in curbs, with exposed reinforcement at Span ANB182.
- 3. Ground wire broken at railing connections or light pole connections at the following locations: ANB191 over Bent ANB192 at light pole, ANB189, ANB198, ANB207, ANB210, ANB213.
- Some spalls on bottom flanges of some prestressed girders. locations with exposed rebar at bearings.
- 5. Girder spall with exposed prestressing strand/exposed reinforcement or signs of deterioration (rust staining / delam) at the following locations in FY2025: ANB177 G1 (web) ANB177 G3, ANB178 G1, ANB187 G4 (rust staining), ANB188 G3, ANB189 G5, ANB190 G8, ANB191 G5, ANB193 G4, ANB193 G7 (delam), ANB194 G7, ANB195 G7, ANB196 G1, ANB201 G5, ANB204 G3, ANB204 G6, ANB204 G7, ANB204 G8, ANB204.5 G5, ANB204.5 G8, ANB208.5 G2, ANB208.5 G3, ANB208.5 G5 (web), ANB213 G8 (web), ANB214 G1, ANB215 G1, ANB215 G1, ANB215 G2 ANB215G7, ANB219 G1, ANB219 G4. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded. In 2024, the District has completed 55 beam repairs from ANB18 through ANB169 and ANB222 and ANB225.
- Some existing spall repairs on concrete girders are failing or have failed, with some exposing two strands near mid-span or exposing rebar at bearings. Hairline diagonal web cracks on approx. 50% of concrete girder ends inspected.
- 7. Minor to moderate defects on approximately 5% of elastomeric bearings.
- 8. One location requiring repair of expansion joint strap in the cable tray at ANB117.
- Scattered minor damage to fiberglass jackets on some piles. Nine piles exhibit damage of fiberglass jacket exposing sacrificial zinc mesh (ANB33-A, ANB81-C, ANB89-A, ANB133-A, ANB133-B, ANB135-C, ANB142-C, ANB152-C, ANB173-A, ANB181-B, ANB197-A, ANB199-A, ANB202-A, ANB202-B, ANB210-C). Anode is disconnected at bent 57 pile A.
- 10. Minor reflective cracking through coating at some piles and bent caps, with isolated rust staining and efflorescence at a few locations. Other areas of spalling coating noted. Piles exhibiting rust staining were sounded and concrete was intact.
- 11. Minor to moderate cracking and map cracking on many of the bent caps inspected, primarily at the ends of the bent caps. Isolated spall noted at ANB184 between Piles B & C with exposed reinforcement with up to 20% section loss.
- 12. Existing repairs to piles with impact damage are typically sound with exception at the following locations: ANB189-A, ANB199-B.
- 13. Spalls recommended for repair noted in the underside of the deck (top flanges) at 57 locations. FY2025 inspection indicated the following locations with exposed reinforcement with section loss includes: ANB180 B2, ANB182 B4, ANB183 B4, ANB195 B6, ANB203 B4, ANB204.5 B6, ANB205.5 B6, ANB206.5 B2, ANB209 B6, ANB214 B4 and ANB215 B7.





Page: 2 of 2

B-6 ANB VDOT – BRIDGE INSPECTION REPORT

- 14. 2023 Hydrographic Survey indicates bay bottom was below Allowable Scour Depth (ASD) at Bent ANB218 ANB222 and ANB225; however, scour remediation is identified in place. Survey also indicates that bay bottom is near ASD at Bent ANB214.
- 15. Scour ratings govern over the condition ratings at only a small percentage of the bents (piles considered to be Scour Condition State 3 (due to scour remediation) are Bent-Pile: ANB218 through ANB222 and ANB225) for all piles located within those bents. In addition to those piles, all piles at Bent ANB214 have been identified as Scour Condition State 3 since it is near bay bottom, but no remediation has been completed.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. Structure load rating controlled by modular superstructure units with reduced capacity from having two-prestressing strands removed due to corrosion (worst-case observed during past inspections).
- 3. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Continue periodic hydrographic surveys in interim between underwater inspections scheduled every 5 years
- 2. Continue to patch spalls on prestressed concrete girders, giving priority to girders exhibiting two or more exposed strands per location.
- 3. Repair bent or damaged railing on spans ANB10, ANB12, ANB23, ANB27, ANB31, ANB192, ANB195 and ANB217.
- 4. Continue to patch any new spalls and failed repairs on prestressed concrete girders.
- 5. Retrofit any deteriorated cable tray expansion joint straps as required.
- 6. Repair spalls in underside of deck with priority to those exposing multiple layers of steel.
- 7. Repair spalls in curbs with exposed reinforcement.
- 8. Repair/reattach broken ground wire at railing or light pole connections at the following locations: ANB191 over Bent ANB192 at light pole, ANB189, ANB198, ANB207, ANB210, ANB213.
- 9. Monitor damaged lifejacket system and repair as needed.
- 10. Replace light poles that exhibit cracking at baseplates.

SIGNATURE OF INSPECTOR

Sharrow M. S. Turner

SIGNATURE OF REVIEWER





B-7 ANB	VDOT – BRIDGE INS	PECTION REPORT	Page: 1 of 4			
Structure-ID: County/City: Structure: Main Route: Milepost: Lead Inspector:	1002 Northampton (Co./Str.No) 13 46.62 Shannon Turner, PE	Type: Date of Inspection: Feature/Intersection: Facility/Carried: Location: Additional Inspector(s):	Regular Inspection 9/9/2024 & 9/10/2024 Chesapeake Bay Over Chesapeake Bay Ethan Stivers, Jon Krites			
SPECIAL CONDITI	SPECIAL CONDITIONS OR REQUIREMENTS					
 Fracture Critical Underwater Scour Critical Moveable 	l _ <u>X</u> X	5. Segmental 6. Pin & Hanger 7. Fatigue Prone	- - -			
36 TRAFFIC SAFE	TY FEATURES					
1. Bridge Railing 2. Transition	<u>1</u> 1	3. Approach 4. Approach Guardrail	1 1			

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK		GENERAL CONDITION RATING		[7]
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

- a. Replacement of Asphalt Wearing Surface and Open Expansion Joint Header Material was completed in Spring 2020.
- b. Scattered minor cracking with some locations having efflorescence and spalling and along joints on underside of deck and top flange.
- c. Spalls recommended for repair noted in the underside of the deck (top flanges) at 57 locations. FY2025 inspection indicated the following locations with exposed reinforcement with section loss includes: ANB180 B2, ANB182 B4, ANB183 B4, ANB195 B6, ANB203 B4, ANB204.5 B6, ANB205.5 B6, ANB206.5 B2, ANB209 B6, ANB214 B4 and ANB215 B7.
- d. Isolated delaminations/spalls noted on overhang fascia between scuppers.
- e. Bridge railing has surface rust noted on hardware, typical.
- f. Bent or damaged railing on spans ANB10, ANB12, ANB23, ANB27, ANB31, ANB192, ANB195 and ANB217.
- g. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Spall with exposed reinforcement noted in Span 182.
- h. Ground wire broken at railing connections or light pole connections at the following locations: ANB191 over Bent ANB192 at light pole, ANB189, ANB198, ANB207, ANB210, ANB213.





B-7 ANB	VDOT – BRIDGE INSPECTION REPORT		Page: 2	2 of 4
59 SUPERSTRUCTURE		GENERAL CONDITION RAT	ING	[7]
1. Bearing Devices	E	4. Trusses		
2. Stringers	<u>N</u>	A. General	<u>N</u>	
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>	
A. General	<u>F</u>	C. Bracing	<u>N</u>	
B. Diaphragms or	<u>G</u>	5. Paint	<u>N</u>	
Cross Frames		Year Painted	<u>N</u>	
C. Bracing	N	6. Machinery (Moveable Span)	N	

- a. Minor to moderate splitting of isolated bearings pads on 5% of bearings.
- b. Hairline diagonal web cracks noted on girder ends; with approx. 50% of concrete girders inspected.
- c. Isolated spalls on bottom face of random prestressed girder flanges, and some existing spall repairs on girders are failing or have failed with exposing two strands near mid-span or exposing rebar at bearings. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded.
- d. Girder spall with exposed prestressing strand/exposed reinforcement or signs of deterioration (rust staining / delam) at the following locations in FY2025: ANB177 G1 (web) ANB177 G3, ANB178 G1, ANB187 G4 (rust staining), ANB188 G3, ANB189 G5, ANB190 G8, ANB191 G5, ANB193 G4, ANB193 G7 (delam), ANB194 G7, ANB195 G7, ANB196 G1, ANB201 G5, ANB204 G3, ANB204 G6, ANB204 G7, ANB204 G8, ANB204.5 G5, ANB204.5 G8, ANB208.5 G2, ANB208.5 G3, ANB208.5 G5 (web), ANB213 G8 (web), ANB214 G1, ANB215 G1, ANB215 G2 ANB215G7, ANB219 G1, ANB219 G4. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded. In 2024, the District has completed 55 beam repairs from ANB18 through ANB169 and ANB222 and ANB225.
- e. Isolated spalls noted on diaphragms.
- f. Expansion joint strap in cable tray broken at ANB117.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [7]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	G G G N N S G *	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	- - - - - - <u>F</u> <u>G</u> <u>G</u> <u>N</u>
REMARKS:			





Page: 3 of 4

B-7 ANB VDOT – BRIDGE INSPECTION REPORT

- a. Underwater Inspection performed 5/2023 10/2023. See FY2023 Underwater Inspection Forms for location of underwater defects and Element Level Inspection Forms for condition ratings of substructure units.
- b. Minor to moderate map cracking at ends of some of the bent caps inspected. Isolated spall noted at ANB184 between Piles B & C with exposed reinforcement with up to 20% section loss.
- c. Most all repairs completed as part of Substructure Repair Project generally appear in good condition. Scattered minor damage to fiberglass jackets on some piles. Fourteen piles exhibit damage of fiberglass jacket exposing sacrificial zinc mesh (ANB33-A, ANB81-C, ANB89-A, ANB133-A, ANB133-B, ANB135-C, ANB142-C, ANB152-C, ANB173-A, ANB181-B, ANB197-A, ANB199-A, ANB202-A, ANB202-B, ANB210-C). Anode is disconnected at bent 57 pile.
- d. Minor reflective cracking through coating at some pile caps and piles, with isolated rust staining and efflorescence at a few locations. Other areas of spalling coating noted. Piles exhibiting rust staining were sounded and concrete was intact.
- e. Most of the underwater portions of the piles are rated CS1 or CS2 (86%) with a majority of the CS2 ratings attributed to good repairs.

61 CHANNEL: CHANN	IEL/SLOPE PROTECTION	GENERAL CONDITIO	N RATING	[8]
Channel Scour Embankment Erosion	<u>G</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope	- - <u>G</u>	
3. Drift4. Vegetation	-	Protection 8. Adequacy of Opening	<u>E</u>	

REMARKS:

- 16. 2023 Hydrographic Survey indicates bay bottom was below Allowable Scour Depth (ASD) at Bent ANB218 ANB222 and ANB225; however, scour remediation is identified in place. Survey also indicates that bay bottom is near ASD at Bent ANB214.
- 17. Scour ratings govern over the condition ratings at only a small percentage of the bents (piles considered to be Scour Condition State 3 (due to scour remediation) are Bent-Pile: ANB218 through ANB222 and ANB225) for all piles located within those bents. In addition to those piles, all piles at Bent ANB214 have been identified as Scour Condition State 3 since it is near bay bottom, but no remediation has been completed.

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	Visibility	<u>N</u>
(R12 – 5) – Single	<u>N</u>		
(R12 – 5) – Semi	<u>N</u>		

REMARKS:

a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.





B-7 ANB VDOT – BRIDGE INSPECTION REPORT

- Page: 4 of 4
- b. Structure load rating controlled by modular superstructure units with reduced capacity from having two-prestressing strands removed due to corrosion (worst-case observed during past inspections, not present this year).
- c. No posting of the structure is required.





5.2 Trestle ASB SI&A Forms

B-6 ASB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1010 Type: Regular Inspection

County/City: Northampton Date of Inspection: 9/26/2024 & 9/27/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 46.72 Location: Over Chesapeake Bay
Lead Inspector: Jon Chapman, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance.

CONDITION OF STRUCTURE: Good

- Light poles at ASB42, ASB45, ASB57, ASB69, ASB81, ASB86, ASB99, ASB105, ASB107, ASB109, ASB115, ASB119, ASB121, ASB129, ASB131, and ASB133 exhibited cracking at the baseplate.
- 2. Minor hairline cracking and minor spalls on some of the piles above waterline.
- 3. Exposed reinforcement chairs located on cap faces, underside of girder (typically near ends) and underside of deck throughout spans.
- 4. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- 5. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- 6. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement.
- 7. Bent bridge railing on Spans ASB125 and 126 and loose shim plate or anchor plate at Span ASB151 and 167. Misaligned anchor plates along ASB56 (5 total). Two posts with impact damage/alignment, ASB3 and ASB56
- 8. Deck spall with exposed reinforcement at Span ASB47, ASB109 and ASB164, and minor cracking and efflorescence at scattered locations in underside of deck. Spalls in concrete wearing surface of span ASB2 with no exposed reinforcement.
- Spalled and cracked concrete at lower post tension duct at ASB82 and ASB93.
- 10. Girder spall with exposed prestressing strand at Span 54 Girder 3 and Span 56 Girder 2, and reinforcement at Span ASB132, and ASB48 G5.
- 11. Scattered minor spalls along bottom flanges and webs where insufficient cover is provided to shear reinforcement or prestressing strands.
- 12. Large spalling or locations with exposed reinforcement or exposed strands at girder ends or in the lower portion of the continuity closure pours at 75 locations.
- 13. Roadway striping replaced in 2018 remains in good condition.
- 14. Bay bottom is typically at or above the Allowable Scour Depth (ASD), with some previous scour remediation in place. Bent ANB202 through ANB204 have bay bottom below ASD but has scour remediation in place.
- 15. Most of the underwater portions of the piles are in very good condition. Minor spalling, a few hairline cracks and one narrow crack at a few bents at or below the waterline. Based on FY2025 inspection piles with exposed reinforcement/prestressing strands includes: ASB43-B.
- 16. Erosion on east side of the South Abutment ASB1 around water and sewer lines and under slope protection and on the west side of the abutment.

REVISED STRESS ANALYSIS:

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by the simple span steel structure at Span ASB204.
- c. No posting of the structure is required.



B-6 ASB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

RECOMMENDATIONS:

- 1. Monitor light poles that exhibit cracking or delamination/spalling near the baseplate and replace if vertically oriented steel reinforcement becomes exposed and degraded.
- 2. Continue periodic hydrographic surveys in interim between underwater inspections scheduled every 5 years.
- 3. Install scour remediation at Bents ASB202 through ASB204 due to hydrographic survey identifies these locations as below the Allowable Scour Depth.
- 4. Clean exposed reinforcement and repair spalling in girder bottom flanges noted this year and in previous reports.
- 5. Clean exposed reinforcement and patch spalls/voids previously noted in piles above water.
- 6. Place fill at locations of erosion on east side of the South Abutment ASB1 around water and sewer lines and under slope protection.
- 7. Retrofit any deteriorated cable tray expansion joint straps as required.
- 8. Clean and fill voids at joint header and asphalt wearing surface interface with joint sealant material.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





B-7 ASB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 3

Structure-ID: 1010 Type: Regular Inspection

County/City: Northampton Date of Inspection: 9/26/2024 & 9/27/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost:46.72Location:Over Chesapeake BayLead Inspector:Jon Chapman, PEAdditional Inspector(s):Ethan Stivers, Jon Krites

SPECIAL CONDITIONS OR REQUIREMENTS

1. Fracture Critical	_	5. Segmental	_	
2. Underwater	<u>X</u>	6. Pin & Hanger	_	
3. Scour Critical	<u>X</u>	7. Fatigue Prone	_	
4. Moveable				

36 TRAFFIC SAFETY FEATURES

1. Bridge Railing 1 3. Approach 1 2. Transition 4. Approach Guardrail 1

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK		GENERAL CONDITION RATING		[8]
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

REMARKS:

1 | 1

- a. Light poles at ASB42, ASB45, ASB57, ASB69, ASB81, ASB86, ASB99, ASB105, ASB107, ASB109, ASB115, ASB119, ASB121, ASB129, ASB131, and ASB133 exhibited cracking at the baseplate.
- b. Deck spall with exposed reinforcement at Span ASB109 and ASB164, and minor cracking and efflorescence at scattered locations in underside of deck.
- Exposed reinforcement chairs located on underside of deck throughout spans.
- d. Bent bridge railing on Spans ASB125 and 126 and loose shim plate or anchor plate at Span ASB151 and 167. Misaligned anchor plates along ASB56 (5 total). Two posts with impact damage, ASB2 and ASB56.
- e. Deck spall with exposed reinforcement at Span ASB109 and 164, and minor cracking and efflorescence at scattered locations in underside of deck. Spalls in concrete wearing surface of span ASB3 with no exposed reinforcement.
- f. Roadway striping replaced in 2018 remains in good condition





B-7 ASB	VDOT – BRIDGE INSPECTION REPORT		Page: 2 of 3
59 SUPERSTRUCTURE		GENERAL CONDITION RATI	NG [8]
 Bearing Devices Stringers Girders, Beams, or Sl A. General B. Diaphragms or Cross Frames 	<u>G</u> <u>G</u>	4. Trusses A. General B. Portals C. Bracing 5. Paint Year Painted	N N N G 2013
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>

REMARKS:

- a. Exposed reinforcement chairs located underside of girder (typically near ends).
- b. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- c. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4). Spalls noted at 165 beam ends in FY2025 inspection.
- d. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement
- e. Large spalling or locations with exposed reinforcement or exposed strands at girder ends or in the lower portion of the continuity closure pours at 75 locations.
- f. Scattered minor spalls along bottom flanges and webs where insufficient cover is provided to shear reinforcement or prestressing strands.
- g. Girder spall with exposed prestressing strand at Span 54 Girder 3 and Span 56 Girder 2, and reinforcement at Span ASB132, and ASB48 G5.
- h. Spalled and cracked concrete at lower post tension duct at ASB82 and ASB93.
- i. Damaged expansion joint plates in the cable trays at Bents ASB 125 and ASB128, and disconnected ground wires at ASB123, ASB127, and ASB151.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [8]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	N N N N N N N N N N N N N N N N N N N	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	- - - - - - G G G





B-7 ASB

VDOT – BRIDGE INSPECTION REPORT

Page: 3 of 3

- a. Underwater Inspection performed 6/2023 10/2023. See FY2023 Underwater Inspection Forms for location of underwater defects and Element Level Inspection Forms for condition ratings of substructure units.
- b. Most of the underwater portions of the piles are in very good condition. Minor spalling, a few hairline cracks and one narrow crack at a few bents at or below the waterline. Piles with exposed reinforcement/prestressing strands includes: ASB43-B.
- c. Minor spalling, a few hairline cracks and one narrow crack at a few bents at or below the waterline
- d. Minor hairline cracking and minor spalling on some of the piles
- e. Minor hairline cracking or map cracking on scattered bent caps
- f. Erosion on east side of the South Abutment ASB1 around water and sewer lines and under slope protection

61 CHANNEL: CHAN	NEL/SLOPE PROTECTION	GENERAL CONDITION	ON RATING	[8]
 Channel Scour Embankment Erosion Drift Vegetation 	<u>G</u> - - -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope	- <u>G</u> <u>E</u>	
REMARKS:				
except between	drographic survey, bay botto Bent ANB202 through ANB2 ndition State 4 for element le	04 where it is below the AS	D. These locati	
66 POSTED LOADIN	G			
1. Posted Loading -		2. Legibility	<u>N</u>	

REMARKS:

1 | 1

(R12-1)

(R12 - 5) - Single N(R12 - 5) - Semi N

a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.

3. Visibility

<u>N</u>

b. Structure load rating controlled by the simple span steel structure at Span ASB204. No posting of the structure is required.





5.3 Trestle BNB SI&A Forms

B-6 BNB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1004 Type: Regular Inspection

County/City: Northampton Date of Inspection: 9/11/2024 – 9/13/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 41.31 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance. The wearing surface was replaced in Fall 2020.

CONDITION OF STRUCTURE: Generally Fair to Good

- 1. Light poles at BNB108, BNB111, BNB120, BNB129, BNB132, BNB141, BNB144, BNB156, BNB180, and BNB189 exhibited cracking at the baseplate. Light pole with delamination along face of pole at BNB178.
- 2. Ground wire broken at railing connections or light pole connections at the following locations: BNB160, BNB174, BNB181, BNB192.
- Bent or damaged railing on spans BNB161, BNB163, BNB177, BNB187 (48 LF), BNB193, BNB197 (16 LF), BNB200 (5 LF), BNB201, BNB206, BNB208, BNB210.
- 4. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at BNB169, BNB190, BNB193.
- 5. Exposed sacrificial mesh or wide cracking in grouted annulus of LifeJacket System on Bent BNB23 Pile B, BNB74-C, BNB102-A, BNB117-A, BNB127-A, BNB161-A, and BNB214-A.
- 6. New spall repairs on bottom flange of girders in good condition with minor shrinkage cracks noted.
- Isolated spalls with exposed reinforcement on underside of deck and/or top flange in the following locations in FY2025: BNB162 (3 locations) (w. overhang/sidewalk), BNB163, BNB179 (w. overhang/sidewalk), BNB181 G2 and w. overhang/sidewalk, BNB184 (w. overhang/sidewalk), BNB185 G4, BNB186 (w. overhang/sidewalk), BNB192 G1, BNB199 (w. overhang/sidewalk), BNB201 B6, BNB202 (w. overhang/sidewalk), BNB208 (w. overhang/sidewalk), BNB210 (w. overhang/sidewalk).
- 8. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025 (** notes location is currently being repaired by the District): BNB159 G5**, BNB159 G8, BNB159 G8**, BNB160 G1**, BNB160 G8, BNB162 G8**, BNB163 G1 (2), BNB163 G4 (2), BNB163 G8, BNB165 G5 (rust staining), BNB166 G6 (rust staining), BNB167 G6** (rust staining), BNB167 G2**, BNB169 G2, BNB170 G1**, BNB170 G5**, BNB171 G4, BNB172 G1, BNB172 G2, BNB172 G4, BNB173 G7**, BNB174 G2, BNB176 G1, BNB177 G2, , BNB177 G5 (web), BNB180 G7, BNB182 G2**, BNB184 G3 (rust staining), BNB185 G6 (rust staining), BNB185 G5, BNB185 G7**, BNB187 G3, BNB187 G4, BNB188 G1, BNB190 G1, BNB191 G6 (2), BNB192 G5 (2), BNB192 G8, BNB193 G1 (2), BNB193 G2, BNB193 G4, BNB193 G5, BNB194 G1, BNB194 G6, BNB195 G1, BNB196 G1, BNB198 G2, BNB198 G3, BNB198 G4, BNB198 G6, BNB199 G1(2), BNB200 G2, BNB200 G3 (rust staining), BNB201 G4, BNB201 G6, BNB201 G8, BNB202 G1, BNB202 G6, BNB203 G1, BNB204 G8, BNB205 G1, BNB205 G4, BNB206 G4, BNB208 G1, BNB209 G1 (2), BNB210 G2, BNB210 G8, BNB211 G1, BNB212 G1. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded. In 2024, the District has completed 56 beam repairs from ANB19 through BNB121.
- 9. Scattered cracks and spalls on bottom flanges of prestressed girders. A few failed patches at older repair locations, including exposed strands near mid-span and exposed rebar at some bearings.
- 10. Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: BNB160-B, BNB161-B, BNB168-A, BNB168-C, BNB173-B, BNB176-B, BNB178-C, BNB184-B, BNB190-B, BNB193-B, BNB196-A, BNB199-B, BNB202-B, BNB203-B, BNB209-A, BNB209-B, BNB213-A.





B-6 BNB VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

- 11. Hairline diagonal web cracking on approximately 50% of concrete girder ends inspected.
- 12. Minor to moderate splitting on approximately 9% of elastomeric bearings.
- 13. Minor map cracking on a few of the bent cap ends inspected.
- 14. Several locations where main cable tray grounding straps are disconnected.
- 15. Scour ratings govern over the condition ratings for piles considered to be Scour Condition State 3 or Condition State 4 (due to no scour remediation) for the following bents: Bent-Pile: BNB212 BNB214 (below the Allowable Scour), BNB220-BNB239 (nearing the Allowable Scour), BNB40 BNB43 (below the Allowable Scour) and BNB244 and BNB245 (nearing the Allowable Scour). A total of 51 piles are considered under Condition State 3 and 18 are considered under Condition State 4.
- 16. For complete details on condition of the underwater portion of piles, see the FY2025 Underwater Inspection Forms. Level II and III Inspection of the Pile Repair at BNB78 Pile B was performed and the steel built-up pile used for the repair appears to exhibit moderate corrosion (approximately 25% section loss). ANB262 Pile C UW report indicates a sheared pile with a Steel "H" Pile Driven (note was noted in 2019 UW Inspection Report and verified in 2023 UW report). Note is for informational purposes only as an As-Built Condition.

REVISED STRESS ANALYSIS:

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by modular superstructure units with reduced capacity from having two-prestressing strands removed due to corrosion (worst-case observed during past inspections).
- c. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Repair all cracks narrow or greater and repair spalls with exposed reinforcement in the piles.
- 2. Determine effectiveness of damaged LifeJacket System on Bent BNB23 Pile B, BNB74-C, BNB102-A, BNB117-A, BNB127-A, BNB161-A, and BNB214-A, repair accordingly. CBBT is exploring an alternative repair method that will utilize carbon fiber shells to address the deterioration.
- 3. Continue periodic hydrographic surveys in interim between underwater inspections scheduled every 5 years.
- 4. Continue to monitor paint condition of steel beam span (BNB1/BNB2).
- 5. Repair bent cable tray retainer plate at BNB71.
- 6. Perform a repair to the steel built-up pile at BNB78 Pile B to prevent additional degradation or corrosion.
- 7. Continue to patch spalls on prestressed concrete girders, giving priority to girders exhibiting two or more exposed strands per location.
- 8. Repair bent or damaged railing.
- 9. Continue to patch any new spalls and failed repairs on prestressed concrete girders.
- 10. Repair spalls in underside of deck with priority to those exposing multiple layers of steel.
- 11. Repair spalls in curbs with exposed reinforcement.
- 12. Repair/reattach broken ground wire at railing or light pole connections.
- 13. Replace light poles that exhibit cracking at baseplates.
- 14. Install scour remediation as necessary and verify limits of remediation in accordance with the hydrographic survey.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





B-7 BNB **VDOT – BRIDGE INSPECTION REPORT** Page: 1 of 4 Structure-ID: 1004 Regular Inspection Type: Date of Inspection: 9/11/2024 - 9/13/2024 County/City: Northampton Structure: Feature/Intersection: Chesapeake Bay (Co./Str.No) 13 Main Route: Facility/Carried: Milepost: Location: Over Chesapeake Bay 41.31 Additional Inspector(s): Ethan Stivers, Jon Krites Lead Inspector: Shannon Turner, PE SPECIAL CONDITIONS OR REQUIREMENTS 1. Fracture Critical 5. Segmental 2. Underwater <u>X</u> 6. Pin & Hanger 3. Scour Critical <u>X</u> 7. Fatigue Prone 4. Moveable **36 TRAFFIC SAFETY FEATURES** 1. Bridge Railing 1 3. Approach N

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

4. Approach Guardrail

1

58 DECK		GENERAL CONDITION	ON RATING	[7]
 Wearing Surface Deck – Structural Curbs Median Sidewalks 	<u>G</u> <u>G</u> <u>N</u>	6. Railing 7. Drains 8. Lighting 9. Utilities 10. Expansion Joints	<u>G</u> <u>N</u> <u>G</u> <u>G</u>	
		or Devices		

REMARKS:

2. Transition

N

- a. Replacement of the asphalt wearing surface and open expansion joint header material is underway as part of the Mill and Repave Trestles, Replace End Dams Project (RMF 2053.2025) that began on September 8, 2020 (immediately following the inspection of this structure). Work on this structure was completed by the end of Fall 2020.
- b. Scattered minor cracking and spalling on underside of deck and sidewalk.
- c. Scattered minor transverse cracking and scale up to 1/16" in curbs.
- d. Asphalt cracked full width along expansion joints throughout.
- e. Exterior fascia has longitudinal crack up to 1/16" wide at isolated locations between scuppers.
- f. Light poles at BNB108, BNB111, BNB120, BNB129, BNB132, BNB141, BNB144, BNB156, BNB180, and BNB189 exhibited cracking at the baseplate. Light pole with delamination along face of pole at BNB178.
- g. Ground wire broken at railing connections or light pole connections at the following locations: BNB160, BNB174, BNB181, BNB192.
- h. Bent or damaged railing on spans BNB161, BNB163, BNB177, BNB187 (48 LF), BNB193, BNB197 (16 LF), BNB200 (5 LF), BNB201, BNB206, BNB208, BNB210, BNB219, BNB234, BNB238, BNB249, BNB261.





Page: 2 of 4

B-7 BNB

VDOT – BRIDGE INSPECTION REPORT

- i. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at BNB169, BNB190, BNB193
- j. Isolated spalls with exposed reinforcement on underside of deck in the following locations in FY2025: BNB162 (3), BNB163, BNB179, BNB181, BNB184, BNB185, BNB186, BNB192, BNB199, BNB201, BNB202, BNB208, BNB210.
- k. Isolated pack rust and surface rust on rail hardware throughout.

59 SUPERSTRUCTURE		GENERAL CONDITION RATI	NG [7]	
 Bearing Devices Stringers Girders, Beams, or S A. General B. Diaphragms or Cross Frames C. Bracing 	<u>E</u>	 4. Trusses A. General B. Portals C. Bracing 5. Paint Year Painted 6. Machinery (Moveable Span) 	N N N E 2013 N	

- a. Some spalls on bottom flanges of some prestressed girders, and some existing spall repairs on girders are failing or have failed.
- b. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025 (** notes location is currently being repaired by the District): BNB159 G5**, BNB159 G8, BNB159 G8**, BNB160 G1**, BNB160 G8, BNB162 G8**, BNB163 G1 (2), BNB163 G4 (2), BNB163 G8, BNB165 G5 (rust staining), BNB166 G6 (rust staining), BNB167 G6** (rust staining), BNB167 G2**, BNB169 G2, BNB170 G1**, BNB170 G5**, BNB171 G4, BNB172 G1, BNB172 G2, BNB172 G4, BNB173 G7**, BNB174 G2, BNB176 G1, BNB177 G2, , BNB177 G5 (web), BNB180 G7, BNB182 G2**, BNB184 G3 (rust staining), BNB185 G6 (rust staining), BNB185 G5, BNB185 G7**, BNB187 G3, BNB187 G4, BNB188 G1, BNB190 G1, BNB191 G6 (2), BNB192 G5 (2), BNB192 G8, BNB193 G1 (2), BNB193 G2, BNB193 G4, BNB193 G5, BNB194 G1, BNB194 G6, BNB195 G1, BNB196 G1, BNB198 G2, BNB198 G3, BNB198 G4, BNB198 G6, BNB199 G1(2), BNB200 G2, BNB200 G3 (rust staining), BNB201 G4, BNB201 G6, BNB201 G8, BNB202 G1, BNB202 G6, BNB203 G1, BNB204 G8, BNB205 G1, BNB205 G4, BNB206 G4, BNB208 G1, BNB209 G1 (2), BNB210 G2, BNB210 G8, BNB211 G1, BNB212 G1.The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded. In 2024, the District has completed 56 beam repairs from ANB19 through BNB121.
- c. Scattered cracks and spalls on bottom flanges of prestressed girders. A few failed patches at older repair locations, including exposed strands near mid-span and exposed rebar at some bearings.
- d. Hairline diagonal web cracking on approximately 50% of concrete girder ends inspected.
- e. Minor to moderate splitting on approximately 9% of elastomeric bearings.
- f. Hairline diagonal web cracking on 25% of girders inspected.

60 SUBSTRUCTUR	E	GENERAL CONDITION RATING	[4]
1. Abutments		2. Pier/Bent	
A. Wings	<u>G</u>	A. Caps	





B-7	BNB	VDOT – BRIDGE INSPECTION REPORT		Page:	Page: 3 of 4	
	B. Backwall	<u>G</u>	B. Piles	_		
	C. Bearing Seats	<u>G</u>	C. Column, Stem, Wall	_		
	D. Breastwall	<u>N</u>	D. Piles	_		
	E. Weepholes	<u>N</u>	E. Bracing	_		
	F. Footing	<u>G</u>	F. Erosion/Scour	<u>P</u>		
	G. Piles	*	G. Settlement	_		
	H. Erosion/Scour	<u>G</u>	3. Pile Bent			
	I. Settlement	<u>G</u>	A. Caps	<u>F</u>		
*No	t Visible		B. Bearing Seats	<u>G</u>		
			C. Piles	<u>F</u>		
			D. Bracing	N		

- a. Erosion/Scour Condition rating for the Pier/Bent is controlled by scour, see Item a) under Remarks under 61 Channel: Channel Slope/Protection.
- b. Spalls with exposed reinforcement on pile cap at BNB100 and abutment at BNB266, and delamination on BNB85, BNB243, & BNB265,
- c. Minor cracking on many of the piles. A few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile or rust staining. Some spalling with exposed reinforcement steel noted near top.
- d. Spall of LifeJacket fiberglass jacket with exposed sacrificial mesh on Bent BNB23 Pile B, BNB74-C, BNB102-A, BNB117-A, BNB127-A, BNB161-A, and BNB214-A are recommended for evaluation to determine effectiveness of the cathodic protection system and to be repaired accordingly.
- e. Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: BNB160-B, BNB161-B, BNB168-A, BNB168-C, BNB173-B, BNB176-B, BNB178-C, BNB184-B, BNB190-B, BNB193-B, BNB196-A, BNB196-B, BNB199-A, BNB199-B, BNB202-B, BNB203-B, BNB209-A, BNB209-B, BNB213-A.
- f. For complete details on condition of the underwater portion of piles, see the FY2025 Underwater Inspection Forms. Level II and III Inspection of the Pile Repair at BNB78 Pile B was performed and the steel built-up pile used for the repair appears to exhibit moderate corrosion (approximately 25% section loss). ANB262 Pile C UW report indicates a sheared pile with a Steel "H" Pile Driven (note was noted in 2019 UW Inspection Report and verified in 2023 UW report). Note is for informational purposes only as an As-Built Condition





B-7 BNB	VDOT – BRIDGE INSPECTION REPORT			of 4
61 CHANNEL: CHANNE	EL/SLOPE PROTECTION	GENERAL CONDITION	RATING	[4]
 Channel Scour Embankment Erosion Drift 	<u>P</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/SlopeProtection	– <u>G</u>	
4. Vegetation	-	8. Adequacy of Opening	<u>E</u>	

REMARKS:

g. Scour ratings govern over the condition ratings for piles considered to be Scour Condition State 3 or Condition State 4 (due to no scour remediation) for the following bents: Bent-Pile: BNB212 – BNB214 (below the Allowable Scour), BNB220-BNB239 (nearing the Allowable Scour), BNB40 – BNB43 (below the Allowable Scour) and BNB244 and BNB245 (nearing the Allowable Scour). A total of 51 piles are considered under Condition State 3 and 18 are considered under Condition State 4.

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single	<u>N</u>		
(R12 – 5) – Semi	<u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- Structure load rating controlled by modular superstructure units with reduced capacity from having twoprestressing strands removed due to corrosion (worst-case observed during past inspections, not present this year).
- c. No posting of the structure is required.





5.4 Trestle BSB SI&A Forms

B-6 BSB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1012 Type: Regular Inspection

County/City:NorthamptonDate of Inspection:9/25/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost:41.38Location:Over Chesapeake BayLead Inspector:Jon Chapman, PEAdditional Inspector(s):Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance. Underwater Inspection performed 6/27 - 9/28/21.

CONDITION OF STRUCTURE: Good

- a. Light poles at BSB59, BSB61, BSB63, BSB67, BSB73, BSB91, BSB93 exhibit cracking or delamination/spalling at the base plate, light pole at BSB175 has a spall at the base plate with exposed reinforcement, and light poles at BSB151 and BSB161 exhibit minor spalling.
- b. Dented railing on Span BSB20, BSB21, BSB67, BSB70, BSB79, BSB80, BSB86, BSB89, BSB90, BSB95.
- c. Isolated hairline transverse cracking and spalling on curb. Exposed reinforcement noted on curb at BSB80.
 - Bent caps hairline vertical, longitudinal and map cracks located at isolated locations throughout spans.
- d. Exposed reinforcement chairs located on cap faces, underside of girder (typically near ends) and underside of deck throughout spans.
- e. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- f. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3"L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- g. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement.
- h. Steel beam span BSB1 and BSB202 repainted in 2013, structurally in good condition with scattered minor rusting of girders, diaphragms, and bearing connections in areas over armor stones.
- i. Large spalling or locations with exposed reinforcement or exposed strands at girder ends or in the lower portion of the continuity closure pours at 99 locations.
- j. Minor horizontal cracking, some with efflorescence, in intermediate concrete girder diaphragms.
- k. 2023 Hydrographic Survey indicates bay bottom was near ASD at seventeen (17) bents which includes BSB8 BSB10, BSB182 BSB190, BSB194, BSB195, BSB198 and BSB199. Based on these findings, 48 piles are identified as Condition State 3 for Scour.
- I. The underwater portion of the piles are in generally good condition as noted during the 2021 Underwater Inspection. Large repairs previously completed at Bent BSB90 Pile C and at Bent BSB199 Pile C remain in good condition. Minor cracks and spalls were noted on some underwater portions of piles, with 21% of bents rated 6 or lower.
- m. Roadway striping replaced in 2018 remains in good condition.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. Structure load rating controlled by the simple span steel structures at Span BSB1 and BSB202.
- 3. No posting of the structure is required.



Page: 2 of 2

B-6 BSB

VDOT – BRIDGE INSPECTION REPORT

RECOMMENDATIONS:

- 1. Monitor light poles that exhibit cracking or delamination/spalling near the baseplate and replace if vertically oriented steel reinforcement becomes exposed and degraded.
- 2. Repair/replace dented railing.
- 3. Repair paint system at scattered locations where overcoat is peeling.
- 4. Continue periodic hydrographic surveys in interim between underwater inspections.
- 5. Repair spalls noted this year along with those previously noted on girder bottom flanges.
- 6. Monitor Allowable Scour Depth at locations that are near bay bottom requirement. Implement scour remediation if bay bottom exceeds Allowable Scour Depth.
- 7. Repair portions of piles exhibiting minor spalls with exposed reinforcement.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





B-7 BSB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 3

Structure-ID: 1012 Type: Regular Inspection

County/City:NorthamptonDate of Inspection:9/25/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost:41.38Location:Over Chesapeake BayLead Inspector:Jon Chapman, PEAdditional Inspector(s):Ethan Stivers, Jon Krites

SPECIAL CONDITIONS OR REQUIREMENTS

1. Fracture Critical	_	5. Segmental _
2. Underwater	<u>X</u>	6. Pin & Hanger
3. Scour Critical	<u>X</u>	7. Fatigue Prone
4. Moveable	_	

36 TRAFFIC SAFETY FEATURES

1. Bridge Railing 1 3. Approach 1 2. Transition N 4. Approach Guardrail N

REMARKS: REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK GENERAL CONDITION RATING [7]

1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>
4. Median	<u>N</u>	9. Utilities	<u>G</u>
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>
		or Devices	

- a. Light poles at BSB59, BSB61, BSB63, BSB67, BSB73, BSB91, and BSB93 exhibit cracking or delamination/spalling at the base plate, light pole at BSB175 has a spall at the base plate with exposed reinforcement, and light poles at BSB151 and BSB161 exhibit minor spalling.
- b. Dented railing on Span BSB20, BSB21, BSB67, BSB70, BSB89, BSB80, BSB89, BSB90, BSB95.
- c. Isolated hairline transverse cracking and spalling on curb. Exposed reinforcement noted on curb at BSB80.
- d. Exposed reinforcement chairs located on underside of deck throughout spans.
- e. Minor hairline and map cracking scattered on the underside of the deck.
- f. Longitudinal reflective cracks in the overlay sealed, but a few scattered unsealed cracks were noted.
- g. Transverse cracks in bare concrete deck need sealing.
- h. Roadway striping replaced in 2018 remains in good condition.





B-7 BSB	VDOT – BRIDGE INSPE	VDOT – BRIDGE INSPECTION REPORT		
59 SUPERSTRUCTURE		GENERAL CONDITION RATII	NG [8]	
1. Bearing Devices	<u>G</u>	4. Trusses		
2. Stringers	<u>N</u>	A. General	<u>N</u>	
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>	
A. General	<u>G</u>	C. Bracing	<u>N</u>	
B. Diaphragms or	<u>G</u>	5. Paint	<u>F</u>	
Cross Frames		Year Painted	<u>2013</u>	
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>	

REMARKS:

- a. Steel beam span BSB1 and BSB202 repainted in 2013, structurally in good condition with scattered minor rusting of girders, diaphragms, and bearing connections in areas over armor stones.
- b. Large spalling or locations with exposed reinforcement or exposed strands at girder ends or in the lower portion of the continuity closure pours at 99 locations.
- c. Minor cracking and spalls with exposed reinforcement at isolated locations on a few girders.
- d. Exposed reinforcement chairs located on underside of girder (typically near ends) throughout spans.
- e. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- f. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- g. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING	[8]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	G G G N N S *	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	- - - - - - - - - - - - - - - - - - -	

- a. Repairs to large spalls with exposed spiral reinforcement on underwater portions of BSB90 Pile C and BSB199 Pile C remain in good condition (2021 UWI Report).
- b. Minor vertical hairline cracks, map cracks and spalls in scattered bent caps.
- c. Minor hairline cracking and minor spalling on some of the above water portions of the piles.





B-7 BSB VDOT – BRIDGE INSPECTION REPORT Page: 3 of 3

- d. Bent caps hairline vertical, longitudinal and map cracks located at isolated locations throughout spans.
- e. Exposed reinforcement chairs located on cap faces throughout spans.
- f. The underwater portion of the piles are in generally good condition as noted during the 2021 Underwater Inspection. Large repairs previously completed at Bent BSB90 Pile C and at Bent BSB199 Pile C remain in good condition. Minor cracks and spalls were noted on some underwater portions of piles, with 21% of bents rated 6 or lower

61 CHANNEL: CHANN	NEL/SLOPE PROTECTION	GENERAL CONDITIO	N RATING	[8]
 Channel Scour Embankment Erosion Drift 	<u>G</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/SlopeProtection	– <u>G</u>	
4. Vegetation	-	8. Adequacy of Opening	<u>G</u>	

REMARKS:

a. 2023 Hydrographic Survey indicates bay bottom was near ASD at seventeen (17) bents which includes BSB8 – BSB10, BSB182 – BSB190, BSB194, BSB195, BSB198 and BSB199. Based on these findings, 48 piles are identified as Condition State 3 for Scour.

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single	<u>N</u>		
(R12 – 5) – Semi	<u>N</u>		

- Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by the simple span steel structures at Span BSB1 and BSB202.
- c. No posting of the structure is required.



5.5 Trestle CNB SI&A Forms

B-6 CNB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID:1009Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:9/16 9/18 & 9/19/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 35.23 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance. Replacement of Asphalt Wearing Surface and Open Expansion Joint Header Material was completed in Spring 2020. Open Expansion Joint Steel Angle Headers were repaired or replaced as required as part of this effort.

CONDITION OF STRUCTURE: Generally Fair to Good

- Damaged railing in spans CNB149, CNB181, CNB185, CNB193, CNB201, CNB204, CNB207, CNB216, CNB224, CNB231, CNB233, CNB243 (25), CNB244, CNB245.
- Light poles at CNB108, CNB111, CNB120, CNB129, CNB153, CNB159, CNB162, CNB165, CNB186, CNB192, CNB195, CNB198, CNB222, CNB234, CNB237, CNB243, CNB246, and CNB257, CNB258 exhibited cracking or spalling at the baseplate, and the light pole at CNB131 has a loose anchor nut.
- 3. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at CNB227, CNB228.
- 4. Ground wire broken at railing connections or light pole connections at the following locations: CNB220 and CNB246.
- 5. Damaged expansion joint plates in cable tray and misaligned cable tray plate at Bent CNB124.
- Isolated spalls with exposed reinforcement on underside of deck / top flange in the following locations in FY2025: CNB199 (w. overhang/sidewalk), CNB199 G4, CNB200, CNB203 (w. overhang/sidewalk), CNB245 G7, CNB249 G4, CNB253 G1 (3), CNB260 G3, CNB262 G2, CNB263.
- Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: CNB197 G4 (2), CNB198 G4 (2), CNB198 G1, CNB198 G4, CNB198 G5 (2), CNB198 B6, CNB199 G2 (2), CNB199 G4 (3), CNB199 G6 (2), CNB200 G4 (2), CNB201 G5 (2), CNB203 G2, CNB203 G3, CNB206 G1 (2), CNB206 G4, CNB206 G5 (2), CNB206 G6, CNB206 G8 (2), CNB207 G1, CNB207 G2 (2), CNB211 G2, CNB211 G1, CNB211 G4, CNB211 G5, CNB211 G8, CNB212 G4 (2), CNB213 G2, CNB213 G6, CNB214 G4, CNB214 G5, CNB215 G7, CNB216 G5, CNB216 G4, CNB216 G6, CNB216 G8, CNB217 G4, CNB217 G5, CNB219 G2, CNB221 G2, CNB222 G3, CNB223 G4, CNB223 G5, CNB224 G1, CNB224 G5, CNB224 G8, CNB227 G4, CNB227 G5, CNB227 G2 (4), CNB227 G2, CNB227 G4, CNB228 G2, CNB228 G4, CNB228 G8, CNB230 G4, CNB230 G5, CNB231 G1, CNB231 G4, CNB232 G8, CNB233 G4, CNB233 G5 (2), CNB234 G4, CNB234 G7, CNB234 G3, CNB235 G4, CNB236 G5, CNB237 G4, CNB237 G5, CNB237 G6, CNB238 G3, CNB239 G1 (2), CNB239 G2, CNB239 G4, CNB241 G8, CNB242 G4 (2), CNB243 G2, CNB243 G1, CNB244 G1, CNB245 G6, CNB247 G6, CNB247 G8, CNB248 G1, CNB249 G2, CNB249 G4, CNB250 G1, CNB250 G3, CNB251 G1, CNB251 G4, CNB254 G1, CNB255 G2, CNB255 G4, CNB256 G1, CNB256 G2, CNB256 G3, CNB256 G5, CNB258 G1, CNB258 G4, CNB258 G6, CNB259 B1 (2), CNB259 G2, CNB259 G3 (2), CNB259 G4, CNB260 G1, CNB260 G3, CNB261 G1, CNB261 G2, CNB262 G4, CNB261 G5, CNB262 G2, CNB262 G7. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded.
- Scattered cracks and spalls on bottom flanges of prestressed girders. A few failed patches at older repair locations, including exposed strands near mid-span and exposed rebar at some bearings.
 Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: CNB197-B, CNB199-B, CNB201-A, CNB201-B, CNB203-B, CNB204-C, CNB208-B, CNB209-A, CNB210-A, CNB210-B, CNB210-C, CNB212-A,





Page: 2 of 2

B-6 CNB

VDOT – BRIDGE INSPECTION REPORT

CNB217-A, CNB218-C, CNB219-A, CNB219-B, CNB222-C, CNB224-B, CNB229-B, CNB230-A, CNB231-B, CNB231-C, CNB232-C, CNB234-B, CNB235-B, CNB249-A, CNB251-A, CNB255-C, CNB259-C, CNB261-A, CNB261-B.

- 9. New spall repairs on bottom flange of girders in good condition with minor shrinkage cracks noted.
- 10. A few failed patches at older repair locations, including exposed strands near mid-span and exposed rebar at some bearings.
- 11. Minor to moderate splitting on appr. 13% of the elastomeric bearings.
- 12. Hairline diagonal web cracking on approximately 26% of concrete girder ends inspected.
- 13. Map cracking at the ends of many of the bent caps.
- 14. Scattered minor to moderate cracking and spalling on a few of the piles, some of which are recommended for repair. Spall in grout at pile joint with rust staining and minor cracking with rust staining on CNB267 Pile C.
- 15. Per the 2023 Hydrographic survey bay bottom was below ASD at Bent CNB4 through CNB20, CNB61 through CNB94, and CNB278 thru CNB294 and CNB306 through CNB309 and near ASD at Bents CNB21 through CNB34, CNB60, CNB95 through CNB97 and CNB271 through CNB277; however, scour remediation was noted at all near or below ASD locations.
- 16. Condition State 3 (Poor) will be coded for all below and near ASD piles where a scour remediation has been completed and based on review of hydrographic survey includes 348 piles (roughly 35% of piles).
- 17. Underwater pile repairs previously completed at large spalls exposing spirals and post tensioning strands were at Pile C of Bent CNB59 and Pile A of Bent CNB154 remain in good condition.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. Structure load rating controlled by modular superstructure units with reduced capacity from having two-prestressing strands removed due to corrosion (worst-case observed during past inspections).
- 3. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Patch spalled grout at pile joint on CNB267 Pile C with epoxy.
- Repair/replace dented railing on Span CNB149, CNB181, CNB185, CNB193, CNB201, CNB204, CNB207, CNB216, CNB224, CNB233, CNB243 (25), CNB244, CNB245.
- Replace light pole at CNB108, CNB111, CNB120, CNB129, CNB153, CNB159, CNB162, CNB165, CNB186, CNB192, CNB195, CNB198, CNB222, CNB234, CNB237, CNB243, CNB246, and CNB257, CNB258
- 4. Repair piles that exhibit significant cracking or spalling.
- 5. Continue periodic hydrographic surveys in interim between underwater inspections scheduled every 5 years
- 6. Continue to patch spalls on prestressed concrete girders, giving priority to girders exhibiting two or more exposed strands per location.
- 7. Repair spalls in underside of deck with priority to those exposing multiple layers of steel.
- 8. Repair spalls in curbs with exposed reinforcement.
- 9. Reinstall/Repair ground wire that are broken at railing connections or light pole connections at the following locations: CNB220 and CNB246.

SIGNATURE OF INSPECTOR

1 | 1

Carron M. S. Turner

SIGNATURE OF REVIEWER





B-7 CNB **VDOT – BRIDGE INSPECTION REPORT** Page: 1 of 4 Structure-ID: 1009 Regular Inspection Type: Date of Inspection: 9/16 9/18 & 9/19/2024 County/City: Northampton Structure: Feature/Intersection: Chesapeake Bay (Co./Str.No) 13 Main Route: Facility/Carried: Milepost: Location: Over Chesapeake Bay 35.23 Additional Inspector(s): Ethan Stivers, Jon Krites Lead Inspector: Shannon Turner, PE SPECIAL CONDITIONS OR REQUIREMENTS 1. Fracture Critical 5. Segmental 2. Underwater <u>X</u> 6. Pin & Hanger 3. Scour Critical <u>X</u> 7. Fatigue Prone 4. Moveable **36 TRAFFIC SAFETY FEATURES** 1. Bridge Railing 1 3. Approach <u>1</u>

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

4. Approach Guardrail

1

58 DECK	58 DECK GENERAL CONDITION RATING		ON RATING	[7]	
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>		
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>		
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>		
4. Median	<u>N</u>	9. Utilities	<u>G</u>		
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>		
		or Devices			

REMARKS:

2. Transition

1

- a. Replacement of Asphalt Wearing Surface and Open Expansion Joint Header Material was completed in Spring 2020.
- b. Light poles at CNB108, CNB111, CNB120, CNB129, CNB153, CNB159, CNB162, CNB165, CNB186, CNB192, CNB195, CNB198, CNB222, CNB234, CNB237, CNB243, CNB246, and CNB257, CNB258 exhibited cracking or spalling at the baseplate, and the light pole at CNB131 has a loose anchor nut.
- c. Damaged railing in Spans CNB149, CNB181, CNB185, CNB193, CNB201, CNB204, CNB207, CNB216, CNB224, CNB231, CNB233, CNB243 (25), CNB244, CNB245.
- d. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at CNB227, CNB228.
- e. Ground wire broken at railing connections or light pole connections at the following locations: CNB220 and CNB246.
- f. Damaged expansion joint plates in cable tray and misaligned cable tray plate at Bent CNB124.
- g. Isolated spalls with exposed reinforcement on underside of deck / top flange in the following locations in FY2025: CNB199 (w. overhang/sidewalk), CNB199 G4, CNB200, CNB203 (w. overhang/sidewalk), CNB245 G7, CNB249 G4, CNB253 G1 (3), CNB260 G3, CNB262 G2, CNB263.
- h. Minor cracking, some exhibiting efflorescence on the underside of the deck in scattered spans.





B-7 CNB VDOT – BRIDGE INSPECTION REPORT Page: 2 of 4

59 SUPERSTRUCTURE		GENERAL CONDITION RATING		[7]
1. Bearing Devices	<u>E</u>	4. Trusses		
2. Stringers	<u>N</u>	A. General	<u>N</u>	
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>	
A. General	<u>F</u>	C. Bracing	<u>N</u>	
B. Diaphragms or	<u>G</u>	5. Paint	<u>F</u>	
Cross Frames		Year Painted	<u>N</u>	
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>	

REMARKS:

- a. Minor to moderate splitting of 13% of bearings pads inspected
- b. Some cracks and spalls on bottom flanges of some prestressed girders
- c. Deterioration or failure of some existing patches on concrete girders
- Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: CNB197 G4 (2), CNB198 G4 (2), CNB198 G1, CNB198 G4, CNB198 G5 (2), CNB198 B6, CNB199 G2 (2), CNB199 G4 (3), CNB199 G6 (2), CNB200 G4 (2), CNB201 G5 (2), CNB203 G2, CNB203 G3, CNB206 G1 (2), CNB206 G4, CNB206 G5 (2), CNB206 G6, CNB206 G8 (2), CNB207 G1, CNB207 G2 (2), CNB211 G2, CNB211 G1, CNB211 G4, CNB211 G5, CNB211 G8, CNB212 G4 (2), CNB213 G2, CNB213 G6, CNB214 G4, CNB214 G5, CNB215 G7, CNB216 G5, CNB216 G4, CNB216 G6, CNB216 G8, CNB217 G4, CNB217 G5, CNB219 G2, CNB221 G2, CNB222 G3, CNB223 G4, CNB223 G5, CNB224 G1, CNB224 G5, CNB224 G8, CNB227 G4, CNB227 G5, CNB227 G2 (4), CNB227 G2, CNB227 G4, CNB228 G2, CNB228 G4, CNB228 G8, CNB230 G4, CNB230 G5, CNB231 G1, CNB231 G4, CNB232 G8, CNB233 G4, CNB233 G5 (2), CNB234 G4, CNB234 G7, CNB234 G3, CNB235 G4, CNB236 G5, CNB237 G4, CNB237 G5, CNB237 G6, CNB238 G3, CNB239 G1 (2), CNB239 G2, CNB239 G4, CNB241 G8, CNB242 G4 (2), CNB243 G2, CNB243 G1, CNB244 G1, CNB245 G6, CNB247 G6, CNB247 G8, CNB248 G1, CNB249 G2, CNB249 G4, CNB250 G1, CNB250 G3, CNB251 G1, CNB251 G4, CNB254 G1, CNB255 G2, CNB255 G4, CNB256 G1, CNB256 G2, CNB256 G3, CNB256 G5, CNB258 G1, CNB258 G4, CNB258 G6, CNB259 B1 (2), CNB259 G2, CNB259 G3 (2), CNB259 G4, CNB260 G1, CNB260 G3, CNB261 G1, CNB261 G2, CNB262 G4, CNB261 G5, CNB262 G2, CNB262 G7. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded.
- e. Some spalls on bottom flanges of some prestressed girders, and some existing spall repairs on girders are failing or have failed. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and degraded.
- f. New spall repairs on bottom flange of girders in good condition with minor shrinkage cracks noted.
- g. A few failed patches at older repair locations, including exposed strands near mid-span and exposed rebar at some bearings.
- h. Minor to moderate splitting on appr. 13% of the elastomeric bearings.
- Hairline diagonal web cracking on approximately 26% of concrete girder ends inspected.

60 SUBSTRUCTURE [7] **GENERAL CONDITION RATING** 2. Pier/Bent 1. Abutments A. Wings A. Caps N B. Backwall G B. Piles C. Column, Stem, Wall C. Bearing Seats G D. Piles D. Breastwall N E. Weepholes Ν E. Bracing





B-7 CNB	VDOT – BRIDGE INSPECTION	ON REPORT	Page: 3 of 4
F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	<u>G</u> * <u>G</u> <u>G</u>	F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles	- - <u>E</u> <u>G</u> <u>E</u>
		D. Bracing	<u>N</u>

REMARKS:

- a. Condition rating controlled by scour, see 61 Channel: Channel Slope/Protection.
- b. Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: CNB197-B, CNB199-B, CNB201-A, CNB201-B, CNB203-B, CNB204-C, CNB208-B, CNB209-A, CNB210-A, CNB210-B, CNB210-C, CNB212-A, CNB217-A, CNB218-C, CNB219-A, CNB219-B, CNB222-C, CNB224-B, CNB229-B, CNB230-A, CNB231-B, CNB231-C, CNB232-C, CNB234-B, CNB235-B, CNB249-A, CNB251-A, CNB255-C, CNB259-C, CNB261-A, CNB261-B.
- c. Minor to moderate map cracking at ends of some bent cap beams.
- d. Scattered minor to moderate cracking and spalling on a few of the piles, some of which are recommended for repair. Spall in grout at pile joint with rust staining and minor cracking with rust staining on CNB267 Pile C.
- e. Large underwater spalls exposing spirals and post tensioning strands at Pile C of Bent CNB59 and Pile A of Bent CNB154, repaired in 2012 remain in Good condition.
- f. Bent CNB20 Pile B and Bent CNB46 Pile C retrofitted with Cathodic Protection jackets and remain in good condition.
- g. Defects noted during the Underwater Inspection include wide, narrow and hairline cracks, large spalls and moderate scaling. See Underwater Inspection Forms for additional details.

61 CHANNEL: CHANN	IEL/SLOPE PROTECTION	GENERAL CONDITIO	N RATING	[7]
 Channel Scour Embankment Erosion Drift Vegetation 	<u>E</u> - -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope Protection8. Adequacy of Opening	– <u>G</u> <u>E</u>	

- a. Per the 2023 Hydrographic survey bay bottom was below ASD at Bent CNB4 through CNB20, CNB61 through CNB94, and CNB278 thru CNB294 and CNB306 through CNB309 and near ASD at Bents CNB21 through CNB34, CNB60, CNB95 through CNB97 and CNB271 through CNB277; however, scour remediation was noted at all near or below ASD locations.
- b. Condition State 3 (Poor) will be coded for all below and near ASD piles where a scour remediation has been completed and based on review of hydrographic survey includes 348 piles (roughly 35% of piles).
- c. Scour remediation is present at approximately 50% of the pile bents.





B-7 CNB VDOT – BRIDGE INSPECTION REPORT Page: 4 of 4

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single	<u>N</u>		
(R12 – 5) – Semi	<u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by modular superstructure units with reduced capacity from having two-prestressing strands removed due to corrosion (worst-case observed during past inspections, not present this year).
- c. No posting of the structure is required.





5.6 Trestle CSB SI&A Forms

B-6 CSB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1014 Type: Regular Inspection

County/City:NorthamptonDate of Inspection:9/25/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 35.28 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Good

- 1. Light poles at CSB55, 67, 73, 85, 99, & 101 exhibited cracking or spalling at the baseplate. Light poles at CSB85, 91, 93, & 103 have one anchor bolt that does not extend past the nut when the nut is fully engaged. The light pole at CSB93 has one bolt (same anchor bolt that does not extend past the nut) that is near the outer edge of the baseplate.
- 2. Portions of guardrail on Spans CSB26, CSB64, CSB75, CSB79, CSB82, CSB87, CSB89, CSB90, CSB93, CSB96, CSB97 has minor impact damage on one section of railing.
- 3. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at CSB47, CSB67, CSB71, CSB90, CSB92, CSB94, CSB97
- 4. Exposed reinforcement chairs located on cap faces, underside of girder (typically near ends) and underside of deck throughout spans.
- 5. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- 6. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- 7. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement.
- 8. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: CSB94 G5, CSB94 G3, CSB94 G4, CSB91 G4, CSB90 G1, CSB84 G3, CSB84 G4, CSB82 G5, CSB81 G3, CSB79 G6, CSB77 G3, CSB76 G1, CSB48 G1, CSB46 G4. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and deteriorated.
- Minor hairline cracking and minor map cracking on some of the above-water portions of piles and bent caps.Some minor spalls on a few piles.
- 10. Minor spalling with no exposed reinforcement (except at span 187), and minor cracking and efflorescence at scattered locations in underside of deck.
- 11. Large spalling or locations with exposed reinforcement or exposed strands at girder ends or in the lower portion of the continuity closure pours at 126 locations.
- 12. Minor horizontal cracking, some with efflorescence, in intermediate concrete girder diaphragms.
- 13. Per the 2023 Hydrographic survey bay bottom was below ASD at Bent CSB2, CSB6, CSB7, CBS63 through CSB71 and near ASD at Bents CSB5, CSB8 through CSB12, and CSB228 through CSB235. No scour remediation is noted on this trestle.
- 14. Condition State 4 (Severe) will be coded for all below ASD piles which includes approx. 36 piles and Condition State 3 (Poor) will be coded for all near ASD piles which includes approx. 39 piles.

1 | 1





B-6 CSB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

REVISED STRESS ANALYSIS:

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by the simple span steel structures at Span CSB1.
- c. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Continue periodic hydrographic surveys in interim between underwater inspections.
- 2. Install scour remediation at Bents CSB2, CSB6, CSB7, CBS63 through CSB71 due to hydrographic survey identifying these locations as below the Allowable Scour Depth.
- 3. Replace top protective coating on steel girders at locations where coating has bubbled and exhibits freckled rust.
- 4. Monitor light poles exhibit cracking or delamination/spalling near the baseplate and replace if vertically oriented steel reinforcement becomes exposed and degraded.
- 5. Replace corroded and missing conduit brackets on bent cap faces with stainless steel.
- 6. Repair bent or damaged railing as required.
- 7. Repair curb with exposed reinforcement or failed repairs.
- 8. Repair detached conduit at CSB25 north face and replace broken conduit near light pole at CSB57.
- 9. Continue to patch spalls on prestressed concrete girders, giving priority to girders exhibiting two or more exposed strands per location.

SIGNATURE OF INSPECTOR

Harron M. S. Turner

SIGNATURE OF REVIEWER





Page: 1 of 3 B-7 CSB **VDOT – BRIDGE INSPECTION REPORT** Structure-ID: 1014 Regular Inspection Type: Date of Inspection: County/City: Northampton 9/25/2024 Structure: (Co./Str.No) Feature/Intersection: Chesapeake Bay 13 Main Route: Facility/Carried: Milepost: 35.28 Location: Over Chesapeake Bay Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites Lead Inspector: SPECIAL CONDITIONS OR REQUIREMENTS 1. Fracture Critical 5. Segmental <u>X</u> 2. Underwater 6. Pin & Hanger 3. Scour Critical <u>X</u> 7. Fatigue Prone 4. Moveable **36 TRAFFIC SAFETY FEATURES** 1. Bridge Railing 3. Approach 1 <u>1</u>

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

4. Approach Guardrail

1

58 DECK		GENERAL CONDITION RATING		[8]
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>F</u>	
		or Devices		

REMARKS:

2. Transition

1

- a. Replacement of Asphalt Wearing Surface, Open Expansion Joint Header Material was completed in Fall 2019
- b. Light poles at CSB55, 67, 73, 85, 99, & 101 exhibited cracking or spalling at the baseplate. Light poles at CSB85, 91, 93, & 103 have one anchor bolt that does not extend past the nut when the nut is fully engaged. The light pole at CSB93 has one bolt (same anchor bolt that does not extend past the nut) that is near the outer edge of the baseplate.
- c. Portions of guardrail on Spans CSB26, CSB64, CSB75, CSB79, CSB82, CSB87, CSB89, CSB90, CSB93, CSB96, CSB97 has minor impact damage on one section of railing.
- d. Scattered minor transverse cracking and spalling or failed repairs in curbs typical. Exposed reinforcement noted on curb at CSB47, CSB67, CSB91, CSB90, CSB92, CSB94, CSB97.
- e. Exposed reinforcement chairs located on underside of deck throughout spans.





B-7 CSB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 3

f. Adjust bridge railing baseplate on underside of deck on Spans CSB 98 near midspan on the East side and 170 near midspan on the West side such that both bolts are connected instead of only one as is the current condition.

59 SUPERSTRUCTURE		GENERAL CONDITION RATI	NG [7]
1. Bearing Devices	<u>G</u>	4. Trusses	
2. Stringers	<u>N</u>	A. General	<u>N</u>
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>
A. General	<u>G</u>	C. Bracing	<u>N</u>
B. Diaphragms or	<u>G</u>	5. Paint	<u>N</u>
Cross Frames		Year Painted	<u>N</u>
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>

- a. Minor girder spalling with exposed reinforcement at 81 locations, 75 locations are at girder ends or in the lower portion of the continuity closure pours.
- b. Minor spalling with no exposed reinforcement (except at span 187), and minor cracking and efflorescence at scattered locations in underside of deck.
- c. Exposed reinforcement chairs located on underside of girder (typically near ends) throughout spans.
- d. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x ½" deep with rust stain.
- e. Beam ends delamination/spalls on end corners of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- f. Closure pours at beam ends on continuous spans have isolated spalls up to full height x full length x full depth with exposed reinforcement.
- g. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: CSB94 G5, CSB94 G3, CSB94 G4, CSB91 G4, CSB90 G1, CSB84 G3, CSB84 G4, CSB82 G5, CSB81 G3, CSB79 G6, CSB77 G3, CSB76 G1, CSB48 G1, CSB46 G4. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and deteriorated.
- h. Minor cracking and spalling in concrete girder diaphragms.
- i. Freckled rust and coating failure typical at the flanges and connections from near mid-span to Portal Island
 #4 due to being in the splash zone of breaking waves.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [7]	
1. Abutments		2. Pier/Bent		
A. Wings	<u>G</u>	A. Caps	_	
B. Backwall	<u>G</u>	B. Piles	_	
C. Bearing Seats	<u>G</u>	C. Column, Stem, Wall	_	
D. Breastwall	<u>N</u>	D. Piles	_	
E. Weepholes	<u>N</u>	E. Bracing	_	
F. Footing	<u>G</u>	F. Erosion/Scour	_	
G. Piles	*	G. Settlement	_	
H. Erosion/Scour	<u>G</u>	3. Pile Bent		
I. Settlement	<u>G</u>	A. Caps	<u>G</u>	





B-7	VDOT – BRIDGE INSPECTION RE	PORT	Page: 3 of 3
*Not Visible C. Piles	c	B. Bearing Seats	<u>G</u>
C. Files	<u>G</u>	D. Bracing	<u>N</u>

REMARKS:

- a. Underwater Inspection performed 6/2022 8/2022. See FY2023 Underwater Inspection Forms for location of underwater defects and Element Level Inspection Forms for condition ratings of substructure units
- b. Minor hairline cracking and minor map cracking on some of the above-water portions of piles and bent caps, a few piles with minor efflorescence
- c. Minor shallow spalls noted on a few piles, some with exposed reinforcement.

61 CHANNEL: CHANNEL/SLOPE PROTECTION		GENERAL CONDITION RATING		[7]
Channel Scour Embankment Erosion	<u>E</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope	- - <u>G</u>	
3. Drift4. Vegetation	- -	Protection 8. Adequacy of Opening	<u>F</u>	

REMARKS:

- 1. Per the 2023 Hydrographic survey bay bottom was below ASD at Bent CSB2, CSB6, CSB7, CBS63 through CSB71 and near ASD at Bents CSB5, CSB8 through CSB12, and CSB228 through CSB235. No scour remediation is noted on this trestle.
- 2. Condition State 4 (Severe) will be coded for all below ASD piles which includes approx. 36 piles and Condition State 3 (Poor) will be coded for all near ASD piles which includes approx. 39 piles.

66 POSTED LOADING

1. Posted Loading -	2. Legibility	<u>N</u>
(R12 -1) <u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single <u>N</u>		
(R12 – 5) – Semi <u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by the simple span steel structures at Span CSB1.
- c. No posting of the structure is required.





5.7 NCB-NB SI&A Forms

B-6 NCB-NB VDOT - BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1006 Type: Regular & Fracture Critical

Inspection

County/City: Northampton Date of Inspection: 9/18-9/22/23, 9/25/23

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 34.51 Location: Over Chesapeake Bay

Lead Inspector: Todd Eckhart Additional Inspector(s): Amit Thakkar

WORK DONE: Routine Maintenance. Underwater Inspection performed 6/21 - 9/27/2021

CONDITION OF STRUCTURE: Generally Good

- 1. Steel bridge components received overcoat painting in 2012/2013.
- 2. Concrete deck has scattered transverse hairline cracks.
- 3. Isolated scattered areas of peeling overcoat paint system.
- 4. Existing cracks in steel floor beam webs at stiffener locations over the girders.
- 5. Section loss at random floor beam web stiffeners over the longitudinal girders.
- 6. Scattered areas of minor rust.
- 7. Deteriorated bolts and rivets replaced.
- 8. Pack rust and crevice corrosion are mostly sealed between multiple cover plates at isolated areas along top and bottom girder flanges and at corners of lower chord in Span 9.
- 9. Minor isolated rust in scattered upper and lower truss joints.
- 10. Miscellaneous minor spalls and cracks in the piers.
- 11. Minor cracking and light to moderate scaling in splash zone on some piers.
- 12. For the underwater portion of the piers, see the 2021 UWI Report.
- 13. Scouring of the bay bottom appears stable based on the latest hydrographic survey information provided by the District.
- 14. Bay bottom at each pier is typically protected by a riprap blanket.
- 15. Fender system at Piers 9 and 10 was replaced in 2013 and noted to be in good condition.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. Structure load rating controlled by Unit NCBA1 4-Span Steel Approach Girders, NCBA2 Steel Floorbeams (on Approach), and NCBT4 Truss Gusset Plates.
- 3. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Annually clean debris from interior of lower chords at panel points.
- 2. Seal transverse cracks in the concrete deck.
- 3. Monitor coating placed on interior of lower chords at Panel Points LO and LO'.
- 4. Monitor rusting welds at some hanger connections for lateral bracing.
- 5. Monitor floor beam web cracks.
- 6. Repair floor beam web stiffeners with section loss.
- 7. Monitor scattered minor to moderate corrosion of bolted girder splice connections.
- 8. Monitor section loss on stringer and floor beam details at L3 and L3' in Span 9. (Note: This work has been awarded to Cekra and is expected to start in Spring 2023).
- 9. Monitor any pack rust and crevice corrosion between top and bottom flange cover plates and at corners of lower chord truss members.





B-6 NCB-NB

VDOT - BRIDGE INSPECTION REPORT

Page: 2 of 2

- 10. Monitor minor rust inside scattered upper and lower chord truss joints.
- 11. Seal minor cracks in all pier tops extending from bearing grout pads.
- 12. Monitor stringer-to-floor beam connection for pack rust development and corrosion of connection hardware.
- 13. Seal cracks in splash zone at Piers 1, 2, 3, 7, 8, 11, 12, 13 and 14.
- 14. Repair paint system at scattered location where overcoat is peeling.
- 15. Repair areas of section loss on truss diagonals and verticals.

N/A

SIGNATURE OF INSPECTOR

*Inspection was not performed in 2024 – notes provided within the B-6 and B-7 forms were provided by previous consultant and placed in this document for informational purposes only.

SIGNATURE OF REVIEWER





B-7 NCB-NB VDOT - BRIDGE INSPECTION REPORT Page: 1 of 3

Structure-ID:1006Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:9/18-9/22/23, 9/25/23

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 34.51 Location: Over Chesapeake Bay

Lead Inspector: Todd Eckhart **Additional Inspector(s):** Amit Thakkar

SPECIAL CONDITIONS OR REQUIREMENTS

1. Fracture Critical X 5. Segmental _ 2. Underwater 6. Pin & Hanger _ 3. Scour Critical X 7. Fatigue Prone _ 4. Moveable

36 TRAFFIC SAFETY FEATURES

1. Bridge Railing 1 2. Transition N 3. Approach 1 4. Approach Guardrail N

REMARKS:

58 DECK	GENERAL CONDITION RATING	[8]

 Wearing Surface 	<u>N</u>	6. Railing	<u>G</u>
2. Deck - Structural	<u>G</u>	7. Drains	<u>N</u>
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>
4. Median	<u>N</u>	9. Utilities	<u>G</u>
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>
		or Devices	

- a. Deck replaced in 1998 as part of the Parallel Crossing Project
- b. Scattered transverse hairline cracks, which require sealing
- c. 2005 repair adjacent to joint at Panel Point 3 in Span 9 remains sound
- d. Replace loose guardrail nut at Span 3 Floor beam 10 east side





B-7 NCB-NB	VDOT - BRIDGE INSPECTION REPORT	Page: 2 of 3	
59 SUPERSTRUCTURE	GENERAL CONDITION RATING	[7]	

1. Bearing Devices	<u>G</u>	4. Trusses	
2. Stringers	<u>G</u>	A. General	<u>F</u>
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>G</u>
A. General	<u>F</u>	C. Bracing	<u>G</u>
B. Diaphragms or	<u>G</u>	5. Paint	<u>G</u>
Cross Frames		Year Painted	<u>2013</u>
C. Bracing	<u>F</u>	6. Machinery (Moveable Span)	<u>N</u>

REMARKS:

- a. Steel bridge components received overcoat painting in 2012/2013
- b. Newer repaired lateral bracing hanger connections in good condition, a few others with scattered rust
- c. Pack rust and crevice corrosion mostly sealed between multiple cover plates at isolated areas along top and bottom girder flanges and at corners of lower chord in Span 9, but some bleed-thru emerging
- d. Deteriorated bolts and rivets replaced, some scattered rust on those not replaced
- e. Existing web cracks in floor beams over girders
- f. Minor isolated rust in scattered upper and lower truss joints
- g. Isolated areas of scattered minor rust on various members
- h. Section loss at some floor beam stiffeners over the longitudinal girders. Ongoing repairs to floor beam stiffeners as of FY2024 inspection.
- i. Scattered section loss on floor beam, stringer seats and associated details at L3 and L3' in truss span. Previous repairs are in good condition.
- j. Isolated section loss on truss diagonals and verticals.

60 SUBSTRUCTURE GENERAL CONDITION RATING [7]

1. Abutments		2. Pier/Bent		
A. Wings	<u>N</u>	A. Caps	<u>G</u>	
B. Backwall	<u>N</u>	B. Piles	_	
C. Bearing Seats	<u>N</u>	C. Column, Stem, Wall	<u>G</u>	
D. Breastwall	<u>N</u>	D. Piles	*	
E. Weepholes	<u>N</u>	E. Bracing	<u>G</u>	
F. Footing	<u>N</u>	F. Erosion/Scour	<u>F</u>	
G. Piles	<u>N</u>	G. Settlement	<u>G</u>	
H. Erosion/Scour	<u>N</u>	3. Pile Bent		
I. Settlement	<u>N</u>	A. Caps <u>N</u>		
*Not Visible		B. Bearing Seats <u>N</u>		
		C. Piles	<u>N</u>	
		D. Bracing	<u>N</u>	

- a. Miscellaneous minor spalls and cracks in the piers
- b. Seal cracks in splash zones at Piers 1, 2, 3, 7, 8, 11, 12, 13 and 14. with carbon fiber sheets
- c. Repair cracks in tops of pier columns
- d. For the underwater portions of the piers, see the 2021 Underwater Inspection Report





B-7 NCB-NB VDOT - BRIDGE INSPECTION REPORT Page: 3 of 3

61 CHANNEL: CHAN	NEL/SLOPE PROTECTION	GENERAL CONDITION	ON RATING	[8]
1. Channel Scour	<u>G</u>	5. Fender System	<u>F</u>	
2. Embankment	_	Spur Dikes/Jetties	_	
Erosion		7. Rip Rap/Slope	<u>G</u>	
3. Drift	_	Protection		
4. Vegetation	_	8. Adequacy of	<u>F</u>	
		Opening		

REMARKS:

- a. Scouring of the bay bottom appears stable based on the latest hydrographic survey information provided by the District (Fall 2022) and 2021 Underwater Inspection
- b. Bay bottom at each pier is typically protected by a riprap blanket
- c. Fender systems at Piers 9 and 10 replaced in 2013 and in good condition

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1) <u>N</u>		3. Visibility	<u>N</u>
(R12 - 5) - Single	<u>N</u>		
(R12 - 5) - Semi	<u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by Unit NCBA1 4-Span Steel Approach Girders, NCBA2 Steel Floorbeams (on Approach), and NCBT4 Truss Gusset Plates.
- c. No posting of the structure is required.





5.8 NCB-SB SI&A Forms

B-6 NCB-SB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1015 Type: Regular Inspection

County/City: Northampton Date of Inspection: 9/20/2024 & 9/21/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 34.69 Location: Over Chesapeake Bay
Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance. Replacement of Steel Sliding Plate Expansion Joints with Steel Tooth Expansion Joints completed in Fall 2020. Damaged Pier 9 Fender System repairs completed in July 2021.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Generally Good

- 1. Steel Tooth Expansion Joint at Pier 1 opening was off by about 1/4" from the east side (2 1/2") to the west side (2 1/4"). The weather was cloudy at the time of measurement with an ambient air temperature around 70°F. The opening dimension for these conditions should have been 2 1/16" according to construction documents.
- 2. Steel Tooth Expansion Joint at Pier 15 opening was off by about 1/4" from the east side (2 13/16") to the west side (1 7/8"). The weather was cloudy at the time of measurement (9/14/2020 at 1:12pm) with an ambient air temperature around 77°F with the steel girders and concrete girders measuring at 79°F and 80°F, respectively. The opening dimension for these conditions should have been 2 1/16" according to construction documents.
- Bearing pins not fully seated at the following locations: NCB-SB1 G1 & G2, NCB-SB4 G1, NCB-SB G4, NCB-SB G1-G3.
- 4. Rust around perimeter of some bearing base plates.
- 5. Isolated minor rust spots on cross frames, diaphragms, and lower lateral bracing members.
- 6. Minor to moderate rust on scattered nuts and bolts of longitudinal girder splices and of cross frame, diaphragm and lower lateral bracing connections with pack rust located between connections. Isolated nuts have heavy rust with up to 1/8" section loss and/or flowering.
- 7. Scattered minor rust on ladder systems at Piers NCB-SB9 and NCB-SB10. Surface rust and rust scale with up to 1/16" section loss to anchor bolts at isolated locations throughout catwalk in Pier 10 and up to 90% section loss to anchor bolt at level 4 ladder.
- 8. Minor cracking on some of the pier caps. Repairs to pile cap (footing) completed in fall of 2015 to NCB-SB9 appear satisfactory with minor cracking of the repair material.
- 9. Minor transverse cracks scattered in concrete deck, mostly hairline, but some noted up to 1/16" wide.
- 10. Failing patch in bridge deck surface in Span NCB-SB8.
- 11. Minor scale noted on top of deck and curb throughout.
- 12. Minor rust on steel fender with section loss noted of up to 1/16" loss on panel chairs and anchorage.
- 13. 2023 Hydrographic Survey and the 2019 Underwater Inspection indicates bay bottom was below ASD at Pier NCB-SB13. Bay bottom is stable due to scour blanket in place as indicated in the 2009, 2014, and 2019 Underwater Inspections.
- 14. No significant defects noted on the underwater portions of the piles in the FY2020 Underwater Inspection Forms.





B-6 NCB-SB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. No posting of the structure is required.

RECOMMENDATIONS:

- Monitor Steel Tooth Expansion Joints on extremely cold days when temperatures are expected to remain near or below 25°F for several hours to observe if decreased tooth (finger) overlap has created an unsafe condition for the travelling public or snowplow equipment. At 25°F the tooth overlap is anticipated to be below the 1.5" requirement specified in the AASHTO LRFD Bridge Design Specifications.
- 2. Clean and recoat steel girders, cross frames and diaphragms at locations of pack rust, where zinc primer is exposed, or coating has bubbled and exhibits freckled rust.
- 3. Replace anchor bolts throughout catwalk and ladder system as required with hardware exhibiting over 50% section loss. Clean and recoat ladder and catwalk.
- 4. Update the 2023 Hydrographic Survey to indicate scour remediation (scour blanket) has been installed at below ASD Pier NCB-SB13 and verify if same remediation has been installed around near ASD piers.

SIGNATURE OF INSPECTOR

Karron M. S. Turner

SIGNATURE OF REVIEWER





B-7 NCB-SB	VDOT – BRIDGE INSPECTION REPORT	Page: 1 of 3	

Structure-ID:1015Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:9/20/24 & 9/21/24Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost:34.69Location:Over Chesapeake BayLead Inspector:Shannon Turner, PEAdditional Inspector(s):Ethan Stivers, Jon Krites

SPECIAL CONDITIONS OR REQUIREMENTS

1. Fracture Critical	_	5. Segmental	_	
2. Underwater	<u>X</u>	6. Pin & Hanger	_	
3. Scour Critical	<u>X</u>	7. Fatigue Prone	_	
4. Moveable	_			

36 TRAFFIC SAFETY FEATURES

1. Bridge Railing 1 2. Transition N 3. Approach 1 4. Approach Guardrail N

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK		GENERAL CONDITION RATING		[7]	
1. Wearing Surface	<u>N</u>	6. Railing	<u>G</u>		
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>		
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>		
4. Median	<u>N</u>	9. Utilities	<u>G</u>		
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>		
		or Devices			

- a. Monitor Steel Tooth Expansion Joints on extremely cold days (temperatures approaching 25°F) to observe if decreased tooth (finger) overlap has created an unsafe condition for the travelling public or snowplow equipment. At 25°F the tooth overlap is anticipated to be below the 1.5" requirement specified in the AASHTO LRFD Bridge Design Specifications.
- b. Minor transverse hairline cracks in deck surface, with a few cracks up to 1/8".
- c. Minor scale noted on top of deck and curb throughout.
- d. Curb and railing are generally in good condition
- e. Minor failing patch in deck surface of NCB-SB8





B-7 NCB-SB	VDOT – BRII	OT – BRIDGE INSPECTION REPORT		Page: 2 of 3	
59 SUPERSTRUCTURE		GENERAL CONDITION RATI	NG	[7]	
1. Bearing Devices	<u>G</u>	4. Trusses			
2. Stringers	_	A. General	_		
3. Girders, Beams, or Sl	ab Spans	B. Portals	_		
A. General	<u>E</u>	C. Bracing	_		
B. Diaphragms or	<u>G</u>	5. Paint	<u>G</u>		
Cross Frames		Year Painted	<u>2014</u>		
C. Bracing	<u>G</u>	6. Machinery (Moveable Span)	_		

REMARKS:

- a. Bearing pins not fully seated at the following locations: NCB-SB1 G1 & G2, NCB-SB4 G1, NCB-SB G4, NCB-SB G1-G3.
- b. Rust around perimeter of some bearing base plates.
- c. Isolated minor rust spots on cross frames, diaphragms, and lower lateral bracing members with pack rust located with members up to ¼" thick.
- d. Minor to moderate rust on scattered nuts and bolts of longitudinal girder splices and of cross frame, diaphragm and lower lateral bracing connections with pack rust located between connections. Isolated nuts have heavy rust with up to 1/4" section loss and/or flowering.
- e. NCB-SB1 G1, Bay 1: left bracket at G1, exterior side, 2" tear distorted 1" high.
- f. NCB-SB9 G2, Beam end distorted 1/2" along centerline to cross brace.
- g. Lubricate bearings as required
- h. Replace top protective coating on steel girders at locations where zinc primer is exposed, or coating has bubbled and exhibits freckled rust
- Span 4 Bay 2 Cross Frame 4 Intermediate Cross frame Lower Member Distorted with surface rust, no action required

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [7]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	N N N N N N N N N N N N N N N N N N N	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	G - G - - - - -

- a. Minor cracks in some columns and pier caps.
- b. Minor spalls on pier cap / bearing seats at isolated locations.
- c. Minor cracks in the backwall of pier caps at NCB-SB8 and NCB-SB11





B-7 NCB-SB VDOT - BRIDGE INSPECTION REPORT Page: 3 of 3

- d. The 2019 Underwater Inspection revealed minor structural defects.
- e. Scattered minor rust on ladder systems at Piers NCB-SB9 and NCB-SB10. Surface rust and rust scale with up to 1/16" section loss to anchor bolts at isolated locations throughout catwalk in Pier 10 and up to 90% section loss to anchor bolt at level 4 ladder.

61 CHANNEL: CHANNEL/SLOPE PROTECTION		GENERAL CONDITION RATING		[7]
1. Channel Scour	<u>E</u>	5. Fender System	<u>G</u>	
2. Embankment	_	Spur Dikes/Jetties	_	
Erosion		7. Rip Rap/Slope	_	
3. Drift	_	Protection		
4. Vegetation	_	8. Adequacy of	<u>E</u>	
-		Opening		

REMARKS:

- a. 2023 Hydrographic Survey indicates bay bottom was below ASD at Pier NCB-SB13 and near ASD at Pier NCB-SB10 and NCB-SB11. Bay bottom is stable due to scour blanket in place as indicated in the 2009, 2014, and 2019 Underwater Inspections, and based on current Hydro Survey data provided.
- b. Condition State 3 (Poor) will be coded for all below ASD piles where a scour remediation has been completed and for all near ASD piles.
- c. Fender System Minor rust on steel fender with section loss noted of up to 1/16" loss on panel chairs and anchorage. Southwest panel chains broken/not connected at 3 locations.

66 POSTED LOADING

1. Posted Loading -	2. Legibility	<u>N</u>
(R12 -1) <u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single <u>N</u>		
(R12 – 5) – Semi N		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. No posting of the structure is required.



5.9 Trestle DNB SI&A Forms

B-6 DNB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID:1008Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:11/26/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 34.25 Location: Over Chesapeake Bay
Lead Inspector: Jon Chapman, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance. Underwater Inspection performed 8/3/21.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Generally Good

- 1. The 2021 Underwater Inspection Report revealed only light to moderate scaling and no other apparent defects.
- 2. Bridge rail has impact damage in the following spans: DNB5 (1LF), DNB7 (6LF) with torn guide hole for rail, DNB14 (1LF).
- 3. Isolated spalls on curb facing up to 2' long x full height x up to 1" deep. Isolated vertical cracks on curb face up to 1/16" wide.
- 4. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: DNB2 G5, DNB4 G2, DNB5 G3, DNB7 G5, DNB7 G4, DNB8 G1and DNB12 G3. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and deteriorated
- 5. Some spalls on bottom flanges of some prestressed girders and some deteriorating patches.
- 6. Girder spall repairs with new method are in good condition.
- 7. Hairline diagonal web cracking on the concrete girders noted at scattered locations.
- 8. Minor to moderate defects on several of the elastomeric bearings.
- Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: DNB4-C, DNB6-B, DNB7-C and DNB8-C.
- 10. Rocker bearings in different degree of expansion/contraction (typical), bearings have corrosion on up to 30% of the surface area and up to 5% section loss
- 11. West wingwall undermined up to 1' wide x 6" high x up to 4' under wingwall.
- 12. Roadway striping replaced in 2018.
- 13. Bay bottom depths noted in the 2021 Underwater Inspection Report are well above the acceptable scour depth.

REVISED STRESS ANALYSIS:

- Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- Structure load rating controlled by modular superstructure units with reduced capacity from having twoprestressing strands removed due to corrosion (worst-case observed during inspection).
- 3. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Continue to patch spalls on prestressed concrete girders, giving priority to girders exhibiting two or more exposed strands per location.
- 2. Patch spalls on cylinder piles.





B-6 DNB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

- 3. Fill in undermining of west wingwall.
- 4. Future inspections and hydrographic surveys should be monitored for active scouring.
- 5. Clean and paint steel superstructure adjacent to NCB-N.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





B-7 DNB	VDOT – BRIDGE INSP	ECTION REPORT	Page: 1 of 3
Structure-ID: County/City: Structure: Main Route: Milepost: Lead Inspector:	1008 Northampton(Co./Str.No) 13 34.25 Jon Chapman, PE	Type: Date of Inspection: Feature/Intersection: Facility/Carried: Location: Additional Inspector(s):	Regular Inspection 11/26/2024 Chesapeake Bay Over Chesapeake Bay Ethan Stivers, Jon Krites
SPECIAL CONDITI	ONS OR REQUIREMENTS		
 Fracture Critica Underwater Scour Critical Moveable 	l <u>X</u> <u>X</u> <u>X</u> –	5. Segmental 6. Pin & Hanger 7. Fatigue Prone	- - -
36 TRAFFIC SAFE	TY FEATURES		
1. Bridge Railing 2. Transition	1 1	3. Approach 4. Approach Guardrail	1 1

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK	B DECK GENERAL CONDITION RATING		[7]	
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

- a. Hairline cracks with efflorescence in the deck overhangs of several spans inspected.
- b. Isolated spalls on underside of deck, exposed chairs also noted throughout on underside of deck.
- c. Widespread map cracking (hairline to narrow) of concrete deck surface in variety of spans.
- d. Bridge rail has impact damage in the following spans: DNB5 (1LF), DNB7 (6LF) with torn guide hole for rail, DNB14 (1LF).
- e. Isolated spalls on curb facing up to 2' long x full height x up to 1" deep. Isolated vertical cracks on curb face up to 1/16" wide.
- f. Portions of railing in Spans 9, 10 and 17 have been replaced in the past.





ige: 2	of	f		3	3
ĺ	ge: 2	ge: 2 o	ge: 2 of	ge: 2 of	ge: 2 of :

59 SUPERSTRUCTURE		GENERAL CONDITION RAT	GENERAL CONDITION RATING	
1. Bearing Devices	<u>E</u>	4. Trusses		
2. Stringers	<u>N</u>	A. General	<u>N</u>	
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>	
A. General	<u>F</u>	C. Bracing	<u>N</u>	
B. Diaphragms or	<u>G</u>	5. Paint	<u>N</u>	
Cross Frames		Year Painted	<u>N</u>	
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>	

REMARKS:

- a. Minor splitting of isolated bearing pads and bulging of bearing pad at DNB1 G1 greater than 15% of the bearing thickness.
- b. Some cracks and spalls on bottom flanges of some prestressed girders and some deteriorating patches.
- c. Hairline diagonal web cracking on the concrete girders noted at scattered locations.
- d. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: DNB2 G5, DNB4 G2, DNB5 G3, DNB7 G5, DNB7 G4, DNB8 G1and DNB12 G3. The District is continuously performing girder repairs as weather conditions allow. Worse conditions observed do not exhibit more than one prestressing strand being exposed and deteriorated.
- e. Rocker bearings in different degree of expansion/contraction (typical), bearings have corrosion on up to 30% of the surface area and up to 5% section loss.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [7]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	G G G N N G *	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	- - - - - - G G E N

- a. The 2021 Underwater Inspection revealed only some light to moderate scaling and a shallow failed repair and spall
- b. Minor cracking on many of the piles and some spalling also noted on a few piles and a few longitudinal wide cracks noted with exposed prestressing strands in the top portion of the pile. Spalls with exposed reinforcement/strands noted on the following piles in FY2025: DNB4-C, DNB6-B, DNB7-C and DNB8-C.
- c. West wingwall undermined up to 1' wide x 6" high x up to 4' under wingwall.
- d. Cracking noted over 1/8" wide on piles includes: DNB3-A, DNB6-B.





B-7 DNB **VDOT – BRIDGE INSPECTION REPORT** Page: 3 of 3 **61 CHANNEL: CHANNEL/SLOPE PROTECTION GENERAL CONDITION RATING** [8] 1. Channel Scour 5. Fender System <u>G</u> 2. Embankment 6. Spur Dikes/Jetties **Erosion** 7. Rip Rap/Slope <u>G</u> 3. Drift Protection 4. Vegetation 8. Adequacy of <u>F</u> Opening

REMARKS:

a. Bay bottom depths noted in the 2021 Underwater Inspection Report are well above the acceptable scour depth.

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Sin	gle <u>N</u>		
(R12 – 5) – Ser	mi <u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- Structure load rating controlled by modular superstructure units with reduced capacity from having twoprestressing strands removed due to corrosion (worst-case observed during past inspections, not present this year).
- c. No posting of the structure is required.





5.10 Trestle DSB SI&A Forms

B-6 DSB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 1

Structure-ID: 1016 Type: Regular Inspection

County/City:NorthamptonDate of Inspection:12/3/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 34.29 Location: Over Chesapeake Bay

Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers

WORK DONE: Routine Maintenance and Roadway Striping Replaced in Spring 2020.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Generally Good

- 1. Scour remediation was performed at Bent DSB1 in 2002.
- 2. Minor hairline cracks in many piles above water/land and some bent caps.
- 3. Isolated spalls on underside of deck, exposed chairs also noted throughout on underside of deck.
- 4. Widespread map cracking (hairline to narrow) of concrete deck surface in Spans DSB2, DSB3, DSB,5, DSB13, DSB14, DSB17, and DSB18.
- 5. Asphalt breaking up in top of backwall in L1.
- 6. Bridge rail has impact damage in the following spans: DSB6 (12LF), DSB21 (1LF).
- 7. Isolated spalls on curb facing up to 6" long x full height x up to 2" deep
- 8. East railing in Span 20 and 21 have been replaced in the past.
- The 2024 Underwater Inspection revealed only a few hairline cracks and a few minor shallow spalls on some of the piles. No major changes since the previous underwater inspection.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Continue periodic hydrographic surveys in interim between underwater inspections scheduled every 5 years.
- 2. Update the 2023 Hydrographic to indicate Bent DSB1 scour remediation completed in 2002.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





VDOT – BRIDGE INSPECTION REPORT Page: 1 of 3 B-7 DSB Structure-ID: 1016 Regular Inspection Type: Northampton Date of Inspection: 12/3/2024 County/City: _(Co./Str.No) Structure: Feature/Intersection: Chesapeake Bay 13 Main Route: Facility/Carried: Milepost: 34.29 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): **Ethan Stivers** SPECIAL CONDITIONS OR REQUIREMENTS 1. Fracture Critical 5. Segmental <u>X</u> 2. Underwater 6. Pin & Hanger 3. Scour Critical <u>X</u> 7. Fatigue Prone 4. Moveable **36 TRAFFIC SAFETY FEATURES** 1. Bridge Railing 3. Approach 1 <u>1</u>

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

4. Approach Guardrail

1

58 DECK		GENERAL CONDITION RATING		[7]
1. Wearing Surface	_	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	_	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

REMARKS:

2. Transition

1

- g. Hairline cracks with efflorescence in the deck overhangs of several spans inspected.
- h. Isolated spalls on underside of deck, exposed chairs also noted throughout on underside of deck.
- i. Widespread map cracking (hairline to narrow) of concrete deck surface in Spans DSB2, DSB3, DSB,5, DSB13, DSB14, DSB17, and DSB18.
- j. Asphalt breaking up in top of backwall in left lane near asphalt approach.
- k. Bridge rail has impact damage in the following spans: DSB6 (12LF), DSB21 (1LF).
- l. Isolated spalls on curb facing up to 6" long x full height x up to 2" deep.
- m. East railing in Span 20 and 21 have been replaced in the past.





B-7 DSB VDOT – BRIDGE INSPECTION REPORT Page: 2 of 3

59 SUPERSTRUCTURE GENERAL CONDITION RATING [8] 1. Bearing Devices <u>G</u> 4. Trusses 2. Stringers N A. General N 3. Girders, Beams, or Slab Spans B. Portals N A. General C. Bracing <u>N</u> B. Diaphragms or **G** 5. Paint <u>N</u> **Cross Frames** Year Painted 6. Machinery (Moveable Span) C. Bracing Ν Ν

REMARKS:

- Minor cracking and spalling at scattered closure pour diaphragms with exposed reinforcement/prestressing with little to no relevant section loss.
- b. Isolated spalls with exposed reinforcement on beam ends. Spalls up to 6" high x 6" long x up to 4" deep.
- c. Spans 1 thru 19 inspected from Snooper vehicle

60 SUBSTRUCTURE		GENERAL CONDITION	RATING	[7]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	- - <u>N</u> <u>N</u> -	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles D. Bracing	- - - <u>F</u> - <u>G</u> <u>G</u> <u>G</u>	

- a. The 2024 Underwater Inspection revealed only a few hairline cracks and a few minor shallow spalls on some of the piles. No major changes since the previous underwater inspection.
- b. Minor hairline cracks in most piles above water or above ground line adjacent to pile section joints. Crack with rust staining observed above water on Bent 7 Pile B.
- c. Minor hairline cracks on a few bent caps.





B-7 DSB	VDOT – BRIDGE INSPECTION REPORT		Page:	3 of 3
61 CHANNEL: CHANI	NEL/SLOPE PROTECTION	GENERAL CONDITION	ON RATING	[7]
 Channel Scour Embankment Erosion Drift 	E - -	5. Fender System 6. Spur Dikes/Jetties 7. Rip Rap/Slope Protection	- <u>G</u>	
4. Vegetation	-	8. Adequacy of Opening	<u>F</u>	

REMARKS:

- a. Scour remediation was performed at Bent DSB1 in 2002.
- 2023 Hydrographic Survey indicates bay bottom was below ASD at DSB1 and near ASD at DSB-2 and DSB-3. It is noted above that Bent DSB1 has scour remediation completed in 2002; however, the remediation is not noted on the ASD survey limits.
- c. Bay bottom is relatively stable as indicated in the 2004, 2009, 2014, and the 2019 Underwater Inspections.

66 POSTED LOADING

1. Posted Loading -		2. Legibility	<u>N</u>
(R12 -1)	<u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Sin	gle <u>N</u>		
(R12 – 5) – Ser	mi N		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. No posting of the structure is required.





5.11 Trestle ENB, FIB-NB, & FNB SI&A Forms

B-6 ENB, FIB-NB, & FNB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID: 1017 Type: Regular Inspection

County/City: Northampton Date of Inspection: 11/25/2024 & 11/26/2024

Structure: _____(Co./Str.No) Feature/Intersection: Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 32.32 Location: Over Chesapeake Bay Lead Inspector: Jon Chapman, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Good

- 1. Minor hairline cracking on many of the piles above water or land.
- 2. Steel Tooth Expansion Joint at FIB-NB Pier 4 opening was off by about 1/2" from the east side (1 3/4") to the west side (1 1/4"). The weather was cloudy at the time of measurement (9/10/2020 at 2:11pm) with an ambient air temperature around 80°F with both the steel girders and concrete girders measuring at 80°F. The opening dimension for these conditions should have been 1 15/16" according to construction documents.
- 3. Exposed reinforcement chairs located on underside of girder (typically near ends) throughout spans.
- 4. Isolated spalls some with exposed reinforcement on underside of deck up to 2" deep.
- 5. Beam ends delamination/spalls on end corners / back face of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- 6. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x 1" deep with rust stain.
- 7. No apparent change in spiral cracking on piles at Bents ENB7 and ENB9 (caused during construction).
- 8. Rip rap encompassing Bent FNB4 remains well placed and stable.
- 9. Minor spalls noted in the girders and at the diaphragm closure pours at scattered locations in the trestle spans some with exposed reinforcement and some with section loss to exposed reinforcement.
- 10. Isolated surface spalls in Span ENB1.
- 11. Scattered minor transverse cracks in concrete portions of FIB bridge deck.
- 12. Isolated minor rust spots and peeled off topcoat with exposed zinc primer on FIB girders, cross frames and bracing.
- 13. Minor rust continues to develop on nuts and bolts of FIB girder splices and at cross frames and lower lateral bracing connections.
- 14. Moderate rust around perimeter of FIB rocker bearing base plates and isolated rust spots on some pins and anchor bolts.
- 15. The 2024 Underwater Inspection revealed only a few hairline cracks and a few minor shallow spalls on some of the piles.
- 16. North Fender system was repaired in Fall 2018 after damage that was caused around May 27, 2018.
- 17. Portions of the top wale and lower wale are missing in fender system in FIBSB / FIBNB navigation channel.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. Structure load rating controlled by the interior girders of FIB-NB.
- 3. No posting of the structure is required.



B-6 ENB, FIB-NB, & FNB

VDOT – BRIDGE INSPECTION REPORT

Page: 2 of 2

RECOMMENDATIONS:

- 1. Monitor the Steel Tooth Expansion Joint opening on extremely hot days (temperatures approaching 120°F) to observe if opening closes and if any distress occurs on the superstructure.
- 2. Clean exposed reinforcement and patch girder spalls on trestle spans.
- 3. Replace top protective coating on steel girders at locations where zinc primer is exposed, or coating has bubbled and exhibits freckled rust (girder splices, cross frames, and rocker bearings).
- 4. Monitor piles exhibiting spiral cracking for further degradation.
- 5. Repair/reinstall top wale and portions of lower wale in fender system in FIBSB / FIBNB navigation channel.

SIGNATURE OF INSPECTOR

SIGNATURE OF REVIEWER





B-7 ENB, FIB-NB,	, & FNB VDOT – E	BRIDGE INSPECTION REPORT	Page: 1 of 3
Structure-ID:	1017	Type:	Regular Inspection
County/City:	Northampton	Date of Inspection:	11/25/2024 & 11/26/2024
Structure:	(Co./Str.No)	Feature/Intersection:	Chesapeake Bay
Main Route:	13	Facility/Carried:	
Milepost:	32.32	Location:	Over Chesapeake Bay
Lead Inspector:	Jon Chapman, PE	Additional Inspector(s):	Ethan Stivers, Jon Krites
	IONS OR REQUIREMENTS		
SPECIAL CONDITION 1. Fracture Critica 2. Underwater		5. Segmental 6. Pin & Hanger	_

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

3. Approach

4. Approach Guardrail

<u>1</u>

1

58 DECK		GENERAL CONDITION RATING [8]		[8]
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

REMARKS:

1. Bridge Railing

2. Transition

- a. Minor transverse hairline cracks scattered in concrete FIB-NB deck surface.
- b. Steel Tooth Expansion Joint at FIB-NB Pier 4 opening was off by about 1/2" from the east side (1 3/4") to the west side (1 1/4"). The weather was cloudy at the time of measurement (9/10/2020 at 2:11pm) with an ambient air temperature around 80°F with both the steel girders and concrete girders measuring at 80°F. The opening dimension for these conditions should have been 1 15/16" according to construction documents.
- c. Isolated surface spalls on ENB1 east side.

<u>1</u>

1

- d. Exposed reinforcement chairs located on underside of girder (typically near ends) throughout spans.
- e. Isolated spalls some with exposed reinforcement on underside of deck up to 2" deep.





B-7 ENB, FIB-NB, & FN	IB	VDOT – BRIDGE INSPECTION REPORT	F	Page: 2 of 3
59 SUPERSTRUCTURE		GENERAL CONDITION RATII	NG	[8]
 Bearing Devices Stringers Girders, Beams, or Sl A. General B. Diaphragms or Cross Frames C. Bracing 	<u>G</u> .	4. Trusses A. General B. Portals C. Bracing 5. Paint Year Painted 6. Machinery (Moveable Span)	N N N G 2013 N	

REMARKS:

- a. Minor spalls noted in the girders and at the diaphragm closure pours at scattered locations in the trestle spans some with exposed reinforcement and some with section loss to exposed reinforcement.
- b. Isolated minor rust spots and peeled off topcoat with exposed zinc primer on FIB girders, cross frames and bracing.
- c. Minor rust continues to develop on nuts and bolts of FIB girder splices and at cross frame and lower lateral bracing connections.
- d. Rust around perimeter of FIB rocker bearing base plates and isolated rust spots on some pins and anchor bolts.
- e. Beam ends delamination/spalls on end corners / back face of beams (some repaired during construction). Typical spalls measure up to 3" L x up to 12" h x 1" d x full width of back exposing prestressing strands (L1-L4).
- f. Delamination/spalls noted at lifting eyes on girder. Delaminations/spalls up to 6" diameter x 1" deep with rust stain.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING	[8]
1. Abutments A. Wings B. Backwall C. Bearing Seats D. Breastwall E. Weepholes F. Footing G. Piles H. Erosion/Scour I. Settlement *Not Visible	G G G N N G * G G	2. Pier/Bent A. Caps B. Piles C. Column, Stem, Wall D. Piles E. Bracing F. Erosion/Scour G. Settlement 3. Pile Bent A. Caps B. Bearing Seats C. Piles	- - - - - <u>G</u> <u>G</u>	
		D. Bracing	<u>N</u>	

- a. Minor hairline cracking on many of the piles above water or land
- b. Some spalls with exposed reinforcement noted at pile joints of a few piles.
- c. No apparent change in spiral cracking on piles at Bents ENB7 and ENB9 (caused during construction)
- d. The 2025 Underwater Inspection revealed isolated cracks up to 1/16" wide and a few minor shallow spalls on some of the piles.





B-7 ENB, FIB-NB, & FNB VDOT - BRIDGE INSPECTION REPORT Page: 3 of 3

61 CHANNEL: CHANN	NEL/SLOPE PROTECTION	GENERAL CONDITION	ON RATING	[7]
Channel Scour Embankment Erosion	<u>G</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope	<u>E</u>	
3. Drift 4. Vegetation	-	Protection 8. Adequacy of	<u>G</u> <u>F</u>	
		Opening		

REMARKS:

- a. Rip rap encompassing Bent FNB4 remains well placed and stable.
- b. Portions of the top wale and lower wale are missing in fender system in FIBSB / FIBNB navigation channel.

66 POSTED LOADING

1. Posted Loading -	2. Legibility	<u>N</u>
(R12 -1) <u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single <u>N</u>		
(R12 – 5) – Semi <u>N</u>		

- a. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- b. Structure load rating controlled by the interior girders of FIB-NB.
- c. No posting of the structure is required.





5.12 Trestle ESB, FIB-SB, & FSB SI&A Forms

B-6 ESB, FIB-SB, & FSB VDOT – BRIDGE INSPECTION REPORT Page: 1 of 2

Structure-ID:1007Type:Regular InspectionCounty/City:NorthamptonDate of Inspection:11/25/2024Structure:_____(Co./Str.No)Feature/Intersection:Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 32.32 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

WORK DONE: Routine Maintenance.

REVISED DIMENSIONS: None

MISCELLANEOUS: No Encroachment Violations

CONDITION OF STRUCTURE: Generally Fair to Good

- 1. Underside of deck, exposed chairs throughout (all spans).
- Underside of deck isolated spalls up to 4" deep. Spall with exposed reinforcement up to 60% loss of section to reinforcement located in Span 10 Bay 2.
- 3. Top of flange, edge spalls up to 2-1/2" wide x ½" deep (all spans).
- Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: FSB7 G8, FSB8 G1, ESB13 G1, ESB10 G3, ESB10 G4, ESB10 G3, ESB9 G1, ESB7 G1, ESB7G4, ESB1 G7.
- 5. Minor cracking and/or spalling with isolated exposed reinforcement at diaphragms.
- 6. Minor longitudinal cracks up to 1/32" wide at joints typically 1' to 2' on center.
- 7. FIB Span 3 railing 10th post from Pier 4 has a 3" diameter perforation in web of post.
- 8. E Trestle surface rust/corrosion on bolts on railing, typical.
- 9. E Trestle 7th rail section from Bent 12 (west side) has impact damage to top rail for 1 LF and 8th post from Bent 1 east side has impact damage with a 2" long tear in bottom of post; impact damage continues into Span 3 for 30LF.
- 10. E Trestle end cap at Bent 9 bottom rail cracked on west side.
- 11. E Trestle curb with map cracking with efflorescence and isolated spalls up to 1" deep.
- 12. F Trestle asphalt in Lane 2 missing on top of backwall and asphalt breaking up on north approach.
- 13. F Trestle curb with map cracking with efflorescence and isolated spalls up to 2" deep. Exposed reinforcement noted in Span 3 on west side.
- 14. Minor horizontal cracking, some with efflorescence, in intermediate concrete girder diaphragms. Isolated diaphragms have spalls with exposed reinforcement with up to 10% loss of section.
- 15. Random spalls on bottom flanges of prestressed girders, with some previous patches failed or failing.
- 16. Isolated minor rust spots on FIB girders, cross frames & bracing members.
- 17. Distortion on one FIB girder bottom flange for approximately 3 feet that does not require mitigation; and buckling of one lower lateral bracing diagonal.
- 18. Minor rust on isolated nuts and bolts of FIB girder splices and other field connections.
- 19. Cracks in the asphalt wearing surface sealed, but a few scattered unsealed cracks were noted and recommended for sealing.
- 20. Minor transverse cracks scattered in the FIB concrete deck surface.
- 21. Roadway striping replaced in 2020.
- 22. Hairline cracking on many of the piles above water or land.
- 23. Isolated spalling on some of the piles. Spalls noted on the following pile locations with exposed reinforcement. Some reinforcement has up to 100% loss of section to rebar: ESB-B2-A, FIB-SB-B4-B, FIB-SB-B3-CN, FIB-SB-B3-CN, FIB-SB-B3-DN, FIB-SB-B2-AN, FIB-SB-B2-CN, FIB-SB-B3-DN, FIB-SB-B2-AN, FIB-SB-B2-CN, FIB-SB-B1-B, FIB-SB-B1-D, FIB-SB-B1-E.



B-6 ESB, FIB-SB, & FSB

VDOT – BRIDGE INSPECTION REPORT

- Page: 2 of 2
- 24. Repairs involving installation of cathodic protection jackets on FIB-SB2 and FIB-SB3 were completed in 2014. Several piles at Pier FIB-SB2 and FIB-SB3 exhibited damage to the fiberglass jackets.
- 25. Previously noted cracks above the jackets on FIB-SB2 and FIB-SB3 were noted to be routed and packed with minor periphery cracking.
- 26. Fender System at Bent FIB-SB3 was replaced in 2006. Bottom whaler exhibiting 70% section loss at the waterline, but the rest exhibit minimal deterioration. North Fender system was repaired in Fall 2018 after damage that was caused around May 27, 2018.
- 27. Three pile cluster dolphins and portions of each whaler replaced at west end of fender system in front of Pier FIB-SB2 in 2016 and in good condition.
- 28. Isolated minor cracks and spalls on a few piles underwater. For a detailed condition of piles underwater and bay bottom scour conditions, see the 2021 Underwater Inspection Report.
- 29. Portions of the top wale and lower wale are missing in fender system in FIBSB / FIBNB navigation channel.

REVISED STRESS ANALYSIS:

- 1. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- 2. No posting of the structure is required.

RECOMMENDATIONS:

- 1. Patch spalls on prestressed concrete girders and piles.
- 2. Perform periodic hydrographic surveys in interim between underwater inspections.
- 3. Monitor and repair any exposed zinc mesh under the spalled fiberglass jackets on Pier FIB-SB2 and FIB-SB3. CBBT is exploring an alternative repair method that will utilize carbon fiber shells to address the deterioration.
- 4. Clean out and seal transverse cracks in concrete deck spans of FIB-SB.
- 5. Clean and lubricate rocker bearings on FIB-SB.
- 6. Replace buckled lower lateral bracing diagonal.
- 7. Replace top protective coating on steel girders at locations where zinc primer is exposed, or coating has bubbled and exhibits freckled rust (girder splices, cross frames, and rocker bearings).
- 8. Repair drainage swale in slope protection on west end of FSB.
- 9. Repair / reinstall top wale and portions of bottom wale in fender system in FIBSB / FIBNB navigation channel.

SIGNATURE OF INSPECTOR

harron M. S. Turner

SIGNATURE OF REVIEWER





B-7 ESB, FIB-SE	3, & FSB	VDOT – BRIDG	E INSPECTION REPORT	Page: 1 of 3
Structure-ID:	1007		Type:	Regular Inspection
County/City:	Northamp	oton	Date of Inspection:	11/25/2024
Structure:	(0	Co./Str.No)	Feature/Intersection:	Chesapeake Bay

Main Route: 13 Facility/Carried:

Milepost: 32.32 Location: Over Chesapeake Bay Lead Inspector: Shannon Turner, PE Additional Inspector(s): Ethan Stivers, Jon Krites

SPECIAL CONDITIONS OR REQUIREMENTS

1. Fracture Critical	_	5. Segmental	_
2. Underwater	<u>X</u>	6. Pin & Hanger	_
3. Scour Critical	<u>X</u>	7. Fatigue Prone	_
4 Moveable			

36 TRAFFIC SAFETY FEATURES

1. Bridge Railing13. Approach12. Transition4. Approach Guardrail1

REMARKS: Traffic Safety Features have been identified as "1" and is based on memo provided by the District that indicates the due diligence has been completed to determine that bridge railings are acceptable.

58 DECK	GENERAL CONDITION RATING		[8]	
1. Wearing Surface	<u>G</u>	6. Railing	<u>G</u>	
2. Deck – Structural	<u>G</u>	7. Drains	<u>N</u>	
3. Curbs	<u>G</u>	8. Lighting	<u>G</u>	
4. Median	<u>N</u>	9. Utilities	<u>G</u>	
5. Sidewalks	<u>N</u>	10. Expansion Joints	<u>G</u>	
		or Devices		

- a. The typical simple span fixed and expansion open deck joints of the trestle spans were paved over during the 1999 overlay
- b. Minor transverse cracks scattered in the FIB concrete deck surface recommended for sealing.
- c. Minor longitudinal cracks up to 1/32" wide at joints typically 1' to 2' on center.
- FIB Span 3 railing 10th post from Pier 4 has a 3" diameter perforation in web of post.
- e. E Trestle surface rust/corrosion on bolts on railing, typical.
- f. E Trestle 7th rail section from Bent 12 (west side) has impact damage to top rail for 1 LF and 8th post from Bent 1 east side has impact damage with a 2" long tear in bottom of post; impact damage continues into Span 3 for 30LF.
- g. E Trestle end cap at Bent 9 bottom rail cracked on west side.
- E Trestle curb with map cracking with efflorescence and isolated spalls up to 1" deep.
- i. F Trestle asphalt in Lane 2 missing on top of backwall and asphalt breaking up on north approach.
- j. F Trestle curb with map cracking with efflorescence and isolated spalls up to 2" deep. Exposed reinforcement noted in Span 3 on west side.



B-7 ESB, FIB-SB, & FSB VDOT – BRIDGE INSPECTION REPORT Page: 2 of 3

59 SUPERSTRUCTURE		GENERAL CONDITION RATI	NG [7]
1. Bearing Devices	<u>G</u>	4. Trusses	
2. Stringers	<u>N</u>	A. General	<u>N</u>
3. Girders, Beams, or Sl	ab Spans	B. Portals	<u>N</u>
A. General	<u>F</u>	C. Bracing	<u>N</u>
B. Diaphragms or	<u>G</u>	5. Paint	<u>G</u>
Cross Frames		Year Painted	<u>2013</u>
C. Bracing	<u>N</u>	6. Machinery (Moveable Span)	<u>N</u>

REMARKS:

- a. Some spalls on bottom flanges of some prestressed girders of trestle spans, with some previous patches failed or failing
- b. Minor rust spots on FIB girders, cross frames & bracing members
- c. Minor rust on some nuts and bolts of FIB girder splices and other field connections
- d. Distortion on one FIB girder bottom flange for approximately 3 feet that does not require mitigation and buckling of one lower lateral bracing diagonal.
- e. Minor horizontal cracking, some with efflorescence, in intermediate concrete girder diaphragms. Isolated diaphragms have spalls with exposed reinforcement with up to 10% loss of section.
- f. Girder spall with exposed prestressing strand/exposed reinforcement with section loss at the following locations in FY2025: FSB7 B8, FSB8 B1, ESB13 B1, ESB10 B3, ESB10 B4, ESB10 B3, ESB9 B1, ESB7 B1, ESB7B4, ESB1 B7.

60 SUBSTRUCTURE		GENERAL CONDITION	RATING [7]
1. Abutments		2. Pier/Bent	
A. Wings	<u>G</u>	A. Caps	_
B. Backwall	<u>G</u>	B. Piles	_
C. Bearing Seats	<u>G</u>	C. Column, Stem, Wall	_
D. Breastwall	<u>N</u>	D. Piles	_
E. Weepholes	<u>N</u>	E. Bracing	_
F. Footing	<u>G</u>	F. Erosion/Scour	_
G. Piles	*	G. Settlement	_
H. Erosion/Scour	<u>G</u>	3. Pile Bent	
I. Settlement	<u>G</u>	A. Caps	<u>G</u>
*Not Visible		B. Bearing Seats	<u>G</u>
		C. Piles	<u>F</u>
		D. Bracing	<u>N</u>

- a. Repairs involving installation of cathodic protection jackets on Piers FIB-SB2 and FIB-SB3 were completed in 2014. Several piles noted to have large sections of spalled fiberglass jackets at Pier FIB-SB3
- b. Previously noted cracks above the jackets on FIB-SB2 and FIB-SB3 were noted to be routed and packed with minor periphery cracking
- c. Minor hairline cracking with efflorescence and/or rust staining on many of the piles above water or land.
- d. Spalls noted on the following pile locations with exposed reinforcement. Some reinforcement has up to 100% loss of section to rebar: ESB-B2-A, FIB-SB-B4-B, FIB-SB-B4-B, FIB-SB-B3-AN, FIB-SB-B3-CN, FIB-SB-B3-CN, FIB-SB-B4-B, FIB-SB-B4-B,





B-7 ESB, FIB-SB, & FSB VDOT – BRIDGE INSPECTION REPORT Page: 3 of 3

B3-CS, FIB-SB-B3-DN, FIB-SB-B2-AN, FIB-SB-B2-CN, FIB-SB-B2-CS, FIB-SB-B1-A, FIB-SB-B1-B, FIB-SB-B1-D, FIB-SB-B1-E.

- e. FIB-SB Bent 2, and 3 piles have jackets. Jackets broken with grout missing typically 10' above waterline.
- f. No apparent change in spiral cracking on piles at Bents ENB7 and ENB9 (caused during construction)
- g. The 2025 Underwater Inspection revealed **isolated cracks up to 1/16" wide** and a few minor shallow spalls on some of the piles.
- h. 2023 Hydrographic Survey indicates bay bottom was above ASD.

61 CHANNEL: CHANN	IEL/SLOPE PROTECTION	GENERAL CONDITION	N RATING	[8]
1. Channel Scour 2. Embankment Erosion	<u>G</u> -	5. Fender System6. Spur Dikes/Jetties7. Rip Rap/Slope	<u>G</u> - <u>G</u>	
3. Drift 4. Vegetation	-	Protection 8. Adequacy of Opening	<u>F</u>	

REMARKS:

- a. Fender system at Pier FIB-SB3 was replaced in 2006. Bottom whaler exhibits 70% section loss at the waterline, but the rest exhibit minimal deterioration. North Fender system was repaired in Fall 2018 after damage that was caused around May 27, 2018.
- b. Three pile cluster dolphins and portions of each whaler replaced at west end of fender system in front of Pier FIB-SB2 in 2016 and in good condition
- c. Portions of the top wale and lower wale are missing in fender system in FIBSB / FIBNB navigation channel.
- d. FSB at south end on west side, drainage swale/flume settled and disconnected for 10LF.

66 POSTED LOADING

1. Posted Loading -	2. Legibility	<u>N</u>
(R12 -1) <u>N</u>	3. Visibility	<u>N</u>
(R12 – 5) – Single <u>N</u>		
(R12 - 5) - Semi N		

- c. Load rating completed in 12/2018 in accordance with VDOT Instructional and Informational Memorandum 86.2: Load Rating and Posting of Structures (Bridges and Culverts) dated 11/16/2018.
- d. No posting of the structure is required.





6. Bridge Element Level Data

The tables provided in this section display the element level bridge condition data and condition states coded per the AASHTO Manual for Bridge Element Inspection, First Edition and the VDOT Supplement to the AASHTO Manual for Bridge Element Inspection (2016) at the time of this inspection.

For Defect 6000 Scour, the Condition State (CS) for each element was determined utilizing the following system:

Condition State	Description	
1 Good	No Scour	The bay bottom profile at the substructure unit is greater than 10-feet above the Allowable Scour Depth (ASD)
2 Fair	Within Tolerable Limits	The bay bottom at the substructure unit is greater than 5-feet above the ASD, or scour remediation has been placed at the substructure unit, but not below ASD
3 Poor	Within Critical Limits	The bay bottom at the substructure unit is above the ASD, or scour remediation has been placed at the substructure unit at or above the ASD
4 Severe	Warrants a Structural Review	The bay bottom at the pile is below its ASD and scour remediation has not been placed at the substructure unit





6.1 Trestle ANB Element Level Data

Trestle ANB consists of Spans A'NB1-10 and Spans ANB 1-226. Spans ANB 177 - 236 received a hands-on inspection this year. Element level data is presented in this section for all portions of the trestle that have a received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2024, and element level data has been provided for all piles that were inspected along the trestle.

Table 51: ANB Element Level Data

	ANB (1002) Element Level Data							
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
DECKS AN	D SLABS							
16	Reinforced Concrete Top Flange	SF	527,577	575	122	0	528,262	
1080	Delamination/Spall/ Patched Area	SF		41	63		104	
1090	Exposed Rebar	SF		38	59		97	
1120	Efflorescence/Rust Staining	SF		341			341	
1130	Cracking (RC and Other)	SF		143			143	
38	Reinforced Concrete Slab	SF	1,188	6			1,194	
1080	Delamination/Spall/ Patched Area	SF		6			6	
RAILINGS								
330	Metal Bridge Railing	LF	33,093	492	15	0	33,600	
1020	Connection	LF		3			3	
1080	Delamination/Spall/ Patched Area	LF		75	13		88	
1090	Exposed Rebar	LF		13	2		15	
1130	Cracking (RC and Other)	LF		383			383	
7000	Damage	LF		18			18	



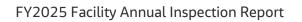


	ANB (1002) Element Level Data							
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
SUPERSTR	UCTURE							
109	Prestressed Concrete Open Girder/Beam	LF	141,232	4,632	247	0	146,111	
1080	Delamination/Spall/ Patched Area	LF		726	83		809	
1090	Exposed Rebar	LF		67	91		158	
1100	Exposed Prestressing	LF		15	19		34	
1110	Cracking (PSC)	LF		3,817	3		3,820	
1120	Efflorescence/Rust Staining	LF		7	51		58	
811	Beam/Girder End	EA	2,035	1,758	115	0	3,908	
1080	Delamination/Spall/ Patched Area	EA		55	30		85	
1090	Exposed Rebar	EA		13	52		65	
1100	Exposed Prestressing	EA			8		8	
1110	Cracking (PSC)	EA		1,679	1		1,680	
1120	Efflorescence/Rust Staining	EA		11	24		35	
BEARINGS								
310	Elastomeric Bearing	EA	3,717	131	68	0	3,916	
1000	Corrosion	EA		8			8	
2230	Bulging, Splitting, or Tearing	EA		123	68		191	





	ANB (1002) Element Level Data							
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
SUBSTRUCTURE								
215	Reinforced Concrete Abutment	LF	66	0	0	0	66	
226	Prestressed Concrete Pile	EA	170	450	100	0	720	
1080	Delamination/Spall/ Patched Area	EA		175	25		200	
1090	Exposed Rebar	EA		1	4		5	
1110	Cracking (PSC)	EA		190	6		196	
1120	Efflorescence/Rust Staining	EA		78	44		122	
1190	Abrasion/Wear (PSC/RC)	EA		6			6	
6000	Scour	EA			21		21	
234	Reinforced Concrete Pier Cap	LF	7,546	562	17	0	8,125	
1080	Delamination/Spall/ Patched Area	LF		45	14		59	
1090	Exposed Rebar	LF			2		2	
1120	Efflorescence/Rust Staining	LF		196	1		197	
1130	Cracking (RC and Other)	LF		321			321	
824	Reinforced Concrete Wingwall	EA	2	0	0	0	2	
JOINTS								
304	Open Expansion Joint	LF	8,541	0	0	0	8,541	
845	Joint Effectiveness	EA	246	0	0	0	246	
	SURFACES, PROTECTIVE (/E SYSTEMS	COATIN	GS, AND CON	CRETE REINF	ORCEMENT S	TEEL		
510	Wearing Surfaces	SF	502,235	5,186	196	0	507,617	
3210	Delamination/Spall/ Patched Area/Pothole (Wearing Surface)	SF		2			2	
3220	Crack	SF		5,184	196		5,380	





ANB (1002) Element Level Data								
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
520	Concrete Reinforcement Steel Protective System	SF	59,941	2,568	1,490	587	64,586	
3600	Effectiveness - Protective System (e.g. cathodic)	SF		2,568	1,484	407	4,459	
7000	Damage	SF			6	180	186	
521	Concrete Protective Coating	SF	138,800	257	170	2,092	141,319	
3540	Effectiveness (Concrete Protective Coatings)	SF		257	170	2,092	2,519	
SLOPE ANI	CHANNEL ELEMENTS							
852	Protected Slope – Paved	EA	1	0	0	0	1	
6506	Paved Slope Effectiveness	EA	1				1	
853	Protected Slope - Riprap	EA	1	0	0	0	1	
6507	Loss of Riprap	EA	1				1	
854	Channel	EA	0	1	0	0	1	
6000	Scour	EA		1			1	





6.2 Trestle ASB Element Level Data

Trestle ASB consists of Spans ASB 1-205. Spans 39 - 80 received a hands-on inspection this year (approximately 20% of the entire trestle). Element level data is presented in this section for all portions of the trestle that have a received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2024, and element level data has been provided for all piles that were inspected along the trestle.

Table 52: ASB Element Level Data

	ASB (1010) Element Level Data							
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
DECKS ANI	DECKS AND SLABS							
12	Reinforced Concrete Deck	SF	35,961	666	3	0	36,630	
1080	Delamination/Spall/ Patched Area	SF			3		3	
1120	Efflorescence/Rust Staining	SF		178			178	
1130	Cracking (RC and Other)	SF		488			488	
16	Reinforced Concrete Top Flange	SF	619,723	1,164	75	0	620,962	
1080	Delamination/Spall/ Patched Area	SF		22	46		68	
1090	Exposed Rebar	SF		2	27		29	
1120	Efflorescence/Rust Staining	SF		8			8	
1130	Cracking (RC and Other)	SF		1,132	2		1,134	
RAILINGS								
330	Metal Bridge Railing	LF	35,012	119	35	0	35,166	
1020	Connection	LF		3			3	
1080	Delamination/Spall/ Patched Area	LF		32	6		38	
1090	Exposed Rebar	LF			6		6	
1120	Efflorescence/Rust Staining	LF			21		21	
1130	Cracking (RC and Other)	LF		72			72	
1900	Distortion	LF		12	2		14	
SUPERSTR	UCTURE							



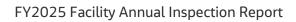


	1	ASB (1	010) Eleme	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
107	Steel Open Girder/Beam	LF	1,340	20	0	0	1,360
1000	Corrosion	LF		20			20
109	Prestressed Concrete Open Girder/Beam	LF	104,605	219	58	0	104,882
1080	Delamination/Spall/ Patched Area	LF		112	12		124
1090	Exposed Rebar	LF		60	46		106
1100	Exposed Prestressing	LF		14			14
1110	Cracking (PSC)	LF		27			27
1120	Efflorescence/Rust Staining	LF		6			6
811	Beam/Girder End	EA	3,611	214	43	0	3,868
1080	Delamination/Spall/ Patched Area	EA		165	7		172
1090	Exposed Rebar	EA		41	35		76
1100	Exposed Prestressing	EA		3			3
1110	Cracking (PSC)	EA		4	1		5
1120	Efflorescence/Rust Staining	EA		1			1
BEARINGS							
310	Elastomeric Bearing	EA	2,448	0	0	0	2,448
SUBSTRUC	TURE						
215	Reinforced Concrete Abutment	LF	89	0	0	0	89





Element Number Element Name Unit Condition State 1 Condition State 2 Condition State 3 Condition State 4 226 Prestressed Concrete Pile EA 281 314 30 10 1080 Delamination/Spall/Patched Area EA 76 20	Total 635 96 10 135 58
226 Concrete Pile EA 281 314 30 10 1080 Delamination/Spall/ Patched Area EA 76 20	96 10 135
1080 Patched Area EA 76 20	10 135
1000 Funesad Dahay FA	135
1090 Exposed Rebar EA 8 2	
1110 Cracking (PSC) EA 132 3	58
1120 Efflorescence/Rust EA 54 4	
1190 Abrasion/Wear (PSC/RC) EA 44 1	45
6000 Scour EA 10	10
234 Reinforced Concrete Pier Cap LF 7,056 466 15 0	7,537
1080 Delamination/Spall/ LF 17 14	31
1090 Exposed Rebar LF 1	1
1120 Efflorescence/Rust LF 1	1
1130 Cracking (RC and Other) LF 448	448
824 Reinforced Concrete Wingwall EA 2 0 0 0	2
JOINTS	
304 Open Expansion Joint LF 3,888 44 0 0	3,932
2360 Adjacent Deck or LF 44	44
845 Joint Effectiveness EA 67 0 0	67
WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS	
510 Wearing Surfaces SF 574,565 1,210 0 0 5	575,775
Delamination/Spall/ 3210 Patched Area/Pothole SF (Wearing Surface)	1
3220 Crack (Wearing SF 1,209	1,209
SLOPE AND CHANNEL ELEMENTS	
853 Protected Slope - EA 1 0 0 0	1





ASB (1010) Element Level Data									
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total		
854	Channel	EA	0	1	0	0	1		
6000	Scour	EA		1			1		





6.3 Trestle BNB Element Level Data

Trestle BNB consists of Spans BNB 1/2-265. Spans 159 - 212 received a hands-on inspection this year (approximately 20% of the entire trestle). Element level data is presented in this section for all portions of the trestle that have a received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2025, and element level data has been provided for all piles that were inspected along the trestle.

Table 53: BNB Element Level Data

	l	BNB (1	004) Elem	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS ANI	D SLABS						
12	Reinforced Concrete Deck	SF	3,500	0	0	0	3,500
16	Reinforced Concrete Top Flange	SF	596,263	1,315	22	0	597,600
1080	Delamination/Spall/ Patched Area	SF		22	6		28
1090	Exposed Rebar	SF		6	16		22
1120	Efflorescence/Rust Staining	SF		489			489
1130	Cracking (RC and Other)	SF		798			798
38	Reinforced Concrete Slab	SF	1,174	15	5	0	1,194
1080	Delamination/Spall/ Patched Area	SF		11			11
1090	Exposed Rebar	SF			4		4
1120	Efflorescence/Rust Staining	SF		4	1		5
RAILINGS							
330	Metal Bridge Railing	LF	35,810	608	12	0	36,430
1000	Corrosion	LF		60			60
1020	Connection	LF		18	2		20
1080	Delamination/Spall/ Patched Area	LF		24	10		34
1130	Cracking (RC and Other)	LF		473			473
7000	Damage	LF		33			33



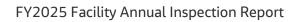


		3NB (1	004) Elem	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
SUPERSTR	UCTURE						
109	Prestressed Concrete Open Girder/Beam	LF	139,833	5,180	787		145,800
1080	Delamination/Spall/ Patched Area	LF		1,820	130		1,950
1090	Exposed Rebar	LF		32	79		111
1100	Exposed Prestressing	LF		10	380		390
1110	Cracking (PSC)	LF		3,315	105		3,420
1120	Efflorescence/Rust Staining	LF		3	93		96
811	Beam/Girder End	EA	2,233	1,543	112	0	3,888
1080	Delamination/Spall/ Patched Area	EA		44	28		72
1090	Exposed Rebar	EA		18	47		65
1110	Cracking (PSC)	EA		1,480	3		1,483
1120	Efflorescence/Rust Staining	EA		1	34		35
BEARINGS							
310	Elastomeric Bearing	EA	3,562	38	288	0	3,888
1000	Corrosion	EA		18			18
2230	Bulging, Splitting, or Tearing	EA		20	288		308
SUBSTRUC	TURE						
215	Reinforced Concrete Abutment	EA	62	3	5	0	70
1080	Delamination/Spall/ Patched Area	LF		2			2
1090	Exposed Rebar	LF			4		4
1120	Efflorescence/Rust Staining	LF			1		1
1130	Cracking (RC and Other)	LF		1			1





	E	3NB (1	004) Elem	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
226	Prestressed Concrete Pile	EA	395	265	136	18	814
1080	Delamination/Spall/ Patched Area	EA		17	14		31
1090	Exposed Rebar	EA		4	21		25
1110	Cracking (PSC)	EA		144	20		164
1120	Efflorescence/Rust Staining	EA		62	30		92
1190	Abrasion/Wear (PSC/RC)	EA		38			38
6000	Scour	EA			51	18	69
234	Reinforced Concrete Pier Cap	LF	6,458	1,536	38		8,032
1080	Delamination/Spall/ Patched Area	LF		32	1		33
1090	Exposed Rebar	LF			1		1
1120	Efflorescence/Rust Staining	LF		3			3
1130	Cracking (RC and Other)	LF		1,501	36		1,537
JOINTS							
304	Open Expansion Joint	LF	8,572	0	0	0	8,572
845	Joint Effectiveness	EA	252	0	0	0	252
	SURFACES, PROTECTIVE (/E SYSTEMS	COATIN	GS, AND CON	CRETE REINF	ORCEMENT S	TEEL	
510	Wearing Surfaces	SF	509,432	4,920	0	0	514,352
3220	Crack (Wearing Surface)	SF		4,920			4,920
520	Concrete Reinforcement Steel Protective System	SF	2,533	745	12	45	3,335
3600	Effectiveness - Protective System (e.g. cathodic)	SF		745	12	45	802





	BNB (1004) Element Level Data										
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total				
SLOPE AND CHANNEL ELEMENTS											
853	Protected Slope - Riprap	EA	1	0	0	0	1				
6507	Loss of Riprap	EA	1				1				
854	Channel	EA	0	0	0	1	1				
6000	Scour	EA				1	1				





6.4 Trestle BSB Element Level Data

Trestle BSB consists of Spans BSB 1-203. Spans 57-100 received a hands-on inspection this year (approximately 20% of the entire trestle). Element level data is presented in this section for all portions of the trestle that have a received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2022, and element level data has been provided for all piles that were inspected along the trestle.

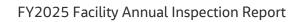
Table 54: BSB Element Level Data

BSB (1012) Element Level Data										
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total			
DECKS AND	O SLABS									
12	Reinforced Concrete Deck	SF	8,907	0	0	0	8,907			
16	Reinforced Concrete Top Flange	SF	781,944	998	59	0	783,001			
1080	Delamination/Spall/ Patched Area	SF		44	34		78			
1090	Exposed Rebar	SF		5	7		12			
1120	Efflorescence/Rust Staining	SF		256	18		274			
1130	Cracking (RC and Other)	SF		693			693			
RAILINGS										
330	Metal Bridge Railing	LF	39,293	126	8	0	39,427			
1080	Delamination/Spall/ Patched Area	LF		8	5		13			
1090	Exposed Rebar	LF		1	3		4			
1130	Cracking (RC and Other)	LF		60			60			
7000	Damage	LF		57			57			
SUPERSTR	UCTURE									
107	Steel Beam/Girder	LF	1,320	462			1,782			
1000	Corrosion	LF		462			462			
109	Prestressed Concrete Open Girder/Beam	LF	119,824	424	89	0	120,337			
1080	Delamination/Spall/ Patched Area	LF		210	23		233			
1090	Exposed Rebar	LF		80	65		145			
1100	Exposed Prestressing	LF		27			27			





		BSB (1	012) Eleme	ent Level Da	ata					
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total			
1110	Cracking (PSC)	LF		103	1		104			
1120	Efflorescence/Rust Staining	LF		4			4			
811	Beam/Girder End	EA	2,099	313	62	0	2,474			
1080	Delamination/Spall/ Patched Area	EA		221	17		238			
1090	Exposed Rebar	EA		37	38		75			
1100	Exposed Prestressing	EA		1	6		7			
1110	Cracking (PSC)	EA		53	1		54			
1120	Efflorescence/Rust Staining	EA		1			1			
BEARINGS										
310	Elastomeric Bearing	EA	2,473	0	1	0	2,474			
2230	Bulging, Splitting, or Tearing	EA			1		1			
SUBSTRUC	TURE	•								
226	Prestressed Concrete Pile	EA	232	329	72	0	633			
1080	Delamination/Spall/ Patched Area	EA		3			3			
1090	Exposed Rebar	EA		9	6		15			
1110	Cracking (PSC)	EA		268	6		274			
1120	Efflorescence/Rust Staining	EA		48	12		60			
1190	Abrasion/Wear (PSC/RC)	EA		1			1			
6000	Scour*	EA			48		48			
234	Reinforced Concrete Pier Cap	LF	7,594	542	7	0	8,143			
1080	Delamination/Spall/ Patched Area	LF		13	1		14			
1090	Exposed Rebar	LF		4	6		10			
1120	Efflorescence/Rust Staining	LF		2			2			





	[3SB (1	012) Eleme	ent Level Da	ata					
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total			
1130	Cracking (RC and Other)	LF		523			523			
JOINTS										
304	Open Expansion Joint	LF	2,720	0	0	0	2,720			
845	Joint Effectiveness	EA	68	0	0	0	68			
WEARING S	WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL									
PROTECTIV	/E SYSTEMS									
510	Wearing Surfaces	SF	673,379	0	686	0	674,065			
3220	Crack (Wearing Surface)	SF			686		686			
515	Steel Protective Coating	SF	13,870	4,624	0	0	18,494			
3440	Effectiveness (Steel Protective Coating)	SF		4,624			4,624			
SLOPE AND	CHANNEL ELEMENTS	•								
854	Channel	EA	0	1	0	0	1			
6000	Scour	EA		1			1			





6.5 Trestle CNB Element Level Data

Trestle CNB consists of Spans CNB 1-322. Spans 198-263 received a hands-on inspection this year (approximately 20% of the entire trestle). Element level data is presented in this section for all portions of the trestle that have a received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2021, and element level data has been provided for all piles that were inspected along the trestle.

Table 55: CNB Element Level Data

	(CNB (1	009) Eleme	ent Level D	ata						
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total				
DECKS ANI	DECKS AND SLABS										
16	Reinforced Concrete Top Flange	SF	734,626	3,222	502	0	738,350				
1080	Delamination/Spall/ Patched Area	SF		28	75		103				
1090	Exposed Rebar	SF		21	3		24				
1120	Efflorescence/Rust Staining	SF		396	400		796				
1130	Cracking (RC and Other)	SF		2,777	24		2,801				
38	Reinforced Concrete Slab	SF	1,194	0	0	0	1,194				
RAILINGS											
330	Metal Bridge Railing	LF	42,922	544	32	0	43,498				
1020	Connection	LF		19			19				
1080	Delamination/Spall/ Patched Area	LF		31	14		45				
1090	Exposed Rebar	LF		9	9		18				
1130	Cracking (RC and Other)	LF		415			415				
7000	Damage	LF		70	9		79				





	(CNB (1	009) Eleme	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
SUPERSTR	UCTURE						
109	Prestressed Concrete Open Girder/Beam	LF	167,321	6,892	981	0	175,194
1080	Delamination/Spall/ Patched Area	LF		4,625	584		5,209
1090	Exposed Rebar	LF		25	88		113
1100	Exposed Prestressing	LF		121	198		319
1110	Cracking (PSC)	LF		1,981	11		1,992
1120	Efflorescence/Rust Staining	LF		140	100		240
811	Beam/Girder End	EA	3,166	1,227	263	0	4,656
1080	Delamination/Spall/ Patched Area	EA		89	148		237
1090	Exposed Rebar	EA		103	51		154
1100	Exposed Prestressing	EA		2	3		5
1110	Cracking (PSC)	EA		1012	4		1016
1120	Efflorescence/Rust Staining	EA		21	57		78
BEARINGS							
310	Elastomeric Bearing	EA	4,119	32	505	0	4,656
1000	Corrosion	EA		16			16
2220	Alignment	EA		5			5
2230	Bulging, Splitting, or Tearing	EA		11	505		516
311	Movable Bearing	EA	0	8	0	0	8
1000	Corrosion	EA		8			8





Element Number		(CNB (1	009) Elem	ent Level D	ata					
Delamination/Spall/Patched Area EA 0 432 552 0 984		Element Name	Unit					Total			
Delamination/Spall/ Patched Area EA	SUBSTRUC	TURE									
1080	226		EA	0	432	552	0	984			
1100 Exposed Prestressing EA	1080	· · · · · · · · · · · · · · · · · · ·	EA		37	73		110			
1110 Cracking (PSC) EA 258 15 273 1120 Efflorescence/Rust Staining EA 104 98 202 6000 Scour EA 348 348 234 Reinforced Concrete Pier Cap LF 8,023 2,616 22 0 10,661 1080 Delamination/Spall/ Patched Area LF 1 14 15 1120 Efflorescence/Rust Staining LF 34 3 37 1130 Cracking (RC and Other) LF 2,581 5 2,586 JOINTS Joint Effectiveness EA 302 0 0 0 10,168 845 Joint Effectiveness EA 302 0 0 0 302 WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS 5 7,728 7,728 3230 Crack (Wearing Surface) SF 7,728 7,728 3230 Surface) SF 10 3 13 Concrete Reinforcement Steel Protective System SLOPE AND CHANNEL ELEMENTS 854 Channel EA 0 1 0 0 1	1090	Exposed Rebar	EA		26	15		41			
1120 Efflorescence/Rust Staining EA 104 98 202	1100	Exposed Prestressing	EA		7	3		10			
1120 Staining	1110	Cracking (PSC)	EA		258	15		273			
Reinforced Concrete Pier Cap	1120	""	EA		104	98		202			
Delamination/Spall/	6000	Scour	EA			348		348			
1120 Efflorescence/Rust LF 34 3 37 1130 Cracking (RC and Other) LF 2,581 5 2,586 JOINTS 304 Open Expansion Joint LF 10,168 0 0 0 10,168 845 Joint Effectiveness EA 302 0 0 0 302 WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS 5 7,728 7,728 3220 Crack (Wearing Surface) SF 7,728 7,728 7,728 3230 Effectiveness (Wearing Surface) SF 10 3 13 520 Reinforcement Steel Protective System SF 1,979 0 0 0 1,979 5LOPE AND CHANNEL ELEMENTS EA 0 1 0 0 1 854 Channel EA 0 1 0 0 1	234		LF	8,023	2,616	22	0	10,661			
1120 Staining LF	1080		LF		1	14		15			
Doints	1120	**	LF		34	3		37			
304 Open Expansion Joint LF 10,168 0 0 0 10,168	1130		LF		2,581	5		2,586			
845 Joint Effectiveness EA 302 0 0 302 WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS 510 Wearing Surfaces SF 606,260 7,738 3 0 614,001 3220 Crack (Wearing Surface) SF 7,728 7,728 7,728 3230 Effectiveness (Wearing Surface) SF 10 3 13 520 Reinforcement Steel Protective System SF 1,979 0 0 0 1,979 SLOPE AND CHANNEL ELEMENTS 854 Channel EA 0 1 0 0 1	JOINTS										
WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS 510 Wearing Surfaces SF 606,260 7,738 3 0 614,001 3220 Crack (Wearing Surface) SF 7,728 7,728 7,728 3230 Effectiveness (Wearing Surface) SF 10 3 13 520 Reinforcement Steel Protective System SF 1,979 0 0 0 1,979 SLOPE AND CHANNEL ELEMENTS EA 0 1 0 0 1	304	Open Expansion Joint	LF	10,168	0	0	0	10,168			
PROTECTIVE SYSTEMS 510 Wearing Surfaces SF 606,260 7,738 3 0 614,001 3220 Crack (Wearing Surface) SF 7,728 7,728 7,728 3230 Effectiveness (Wearing Surface) SF 10 3 13 520 Reinforcement Steel Protective System SF 1,979 0 0 0 1,979 SLOPE AND CHANNEL ELEMENTS 854 Channel EA 0 1 0 0 1	845	Joint Effectiveness	EA	302	0	0	0	302			
3220 Crack (Wearing Surface) SF 7,728 7,728 3230 Effectiveness (Wearing Surface) SF 10 3 13 520 Reinforcement Steel Protective System SF 1,979 0 0 0 1,979 SLOPE AND CHANNEL ELEMENTS 854 Channel EA 0 1 0 0 1			COATIN	GS, AND CON	CRETE REINF	ORCEMENT S	TEEL				
Surface SF	510	Wearing Surfaces	SF	606,260	7,738	3	0	614,001			
Surface SF	3220		SF		7,728			7,728			
520 Reinforcement Steel Protective System SF 1,979 0 0 0 0 1,979 SLOPE AND CHANNEL ELEMENTS 854 Channel EA 0 1 0 0 1	3230		SF		10	3		13			
854 Channel EA 0 1 0 0 1	520	Reinforcement Steel	SF	1,979	0	0	0	1,979			
	SLOPE AND CHANNEL ELEMENTS										
6000 Scour EA 1 1	854	Channel	EA	0	1	0	0	1			
	6000	Scour	EA		1			1			





6.6 Trestle CSB Element Level Data

Trestle CSB consists of Spans CSB 1-246. Spans 53-104 received a hands-on inspection this year (approximately 17% of the entire facility). Element level data is presented in this section for all portions of the trestle that have received hands-on inspection since FY2018. An underwater inspection was performed on the entire trestle in FY2023, and element level data has been provided for all piles that were inspected along the trestle.

Table 56: CSB Element Level Data

		CSB (1	014) Eleme	ent Level Da	ata					
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total			
DECKS ANI	D SLABS									
12	Reinforced Concrete Deck	SF	41,123	435	0	0	41,558			
1120	Efflorescence/Rust Staining	SF		435			435			
16	Reinforced Concrete Top Flange	SF	898,615	273	18	0	898,906			
1080	Delamination/Spall/ Patched Area	SF		45	12		57			
1090	Exposed Rebar	SF		3			3			
1120	Efflorescence/Rust Staining	SF		6	6		12			
1130	Cracking (RC and Other)	SF		219			219			
RAILINGS										
330	Metal Bridge Railing	LF	46,620	203	50	0	46,873			
1020	Connection	LF		62	1		63			
1080	Delamination/Spall/ Patched Area	LF		33	19		52			
1090	Exposed Rebar	LF		1	30		31			
1130	Cracking (RC and Other)	LF		79			79			
7000	Damage	LF		28			28			
SUPERSTR	SUPERSTRUCTURE									
107	Steel Open Girder/Beam	LF	493	419	0	0	912			
1000	Corrosion	LF		419			419			





		CSB (1	014) Eleme	ent Level Da	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
109	Prestressed Concrete Open Girder/Beam	LF	141,604	824	181	0	142,609
1080	Delamination/Spall/ Patched Area	LF		376	71		447
1090	Exposed Rebar	LF		116	95		211
1100	Exposed Prestressing	LF		72	2		74
1110	Cracking (PSC)	LF		158			158
1120	Efflorescence/Rust Staining	LF		102	13		115
811	Beam/Girder End	EA	2,277	516	69	0	2,862
1000	Corrosion	LF		16			16
1080	Delamination/Spall/ Patched Area	EA		412	20		432
1090	Exposed Rebar	EA		55	41		96
1100	Exposed Prestressing	EA		9	1		10
1110	Cracking (PSC)	EA		21			21
1120	Efflorescence/Rust Staining	EA		3	7		10
BEARINGS		•					
310	Elastomeric Bearing	EA	2,847	14	1	0	2,862
1000	Corrosion	EA		14			14
2230	Bulging, Splitting, or Tearing	EA			1		1
SUBSTRUC	TURE	-					
215	Reinforced Concrete Abutment	LF	40	0	0	0	40
226	Prestressed Concrete Pile	EA	367	278	71	36	752
1080	Delamination/Spall/ Patched Area	EA		41	18		59
1090	Exposed Rebar	EA			2		2
1110	Cracking (PSC)	EA		179			179
1120	Efflorescence/Rust Staining	EA		32	12		44
1190	Abrasion/Wear (PSC/RC)	EA		26			26
6000	Scour	EA			39	36	75





	CSB (1014) Element Level Data										
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total				
234	Reinforced Concrete Pier Cap	LF	8,634	687	7	0	9,328				
1080	Delamination/Spall/ Patched Area	LF		12	4		16				
1090	Exposed Rebar	LF			1		1				
1120	Efflorescence/Rust Staining	LF		28	2		30				
1130	Cracking (RC and Other)	LF		647			647				
824	Reinforced Concrete Wingwall	EA	2	0	0	0	2				
JOINTS											
304	Open Expansion Joint	LF	2,680	0	0	0	2,680				
845	Joint Effectiveness	EA	67	0	0	0	67				
APPROACH	I SLABS	'									
321	Reinforced Concrete Approach Slab	SF	771	0	0	0	771				
	SURFACES, PROTECTIVE (/E SYSTEMS	COATIN	GS, AND CON	CRETE REINF	ORCEMENT S	TEEL					
510	Wearing Surfaces	SF	643,528	0	0	0	643,528				
515	Steel Protective Coating	SF	8,994	419	0	10	9,423				
3420	Peeling/Bubbling/ Cracking (Steel Protective Coatings)	SF		419			419				
SLOPE AND	CHANNEL ELEMENTS										
853	Protected Slope - Riprap	EA	1	0	0	0	1				
6507	Loss of Riprap	EA	1				1				
854	Channel	EA	0	1	0	0	1				
6000	Scour	EA		1			1				





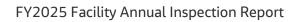
6.7 NCB-NB Element Level Data

NCB-NB consists of Spans 1-17. The entire structure received a routine and fracture critical inspection in FY2024 Element level data is presented in this section. An underwater inspection was performed on the entire trestle in FY2022, and element level data has been provided for all piles that were inspected in this section.

Table 57: NCB-NB Element Level Data

	NC	B-NB	(1006) Eler	ment Level	Data		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS ANI	D SLABS						
12	Reinforced Concrete Deck	SF	109,562	6,749	16	0	116,327
1080	Delamination/Spall/ Patched Area	SF			2		2
1130	Cracking (RC and Other)	SF		6,747	14		6,761
1190	Abrasion/Wear (PSC/RC)	SF		2			2
RAILINGS		1					
330	Metal Bridge Railing	LF	7,582	2	3	0	7,587
1080	Delamination/Spall/ Patched Area	LF		2			2
7000	Damage	LF			3		3
SUPERSTR	UCTURE	1					
107	Steel Open Girder/Beam	LF	1,411	5,362	158	0	6,931
1000	Corrosion	LF		5,360	157		5,517
1020	Connection	LF		2	1		3
113	Steel Stringer	LF	13,944	4,461	494	0	18,899
1000	Corrosion	LF		4,467	494		4,961
120	Steel Truss	LF	123	527	5	0	655
1000	Corrosion	LF		527	5		532
152	Steel Floor Beam	LF	2,780	1,626	510	0	4,916
1000	Corrosion	LF		1,245	503		1,748
1010	Cracking	LF		353	1		354
1020	Connection	LF		28	6		34
162	Gusset Plate	EA	30	10	0	0	40
1000	Corrosion	EA		10			10

	NC	B-NB	(1006) Eler	ment Level	Data				
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total		
811	Beam/Girder End	EA	7	48	11	0	66		
1000	Corrosion	EA		48	11		59		
BEARINGS									
311	Movable Bearing	EA	0	34	0	0	34		
1000	Corrosion	EA		34			34		
313	Fixed Bearing	EA	0	10	0	0	10		
1000	Corrosion	EA		10			10		
SUBSTRUC	TURE								
205	Reinforced Concrete Column	EA	16	19	1	0	36		
1080	Delamination/Spall/ Patched Area	EA			1		1		
1120	Efflorescence/Rust Staining	EA		6			6		
1130	Cracking (RC and Other)	EA		13			13		
226	Prestressed Concrete Pile	EA	2	4	30		36		
1080	Delamination/Spall/ Patched Area	EA		3	6		9		
1120	Efflorescence/Rust Staining	EA		1	7		8		
1130	Cracking (RC and Other)	EA			17		17		
234	Reinforced Concrete Pier Cap	LF	48	8	0	0	56		
1130	Cracking (RC and Other)	LF		8			8		
JOINTS									
305	Assembly Joint without Seal	LF	144	24	0	0	168		
2370	Metal Deterioration or Damage	LF		24			24		
845	Joint Effectiveness	EA	6	0	0	0	6		





	NCB-NB (1006) Element Level Data											
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total					
	SURFACES, PROTECTIVE	COATIN	GS, AND CON	CRETE REINF	ORCEMENT S	TEEL						
PROTECTIV	PROTECTIVE SYSTEMS											
515	Steel Protective Coating	SF	369,229	292	0	27,813	397,334					
3420	Peeling/Bubbling/ Cracking (Steel Protective Coatings)	SF		292			292					
3440	Effectiveness (Steel Protective Coatings)	SF				27,813	27,813					
886	Beam/Girder End Protective Coating - Steel	EA	7	48	11	0	66					
3440	Effectiveness (Steel Protective Coatings)	EA		48	11		59					
SLOPE AND CHANNEL ELEMENTS												
854	Channel	EA	0	1	0	0	1					
6000	Scour	EA		1			1					





6.8 NCB-SB Element Level Data

A hands-on inspection was performed this year and element level data is presented in this section. An underwater inspection was performed on the entire trestle in FY2025 (this year), and element level data has been provided for all piles that were inspected along the trestle.

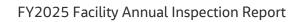
Table 58: NCB-SB Element Level Data

	NO	CB-SB	(1015) Eler	ment Level	Data		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS AN	D SLABS						
12	Reinforced Concrete Deck	SF	90,864	33,136	0	0	124,000
1080	Delamination/Spall/ Patched Area	SF		17			17
1090	Exposed Rebar	SF		3			3
1130	Cracking (RC and Other)	SF		33,116			33,116
RAILINGS							
330	Metal Bridge Railing	LF	6,176	7	17	0	6,200
1080	Delamination/Spall/ Patched Area	LF		2			2
1130	Cracking (RC and Other)	LF		5			5
7000	Damage	LF			17		17
SUPERSTR	UCTURE						
107	Steel Open Girder/Beam	LF	11,151	1,247	2	0	12,400
1000	Corrosion	EA		1,247	2		1,249
811	Steel Open Beam/Girder End	EA	0	32	0	0	32
1000	Corrosion	EA		32			32
BEARINGS							
311	Movable Bearing	EA	0	32	0	0	32
1000	Corrosion	EA		32			32
313	Fixed Bearing	EA	0	40	0	0	40
1000	Corrosion	EA		40			40





	NO	CB-SB	(1015) Eler	nent Level	Data				
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total		
SUBSTRUC	TURE								
205	Reinforced Concrete Column	EA	4	24	2	0	30		
1080	Delamination/Spall/ Patched Area	EA		4	1		5		
1120	Efflorescence/Rust Staining	EA			1		1		
1130	Cracking (RC and Other)	EA		20			20		
220	Reinforced Concrete Pile Cap/Footing	LF	433	5	446	0	884		
1080	Delamination/Spall/ Patched Area	LF		2	430		432		
1120	Efflorescence/Rust Staining	LF			16		16		
1130	Cracking (RC and Other)	LF		3			3		
226	Prestressed Concrete Pile	EA	152	54	13	0	219		
6000	Scour	EA		54	13	0	67		
234	Reinforced Concrete Pier Cap	LF	411	129	0	0	540		
1080	Delamination/Spall/ Patched Area	LF		2			2		
1120	Efflorescence/Rust Staining	LF		23			23		
1130	Cracking (RC and Other)	LF		104			104		
JOINTS									
305	Assembly Joint without Seal	LF	200	0	0	0	200		
845	Joint Effectiveness	EA	5	0	0	0	5		





	NCB-SB (1015) Element Level Data										
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total				
WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL											
PROTECTIV	PROTECTIVE SYSTEMS										
515	Steel Protective Coating	SF	295,198	4,462	0	10	299,670				
3420	Peeling/Bubbling/ Cracking (Steel Protective Coatings)	SF		4,461			4,461				
3440	Effectiveness (Steel Protective Coatings)	SF		1		10	11				
886	Beam/Girder End Protective Coating - Steel	EA	0	32	0	0	32				
SLOPE AND CHANNEL ELEMENTS											
854	Channel	EA	0	1	0	0	1				
6000	Scour	EA		1			1				





6.9 Trestle DNB Element Level Data

A hands-on inspection was performed in this inspection, and element level data is presented in this section. An underwater inspection was performed on the entire trestle in FY2024, and element level data has been provided for all piles that were inspected along the trestle.

Table 59: DNB Element Level Data

	[ONB (1	008) Elem	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS ANI	D SLABS						
16	Reinforced Concrete Top Flange	SF	41,551	116	5	0	41,672
1080	Delamination/Spall/ Patched Area	SF		3	2		5
1090	Exposed Rebar	SF			1		1
1120	Efflorescence/Rust Staining	SF		107	2		109
1130	Cracking (RC and Other)	SF		6			6
RAILINGS							
330	Metal Bridge Railing	LF	2,506	45	0	0	2,551
1130	Cracking (RC and Other)	LF		37			37
7000	Damage	LF		8			8
SUPERSTR	UCTURE	•					
109	Prestressed Concrete Open Girder/Beam	LF	9,896	218	90	0	10,204
1080	Delamination/Spall/ Patched Area	LF		32	5		37
1090	Exposed Rebar	LF		6	15		21
1100	Exposed Prestressing	LF		4	60		64
1110	Cracking (PSC)	LF		173			173
1120	Efflorescence/Rust Staining	LF		3	10		13
811	Beam/Girder End	EA	195	87	6	0	288
1080	Delamination/Spall/ Patched Area	EA		2			2
1090	Exposed Rebar	EA		2	1		3
1110	Cracking (PSC)	EA		81			81



		NB (1	008) Elem	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
1120	Efflorescence/Rust Staining	EA		2	5		7
BEARINGS							
310	Elastomeric Bearing	EA	272	3	5		280
2230	Bulging, Splitting, or Tearing	EA		3	5		8
311	Movable Bearing	EA		8			8
1000	Corrosion	EA		8			8
SUBSTRUC	TURE						
215	Reinforced Concrete Abutment	LF	31	2	0	0	33
1130	Cracking (RC and Other)	LF		2			2
226	Prestressed Concrete Pile	EA	0	37	14	0	51
1080	Delamination/Spall/ Patched Area	EA			4		4
1090	Exposed Rebar	EA			7		7
1110	Cracking (PSC)	EA		34	1		35
1120	Efflorescence/Rust Staining	EA		2	2		4
1190	Abrasion/Wear (PSC/RC)	EA		1			1
234	Reinforced Concrete Pier Cap	LF	560	37	0	0	597
1130	Cracking (RC and Other)	LF		37			37
824	Reinforced Concrete Wingwall	EA	2	0	0	0	2
JOINTS							
304	Open Expansion Joint	LF	588	0	0	0	588
2360	Adjacent Deck or Header	LF	588				588
845	Joint Effectiveness	EA	18				18





	С	NB (1	008) Elem	ent Level D	ata			
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
APPROACH SLABS								
321	Reinforced Concrete Approach Slab	SF	672	0	0	0	672	
WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL								
PROTECTIV	/E SYSTEMS							
510	Wearing Surfaces	SF	35,715	0	0	0	35,715	
SLOPE AND	CHANNEL ELEMENTS							
853	Protected Slope Riprap	EA	1				1	
854	Channel	EA		1			1	
6000	Scour	EA		1			1	





6.10 Trestle DSB Element Level Data

A hands-on inspection was performed this inspection, and element level data is presented in this section. An underwater inspection was performed on the entire trestle in FY2025, and element level data has been provided for all piles that were inspected along the trestle.

Table 60: DSB Element Level Data

	Γ	OSB (1	016) Eleme	ent Level D	ata				
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total		
DECKS AND SLABS									
12	Reinforced Concrete Deck	SF	71,687	11,844	0	0	83,531		
1120	Efflorescence/Rust Staining	SF		84			84		
1130	Cracking (RC and Other)	SF		11,760			11,760		
RAILINGS									
330	Metal Bridge Railing	LF	4,128	49	0	0	4,177		
1080	Delamination/Spall/ Patched Area	LF		32			32		
1130	Cracking (RC and Other)	LF		4			4		
7000	Damage	LF		13			13		
SUPERSTR	UCTURE	•							
109	Prestressed Concrete Open Girder/Beam	LF	10,345	70	26	0	10,441		
1080	Delamination/Spall/ Patched Area	LF		33	26		59		
1090	Exposed Rebar	LF		19			19		
1110	Cracking (PSC)	LF		18			18		
811	Beam/Girder End	EA	119	65	26	0	210		
1080	Delamination/Spall/ Patched Area	EA		29	26		55		
1090	Exposed Rebar	EA		18			18		
1110	Cracking (PSC)	EA		18			18		
BEARINGS									
310	Elastomeric Bearing	EA	210	0	0	0	210		
SUBSTRUC	TURE								





	Į.	OSB (1	016) Eleme	ent Level D	ata		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
215	Reinforced Concrete Abutment	LF	40	0	0	0	40
226	Prestressed Concrete Pile	EA	5	34	17	4	60
1080	Delamination/Spall/ Patched Area	EA		4	3		7
1090	Exposed Rebar	EA		7	1		8
1110	Cracking (PSC)	EA		23			23
1120	Efflorescence/Rust Staining	EA			1		1
6000	Scour	EA			12		12
234	Reinforced Concrete Pier Cap	LF	640	135	0	0	775
1080	Delamination/Spall/ Patched Area	LF		1			1
1120	Efflorescence/Rust Staining	LF		1			1
1130	Cracking (RC and Other)	LF		133			133
824	Reinforced Concrete Wingwall	EA	1	0	1	0	2
6000	Scour	EA			1		1
JOINTS							
304	Open Expansion Joint	LF	280	0	0	0	280
845	Joint Effectiveness	EA	7	0	0	0	7
SLOPE AND	CHANNEL ELEMENTS						
854	Channel	EA	0	1	0	0	1
6000	Scour	EA		1			1





6.11 Trestle ENB, FIB-NB, & FNB Element Level Data

A hands-on inspection was performed in FY2023, and element level data is presented in this section. An underwater inspection was performed on the entire trestle in FY2020, and element level data has been provided for all piles that were inspected along the trestle.

Table 61: ENB, FIB-NB, & FNB Element Level Data

	ENB, FIB-	NB, an	d FNB (101	17) Elemen	t Level Data	a	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS ANI	D SLABS						
12	Reinforced Concrete Deck	SF	17,479	921	0	0	18,400
1130	Cracking (RC and Other)	SF		921			921
16	Reinforced Concrete Top Flange	SF	65,593	96	4	0	65,693
1080	Delamination/Spall/ Patched Area	SF		37	4		41
1090	Exposed Rebar	LF		2			2
1130	Cracking (RC and Other)	SF		57			57
RAILINGS							
330	Metal Bridge Railing	LF	4,185	15	5	0	4,205
1080	Delamination/Spall/ Patched Area	LF		2	1		3
1090	Exposed Rebar	LF			4		4
1130	Cracking (RC and Other)	LF		7			7
7000	Damage	LF		6			6
SUPERSTR	UCTURE						
107	Steel Open Girder/Beam	LF	1,516	320	0	0	1,836
1000	Corrosion	LF		320			320
811	Steel Open Beam/Girder End	EA	0	8	0	0	8
1000	Corrosion	EA		8			8





	ENB, FIB-	NB, an	nd FNB (101	17) Elemen	t Level Data	a		
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
109	Prestressed Concrete Open Girder/Beam	LF	9,729	122	3	0	9,854	
1080	Delamination/Spall/ Patched Area	LF		108	2		110	
1090	Exposed Rebar	LF		12	1		13	
1100	Exposed Prestressing	LF		1			1	
1110	Cracking (PSC)	LF		1			1	
811	Beam/Girder End	EA	25	45	2	0	72	
1080	Delamination/Spall/ Patched Area	EA		39	2		41	
1090	Exposed Rebar	EA		5			5	
1110	Cracking (PSC)	EA		1			1	
BEARINGS								
310	Elastomeric Bearing	EA	204	0	0	0	204	
311	Movable Bearing	EA	0	8	0	0	8	
1000	Corrosion	EA		8			8	
313	Fixed Bearing	EA	0	8	0	0	8	
1000	Corrosion	EA		8			8	
SUBSTRUC	TURE	•						
215	Reinforced Concrete Abutment	LF	80	0	0	0	80	
226	Prestressed Concrete Pile	EA	0	60	5	0	65	
1080	Delamination/Spall/ Patched Area	EA		1	2		3	
1090	Exposed Rebar	EA		2			2	
1110	Cracking (PSC)	EA		52			52	
1120	Efflorescence	EA		4	3		7	
1190	Abrasion/Wear`	EA		1			1	
234	Reinforced Concrete Pier Cap	LF	686	45	1	0	732	
1080	Delamination/Spall/ Patched Area	LF			1		1	
1130	Cracking (RC and Other)	LF		45			45	





	ENB, FIB-	NB, an	nd FNB (101	17) Elemen	t Level Data	a	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
824	Reinforced Concrete Wingwall	EA	4	0	0	0	4
JOINTS							
304	Open Expansion Joint	LF	320	0	0	0	320
305	Assembly Joint without Seal	LF	80	0	0	0	80
845	Joint Effectiveness	EA	10	0	0	0	10
APPROACH	I SLABS						
321	Reinforced Concrete Approach Slab	SF	1,440	0	0	0	1,440
WEARING SURFACES, PROTECTIVE COATINGS, AND CONCRETE REINFORCEMENT STEEL PROTECTIVE SYSTEMS							
510	Wearing Surfaces	SF	59,124	0	0	0	59,124
515	Steel Protective Coating	SF	30,699	101	0	0	30,800
3440	Effectiveness (Steel Protective Coatings)	SF		101			101
886	Beam/Girder End Protective Coating - Steel	EA	0	8	0	0	8
3440	Effectiveness (Steel Protective Coatings)	EA		8			8
SLOPE ANI	D CHANNEL ELEMENTS						
853	Protected Slope Riprap	EA	2	0	0	0	2
854	Channel	EA	0	1	0	0	1





6.12 Trestle ESB, FIB-SB, & FSB Element Level Data

A hands-on inspection was performed in FY2023, and an underwater inspection was performed in FY2022, and element level data is presented in this section.

Table 62: ESB, FIB-SB, & FSB Element Level Data

	ESB, FIB-	SB, an	d FSB (100	7) Element	Level Data	1	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
DECKS ANI	D SLABS						
12	Reinforced Concrete Deck	SF	13,803	1,060	0	0	14,863
1130	Cracking (RC and Other)	SF		1,060			1,060
16	Reinforced Concrete Top Flange	SF	53,809	122	17	0	53,948
1080	Delamination/Spall/ Patched Area	SF		26			26
1090	Exposed Rebar	SF			2		2
1120	Efflorescence/Rust Staining	SF		82	15		97
1130	Cracking (RC and Other)	SF		14			14
RAILINGS							
330	Metal Bridge Railing	LF	4,139	73	1	0	4,213
1080	Delamination/Spall/ Patched Area	LF		8	1		9
1130	Cracking (RC and Other)	LF		62			62
7000	Damage	LF		3			3
SUPERSTR	UCTURE						
107	Steel Open Girder/Beam	LF	0	1,817	3	0	1,820
1000	Corrosion	LF		1,813	3		1,816
1900	Distortion	LF		4			4
811	Steel Open Beam/Girder End	EA	0	8	0	0	8
1000	Corrosion	EA		8			8





	ESB, FIB-	SB, an	d FSB (100	7) Element	Level Data	ı	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
109	Prestressed Concrete Open Girder/Beam	LF	12,595	546	71	0	13,212
1080	Delamination/Spall/ Patched Area	LF		269	28		297
1090	Exposed Rebar	LF		2	6		8
1100	Exposed Prestressing	LF		2	32		34
1110	Cracking (PSC)	LF		271	4		275
1120	Efflorescence/Rust Staining	LF		2	1		3
811	Prestressed Concrete Beam/Girder End	EA	203	145	4	0	352
1080	Delamination/Spall/ Patched Area	EA		2			2
1090	Exposed Rebar	EA		1	1		2
1100	Exposed Prestressing	EA			1		1
1110	Cracking (PSC)	EA		140	2		142
1120	Efflorescence/Rust Staining	EA		2			2
BEARINGS							
310	Elastomeric Bearing	EA	320	32	0	0	352
2230	Bulging, Splitting, or Tearing	EA		32			32
311	Movable Bearing	EA	0	12	0	0	12
1000	Corrosion	EA		12			12
313	Fixed Bearing	EA	0	4	0	0	4
1000	Corrosion	EA		4			4
SUBSTRUC	TURE						
215	Reinforced Concrete Abutment	LF	62	3	0	0	65
1130	Cracking (RC and Other)	LF		3			3





	ESB, FIB-	SB, an	d FSB (100	7) Element	: Level Data	ı	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
226	Prestressed Concrete Pile	EA		73	13	0	86
1080	Delamination/Spall/ Patched Area	EA		2	1		3
1090	Exposed Rebar	EA		11	4		15
1110	Cracking (PSC)	EA		59	2		61
1120	Efflorescence/Rust Staining	EA		1	6		7
234	Reinforced Concrete Pier Cap	LF	556	239	1	0	796
1080	Delamination/Spall/ Patched Area	LF		1			1
1120	Efflorescence/Rust Staining	LF		7	1		8
1130	Cracking (RC and Other)	LF		231			231
824	Reinforced Concrete Wingwall	EA	4	0	0	0	4
JOINTS							
304	Open Expansion Joint	LF	689	30	0	0	719
2360	Adjacent Deck or Header	LF		30			30
305	Assembly Joint without Seal	LF	57	8	0	0	65
2370	Metal Deterioration or Damage	LF		8			8
845	Joint Effectiveness	EA	24	0	0	0	24
APPROACE							
321	Reinforced Concrete Approach Slab	SF	1,344	0	0	0	1,344
	SURFACES, PROTECTIVE (VE SYSTEMS	COATIN	GS, AND CON	ICRETE REINF	ORCEMENT S	TEEL	
510	Wearing Surfaces	SF	45,691	550	0	0	46,241
3220	Crack (Wearing Surface)	SF		259			259
3230	Effectiveness (Wearing Surface)	SF		291			291

1 | 1





	ESB, FIB-	SB, an	d FSB (100	7) Element	Level Data	ı	
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
515	Steel Protective Coating	SF	26,344	6,193	0	0	32,537
3440	Effectiveness (Steel Protective Coatings)	SF		6,193			6,193
520	Concrete Reinforcement Steel Protective System	SF	5,524	0	0	63	5,587
3540	Effectiveness (Concrete Protective Coatings)	SF				63	63
521	Concrete Protective Coating	SF	112	63	0	0	175
886	Beam/Girder End Protective Coating - Steel	EA	0	8	0	0	8
3440	Effectiveness (Steel Protective Coatings)	EA		8			8
SLOPE AND	CHANNEL ELEMENTS						
853	Protected Slope Riprap	EA	2	0	0	0	2
854	Channel	EA	1	0	0	0	1





7. Tunnel Inspection

For general information regarding the District's Tunnels, please see the document entitled, Tunnel Description and Inspection Procedures for Chesapeake Channel Tunnels, a Reference Document for NTIS Inspection. The information presented here will provide an overview of who performed the work and what was accomplished.

7.1 Tunnel Baseline Data

		Inspection Frequency:	24 Months
Tunnel Number	CBBT00000000002	Tunnel Name	Chesapeake Channel Tunnel
County/City:	131-Northampton	Facility Carried:	US 13
Route Number:	00013	Location:	9.8 miles North of US Route 60
Lead Inspector:		Latitude:	37.03640000
Reviewer:		Longitude:	76.07680000

		Inspection Frequency:	24 Months
Tunnel Number:	CBBT0000000001R	Tunnel Name:	Thimble Shoal Channel Tunnel
County/City:	131-Northampton	Facility Carried:	US 13
Route Number:	00013	Location:	4.5 miles North of US Route 60
Lead Inspector:		Latitude:	36.96619400
Reviewer:		Longitude:	76.11261100

Signature of NTIS	William R. Bol
Team Leader:	William Bolt, P.E.*

Signature of	
Reviewer:	Jousha Hill, P.E.

^{*} I have read and followed the CBBT District Inspection Procedures. I have received and reviewed the spreadsheets of major preventative maintenance processes performed and viewed as necessary the Island Operator's log sheets.





7.2 NTIS Inspection Team

Tunnel Inspection Team*		
Name	Position	
William Bolt, P.E.	NTIS Team Leader	
Thomas Burns, P.E.	NTIS Structural Inspector	
Jaden Brajkovich, E.I.T.	NTIS Structural Inspector	
Kyle Gable, P.E.	NTIS Electrical/ Mechanical Inspector	
Alexander Waardenburg, P.E.	NTIS Electrical/ Mechanical Inspector	
Lee Lentz, P.E.	Independent Reviewer	

^{*}Inspection credentials can be provided if requested.

7.3 Summary of Findings

The Chesapeake Channel Tunnel elements were generally in good to very good condition. There was one area of concern observed during the structural inspection. The structural inspection found a delamination in the ceiling slab at Station 504+80 in the Southbound lane, near the centerline of the roadway. This area was previously noted; however, it appeared to have deteriorated further since the previous inspection, separating more from ceiling slab. The affected/unsound area was approximately 3-0" long x 2-0" wide. The immediately loose tiles and grout were removed by the inspectors. The larger portion of the grout bed/ceiling slab that was separated was unable to be removed with inspection tools, demonstrating that it was still well attached to the ceiling. The Maintenance Director was informed within 24 hours, and the unsound material was removed within a week. See Section 3.2.2 for routine repair recommendations.

The electrical tunnel elements were generally in good to very good condition and there were no major findings. The electrical distribution system has been well maintained and has reached an age at which finding experts and parts to repair the aged components may become a challenge. It is notable that the maintenance personnel have been able to perform many repairs in house. The tunnel lighting had been replaced with LEDs and the new fixtures and mounting hardware is in very good condition. The control system is being replaced with an upgraded system that would allow greater monitoring and control. The replacement of the control systems at the portals were almost complete and the systems at the low point pumps were planned to be replaced. The new motor starters were in excellent condition. See Section 4.2.2 for routine repair recommendations.

The Chesapeake Channel Tunnel mechanical tunnel elements were generally in good to very good condition and there were no major findings. Painting was being performed regularly to protect the elements from the corrosive environment, lubrication was being performed regularly, and maintenance was being performed regularly. The portal building elevators are in the process of being replaced. See Section 4.2.2 for routine repair recommendations.





8. Tunnel Inventory Items

The tables provided in this section reflect the current tunnel inventory data coded per the Specifications for the National Tunnel Inventory (SNTI) at the time of this inspection.

8.1 TSCT Inventory Items

Table 63: TSCT Identification Items

TSCT Identification Items			
l.1	Tunnel Number	CBBT000000001R	AN15
1.2	Tunnel Name	Thimble Shoal Tunnel	AN100
1.3	State Code	51	N (2,0)
1.4	County Code	131	N (3,0)
1.5	Place Code	12808	N (5,0)
1.6	Highway Agency District	HR	AN2
1.7	Route Number	00013	AN5
1.8	Route Direction	0	N (1,0)
1.9	Route Type	2	N (1,0)
I.10	Facility Carried	US13	AN100
l.11	LRS Route ID	00000001300	AN120
I.12	LRS Mile Point	43.059	N (8,3)
I.13	Tunnel Portal's Latitude	0000000036.96619400	N (11,8)
1.14	Tunnel Portal's Longitude	0000000076.11261100	N (11,8)
l.15	Border Tunnel State or Country Code		AN2
I.16	Border Tunnel Financial Responsibility		N (3,0)
I.17	Border Tunnel Number		AN15
I.18	Border Tunnel Inspection Responsibility		N (1,0)

Table 64: TSCT Age and Service Items

TSCT Age and Service Items			
A.1	Year Built	1964	N (4,0)
A.2	Year Rehabilitated	2014	N (4,0)
A.3	Total Number of Lanes	02	N (2,0)
A.4	Annual Average Daily Traffic	011650	N (6,0)
A.5	Annual Average Daily Truck Traffic	001065	N (6,0)
A.6	Year of Average Daily Traffic	2021	N (4,0)
A.7	Detour Length	425	N (3,0)



TSCT Age and Service Items			
A.8	Service in Tunnel	1	N (1,0)

Table 65: TSCT Classification Items

TSCT Cla	TSCT Classification Items			
C.1	Owner	32	N (2,0)	
C.2	Operator	32	N (2,0)	
C.3	Direction of Traffic	2	N (1,0)	
C.4	Toll	2	N (1,0)	
C.5	NHS Designation	1	N (1,0)	
C.6	STRAHNET Designation	1	N (1,0)	
C.7	Functional Classification	2	N (1,0)	
C.8	Urban Code	90892	N (5,0)	

Table 66: TSCT Geometric Data Items

TSCT Geometric Data Items			
G.1	Tunnel Length	005738	N (6,0)
G.2	Minimum Vertical Clearance over Tunnel Roadway	00013.5	N (5,1)
G.3	Roadway Width, Curb-to-Curb	0024.0	N (4,1)
G.4	Left Sidewalk Width	002.5	N (3,1)
G.5	Right Sidewalk Width	000.0	N (3,1)

Table 67: TSCT Inspection Items

TSCT Inspection Items			
D.1	Routine Inspection Target Date	10012021	D
D.2	Actual Routine Inspection Date	09162021	D
D.3	Routine Inspection Interval	24	N (2,0)
D.4	In-Depth Inspection	0	N (1,0)
D.5	Damage Inspection	0	N (1,0)
D.6	Special Inspection	0	N (1,0)



Table 68: TSCT Load Rating and Posting Items

TSCT Load Rating and Posting Items			
L.1	Load Rating Method	1	AN1
L.2	Inventory Load Rating Factor	0.63	N (4,2)
L.3	Operating Load Rating Factor	1.05	N (4,2)
L.4	Tunnel Load Posting Status	A	AN1
L.5	Posting Load - Gross		N (2,0)
L.6	Posting Load - Axle		N (2,0)
L.7	Posting Load - Type 3		N (2,0)
L.8	Posting Load - Type 3S2		N (2,0)
L.9	Posting Load - Type 3-3		N (2,0)
L.10	Height Restriction	1	N (1,0)
L.11	Hazardous Material Restriction	1	N (1,0)
L.12	Other Restrictions	1	N (1,0)

Table 69: TSCT Navigation Items

TSCT Navigation Items			
N.1	Under Navigable Waterway	1	N (1,0)
N.2	Navigable Waterway Clearance	055.0	N (3,1)
N.3	Tunnel or Portal Island Protection from Navigation	1	N (1,0)

Table 70: TSCT Structure Type and Materials Items

TSCT Structure Type and Material Items			
S.1	Number of Bores	1	N (1,0)
S.2	Tunnel Shape	4	N (1,0)
S.3	Portal Shapes	3	N (1,0)
S.4	Ground Conditions	1	N (1,0)
S.5	Complex	1	N (1,0)





8.2 CCT Inventory Items

Table 71: CCT Identification Items

CCT Iden	CCT Identification Items			
l.1	Tunnel Number	CBBT00000000002	AN15	
1.2	Tunnel Name	Chesapeake Channel Tunnel	AN100	
1.3	State Code	51	N (2,0)	
1.4	County Code	131	N (3,0)	
1.5	Place Code	12808	N (5,0)	
1.6	Highway Agency District	HR	AN2	
1.7	Route Number	00013	AN5	
1.8	Route Direction	0	N (1,0)	
1.9	Route Type	2	N (1,0)	
I.10	Facility Carried	US13	AN100	
l.11	LRS Route ID	00000001300	AN120	
I.12	LRS Mile Point	37.319	N (8,3)	
I.13	Tunnel Portal's Latitude	0000000037.03638900	N (11,8)	
1.14	Tunnel Portal's Longitude	0000000076.07680600	N (11,8)	
l.15	Border Tunnel State or Country Code		AN2	
l.16	Border Tunnel Financial Responsibility		N (3,0)	
l.17	Border Tunnel Number		AN15	
I.18	Border Tunnel Inspection Responsibility		N (1,0)	

Table 72: CCT Age and Service Items

CCT Age and Service Items			
A.1	Year Built	1964	N (4,0)
A.2	Year Rehabilitated	2019	N (4,0)
A.3	Total Number of Lanes	02	N (2,0)
A.4	Annual Average Daily Traffic	011650	N (6,0)
A.5	Annual Average Daily Truck Traffic	001065	N (6,0)
A.6	Year of Average Daily Traffic	2021	N (4,0)
A.7	Detour Length	425	N (3,0)
A.8	Service in Tunnel	1	N (1,0)



Table 73: CCT Classification Items

CCT Classification Items			
C.1	Owner	32	N (2,0)
C.2	Operator	32	N (2,0)
C.3	Direction of Traffic	2	N (1,0)
C.4	Toll	2	N (1,0)
C.5	NHS Designation	1	N (1,0)
C.6	STRAHNET Designation	1	N (1,0)
C.7	Functional Classification	2	N (1,0)
C.8	Urban Code	90892	N (5,0)

Table 74: CCT Geometric Data Items

CCT Geo	CCT Geometric Data Items								
G.1	Tunnel Length	005424	N (6,0)						
G.2	Minimum Vertical Clearance over Tunnel Roadway	00013.5	N (5,1)						
G.3	Roadway Width, Curb-to-Curb	0024.0	N (4,1)						
G.4	Left Sidewalk Width	002.5	N (3,1)						
G.5	Right Sidewalk Width	000.0	N (3,1)						

Table 75: CCT Inspection Items

CCT Inspection Items								
D.1	Routine Inspection Target Date	10012024	D					
D.2	Actual Routine Inspection Date	09192024	D					
D.3	Routine Inspection Interval	24	N (2,0)					
D.4	In-Depth Inspection	0	N (1,0)					
D.5	Damage Inspection	0	N (1,0)					
D.6	Special Inspection	0	N (1,0)					



Table 76: CCT Load Rating and Posting Items

CCT Load	CCT Load Rating and Posting Items							
L.1	Load Rating Method	1	AN1					
L.2	Inventory Load Rating Factor	0.63	N (4,2)					
L.3	Operating Load Rating Factor	1.05	N (4,2)					
L.4	Tunnel Load Posting Status	A	AN1					
L.5	Posting Load - Gross		N (2,0)					
L.6	Posting Load - Axle		N (2,0)					
L.7	Posting Load - Type 3		N (2,0)					
L.8	Posting Load - Type 3S2		N (2,0)					
L.9	Posting Load - Type 3-3		N (2,0)					
L.10	Height Restriction	1	N (1,0)					
L.11	Hazardous Material Restriction	1	N (1,0)					
L.12	Other Restrictions	1	N (1,0)					

Table 77: CCT Navigation Items

CCT Navigation Items							
N.1	Under Navigable Waterway	1	N (1,0)				
N.2	Navigable Waterway Clearance	055.0	N (3,1)				
N.3	Tunnel or Portal Island Protection from Navigation	1	N (1,0)				

Table 78: CCT Structure Type and Material Items

CCT Stru	CCT Structure Type and Material Items							
S.1	Number of Bores	1	N (1,0)					
S.2	Tunnel Shape	4	N (1,0)					
S.3	Portal Shapes	3	N (1,0)					
S.4	Ground Conditions	1	N (1,0)					
S.5	Complex	1	N (1,0)					





9. Tunnel Element Level Data

The tables provided in this section display the tunnel elements and condition states coded per the Specifications for the National Tunnel Inventory (SNTI) at the time of this inspection.





9.1 Thimble Shoal Channel Tunnel - Element Level Data

A hands-on inspection was performed in FY2024, and element level data is presented in this section.

Table 79: TSCT Element Level Data

TSCT Element Level Data										
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total			
STRUCTUR	STRUCTURAL SECTION									
10002	Precast Concrete Tunnel Liner	SF	610,787	1,040	1,073	0	612,900			
10051	Concrete Portal	SF	3,256	20	10		3,286			
10061	Concrete Ceiling Slab	SF	149,988	178	19		150,185			
10080	Steel Hangers and Anchorages	EA	469				469			
10101	Concrete Invert Slab	SF	118,056	17,010	2,646		137,712			
10132	Compression Joint Seal	LF	184				184			
CIVIL SECT	ION									
10158	Asphalt Wearing Surface	SF	128,627				128,627			
10161	Concrete Traffic Barrier	LF	11,475	1			11,476			
10170	Steel Pedestrian Railing	LF	0	5,710	28		5,738			
10950	Steel Corrosion Protective Coating	SF		6,422		714	7,136			
MECHANIC	AL SYSTEMS SECTION									
10200	Ventilation System	EA	0	2			2			
10201	Fans	EA	0	11	1		12			
10300	Drainage and Pumping System	EA	3				3			
10301	Pumps	EA	13				13			
10400	Emergency Generator System	EA	2				2			





TSCT Element Level Data								
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total	
ELECTRICAL AND LIGHTING SYSTEMS SECTION								
10500	Electrical Distribution System	EA	0	1			1	
10550	Emergency Distribution System	EA	2				2	
10600	Tunnel Lighting Systems	EA	2				2	
10601	Tunnel Lighting Fixture	EA	1,452	1			1,453	
10620	Emergency Lighting System	EA	2				2	
FIRE/LIFE S	SAFETY/SECURITY SYSTE	MS SEC	CTION					
10650	Fire Detection System	EA	1				1	
10700	Fire Protection System	EA	1				1	
10750	Emergency Communication System	EA	1				1	
10800	Tunnel Operations and Security System	EA	1				1	
SIGNS SEC	TION							
10850	Traffic Sign	EA	19				19	
10910	Lane Signal	EA	2				2	





9.2 Chesapeake Channel Tunnel - Element Level Data

A hands-on inspection was performed in FY2025, and element level data is presented in this section.

Table 80: CCT Element Level Data

CCT Element Level Data									
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total		
STRUCTURAL SECTION									
10002	Precast Concrete Tunnel Liner	SF	574,936	2,254	1,348	769	579,307		
10051	Concrete Portal	SF	3,246	20	20		3,286		
10061	Concrete Ceiling Slab	SF	140,338	451	222		141,011		
10080	Steel Hangers and Anchorages	EA	440		6		446		
10101	Concrete Invert Slab	SF	95,481	33,932	696	55	130,164		
10132	Compression Joint Seal	LF	184				184		
CIVIL SECT	ION								
10158	Asphalt Wearing Surface	SF	120,669	908			121,577		
10161	Concrete Traffic Barrier	LF	10,742	6	99		10,847		
10170	Steel Pedestrian Railing	LF	4,881	543			5,424		
10950	Steel Corrosion Protective Coating	SF		6070		674	6,744		
MECHANIC	AL SYSTEMS SECTION								
10200	Ventilation System	EA	2				2		
10201	Fans	EA	12				12		
10300	Drainage and Pumping System	EA	3				3		
10301	Pumps	EA	11	1	1		13		
10400	Emergency Generator System	EA	2				2		





CCT Element Level Data							
Element Number	Element Name	Unit	Condition State 1	Condition State 2	Condition State 3	Condition State 4	Total
ELECTRICA	L AND LIGHTING SYSTEM	IS SECT	ION				
10500	Electrical Distribution System	EA	1				1
10550	Emergency Distribution System	EA	2				2
10600	Tunnel Lighting Systems	EA	2				2
10601	Tunnel Lighting Fixture	EA	1,380	2			1,382
10620	Emergency Lighting System	EA	2				2
FIRE/LIFE S	SAFETY/SECURITY SYSTE	MS SEC	TION				
10650	Fire Detection System	EA	1				1
10700	Fire Protection System	EA	2				2
10750	Emergency Communication System	EA	1				1
10800	Tunnel Operations and Security System	EA	1				1
SIGNS SEC	TION						
10850	Traffic Sign	EA	3				3
10870	Egress Sign	EA	67				67





10. Photographs

10.1 Trestle ANB Photographs



Photo 1: ASB188 Asphalt Wearing Surface – Typical good condition of wearing surface.



Photo 2: ANB181 Metal Bridge Railing – Typical good condition of bridge railing and curb.



Photo 3: ANB183 Open Expansion Joint – spall 2' L x 1" w over expansion joint with $\frac{1}{4}$ " transverse crack full width (typical).



Photo 4: ANB177 – Typical Superstructure & Substructure in good condition.





Photo 5: ANB181 – Pile B – Jacket missing full circumference bottom 8' with missing grout.



Photo 6: ANB182 – Beam 2 at Bent 183: 2' long 1/8" wide crack on underside of beam.







Photo 7: ANB191 – Bent 192 at Bent 5, Bay 5 spall/delam 25" L x 2" H x 8"under x 1" deep with exposed reinforcement with up to 20% section loss.



Photo 8: ANB204.5 – Beam 8, Bay 7 at 1/3 span spall 30" L x 8" H x 1" D with 2 exposed prestressing strands (L1 and L2) with up to 20% section loss.

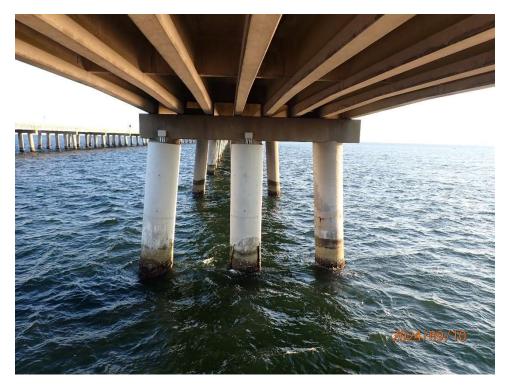


Photo 9: ANB202 – Substructure with fiberglass jackets . Jackets typically in good condition with isolated areas of jacket failure at waterline.





10.2 Trestle ASB Photographs



Photo 10: ASB50 Asphalt Wearing Surface – Typical Good Condition of Wearing Surface



Photo 11: ASB71 Bent 71 north face typical good condition. Bent 71 has detached conduit for navigation light at bent cap.



Photo 12: ASB67 typical substructure elevation. Substructure elements typically in good condition.





10.3 Trestle BNB Photographs



Photo 13: BNB160 – Asphalt wearing surface in good condition with pavement marking peeled and cracked.



Photo 14: BNB167 – up to 1/8" wide transverse crack at expansion joint full width of roadway.



Photo 15: BNB165 – west curb with vertical cracks up to 1/8" wide.



Photo 16: BNB178 – superstructure and substructure in good condition.





Photo 17: BNB199 – Pile A; 12" H x 6" W x 1" Deep spall with exposed reinforcement with up to 10% section loss.



Photo 18: BNB192 Beam 5, Bay 5 at 1/3 span – spall 28" L x 4" H x 5" Under x 1-1/2" D with 1 exposed prestressed strand with 90% section loss and Bay 4 – spall 24" L x 4" H x 3-1/2" Under x 1-1/2" D with 1 exposed prestressed strand with 50% section loss.







Photo 19: BNB177 under side of deck with previous beam repair noted on Beam 4.



Photo 20: BNB200 at Beam 2 on Bay 2 side, beam end delamination/spall 18" L x 1' H x 6" back x 2" deep with exposed reinforcement with up to 50% loss of section at beam end and partial bearing loss.





10.4 Trestle BSB Photographs



Photo 21: BSB100 Deck Wearing Surface in good condition and inspection equipment mobilized.



Photo 22: Superstructure in good condition.



Photo 23: BSB96 Substructure in good condition.





10.5 Trestle CNB Photographs



Photo 24: CNB199 Asphalt Wearing Surface – Typical Good Condition of Wearing Surface.



Photo 25: CNB201 Railing typically in good condition with minor impact damage at isolated locations.







Photo 26: CNB203 Curb typically in good condition with minor cracking noted on face of curb at isolated locations.

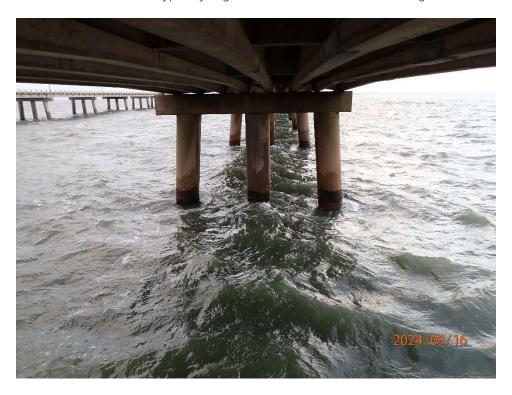


Photo 27: CNB198 Superstructure & Substructure in good condition.







Photo 28: CNB197 Bay 7 typical underside of deck with exposed chairs at isolated areas.



Photo 29: CNB213 Superstructure & Substructure in good condition.



Photo 30: CNB206 Bent 207 Beam 5, Bay $4 - \text{spall } 30^{\circ} \text{ L x } 6^{\circ} \text{ H x } 5^{\circ} \text{ under x } 1^{\circ} \text{ deep with exposed reinforcement with up to } 100\% \text{ section loss.}$



Photo 31: CNB197 Beam 4, Bay 3 1/3 span – spall 30" L x 5" H x 5" under x 1-1/2" deep with exposed prestressing strands with L1 having 10% loss of section.





10.6 Trestle CSB Photographs



Photo 32: CSB97 Top of deck in good condition.



Photo 33: CSB90 Metal Bridge Railing-Typical Good Condition of Bridge Railing.



Photo 34: CSB91 Superstructure and Substructure in good condition.



Photo 35: CSB90 Prestressed Girder 1 spall with exposed prestressing (L1) and reinforcement. Spall measures 2' L \times 7" H \times 2" under \times 1-1/2" deep with 20% section loss to prestressing strand and reinforcement.





10.7 NCB-NB Photographs

Not inspected this year, will be inspected in FY2026





10.8 NCB-SB Photographs



Photo 36: NCB-SB Deck and Wearing Surface – General Good Condition (Looking South)



Photo 37: Modular Joint Seal – Typical Good Condition of Joint and Adjacent Deck Header, Pier 10 Shown (Looking West)







Photo 38: Superstructure – General Condition of Steel Superstructure



Photo 39: Superstructure – General Condition of Steel Superstructure in fair condition.



Photo 40: Substructure – General Condition of Pier Substructure



Photo 41: Fender System – Typical Condition of Pier Footing and Fender System.







Photo 42: Superstructure – Blistering paint and corrosion on bolted connections.



Photo 43: NCB-SB5 Bearings – Typical Peeling Paint with Surface Corrosion at Bearings (Looking Southeast)







Photo 44: NCB-SB – Typical Peeling Paint with Surface Corrosion and mushrooming of nuts on flange bolts.



Photo 45: Fender System – Typical Minor Corrosion of Chain between Pier 9 Footing and Fender System (Looking South)





10.9 Trestle DNB Photographs



Photo 46: DNB Wearing Surface - General Good Condition of Deck and Wearing Surface (Looking South)



Photo 47: DNB2 Metal Bridge Railing & Expansion Joint - General Good Condition of Railing (Looking Northwest) and transverse crack over expansion joint.







Photo 48: Superstructure – Typical condition of Superstructure with steel corrosion noted on up to 30% of surface area. Bearings in different degrees of expansion/contraction. Span 1 looking South.



Photo 49: Superstructure and Substructure – Typical condition of Superstructure in Span 1 looking North.





Photo 50: DNB-4 G2, Beam 2 Bay midspan 3' long x 6" high x 4" under with 1 exposed prestressing strand (L1) up to 50% section loss.



Photo 51: Superstructure & Substructure – Typical Good Condition of Beam and Piles (Span 5), Looking North.





Photo 52: Typical Good Repair Patch on bottom of girder in Span 6 in 2 locations. Looking North.



Photo 53: Prestressed Concrete Girders, DNB-7 G5, Beam 5 midspan 3' long x 5" high x 5" under x 2" deep with 1 exposed prestressing strand up to 100% section loss.



Photo 54: West wingwall undermining up to 1' wide x 6" high x 4' under.





10.10 Trestle DSB Photographs



Photo 55 – DSB2 Deck in good condition with up to 1/16" wide cracking at isolated locations.



Photo 56 – DSB20 Superstructure in good condition. (Typical)







Photo 57 – DSB9 Bent in good condition (typical)



Photo 58: DSB20 Bent 20 Beam 5 Bay 4 with beam end spall; spall up to 6" H x 6" L x 4" Deep with exposed reinforcement. Spalls noted at isolated locations.





10.11 Trestle ENB, FIB-NB, & FNB Photographs



Photo 59: FNB2 - Wearing surface in good condition.



Photo 60: Elevation view of FIBNB and FIBSB.







Photo 61: ENB4 Substructure in good condition.

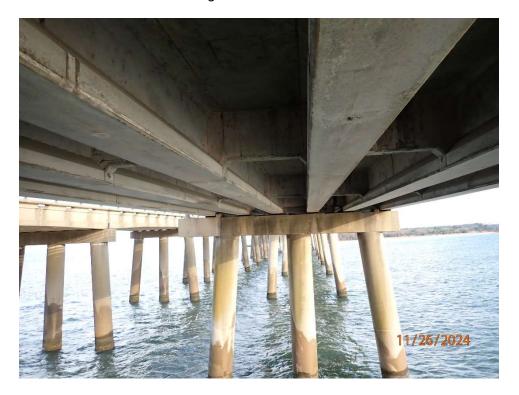


Photo 62: ENB5 Superstructure in good condition.







Photo 63: FIBNB1 Superstructure in good condition with isolated coating loss and surface corrosion.



Photo 64: FIBNB4 G4 beam end spall 5" L x 8" H with up to 5% section loss on bearing area.







Photo 65: FINB1 Steel Superstructure at splice plate with blistering paint with surface corrosion.



Photo 66: FNB6 Substructure in good condition.

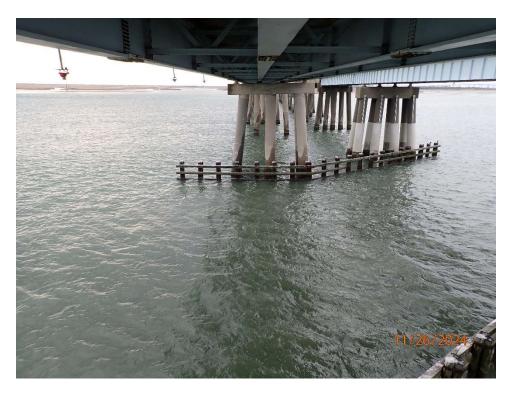


Photo 67: FINB2 Substructure and fender system in small boat channel.





10.12 Trestle ESB, FIB-SB, & FSB Photographs



Photo 68: FSB4 Wearing surface in good condition.



Photo 69: FIBSB2 Railing in good condition and navigation lights with light surface rust.







Photo 70: Abutment – drainage swale on west end settled and disconnected.



Photo 71: FSB4 Superstructure and substructure in good condition.

Photo 72: 531:





Photo 73: FIBSB Superstructure in good condition with isolated corrosion noted on bottom flange.



Photo 74: FSB8 Beam 1, Bay 1 spall 18" L \times 5" H \times 2" under \times 2" deep with 1 exposed prestressing strand with up to 10% section loss.



Photo 75: FIBSB3 Bearings have pack rust up to ¼" thick typical for bearings.

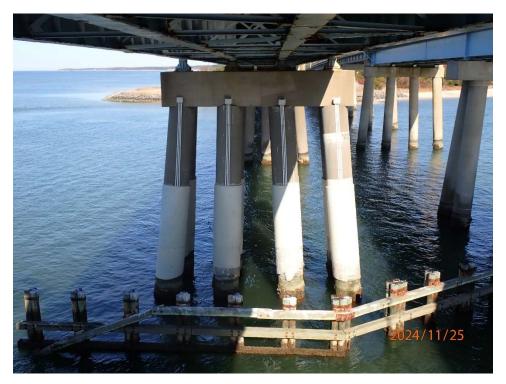


Photo 76: FIBSB Substructure with pile jacket and fender system with broken top wale and portions of lower wale within small navigation channel.





10.13 TSCT Photographs

Not inspected this year, will be inspected in FY2024





10.14 CCT Photographs

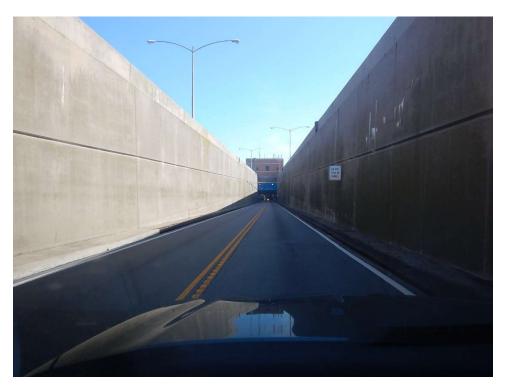


Photo 77 – Island 3 Open Approach Roadway (Looking North)

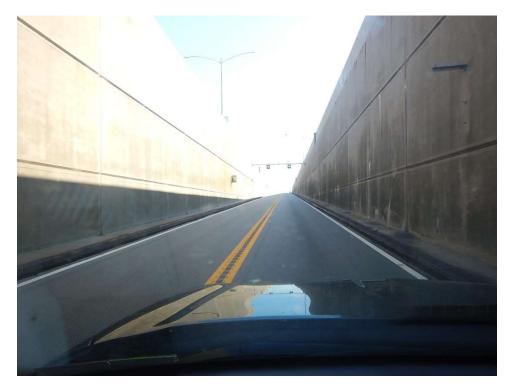


Photo 78 – Island 4 Open Approach Roadway (Looking North)



Photo 79: Tunnel Portal – General View of Island 3 Portal (Looking North)



Photo 80: Tunnel Portal – General View of the Island 3 Portal Spalling (Looking West)

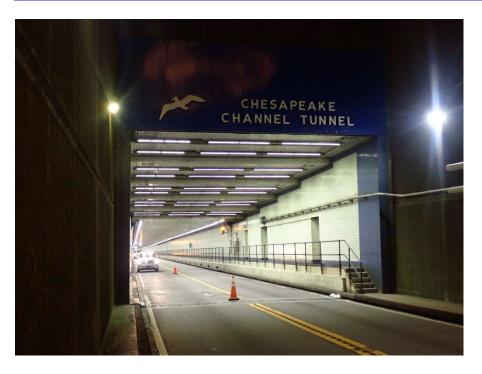


Photo 81: Tunnel Portal – General View of Island 4 Portal (Looking South)

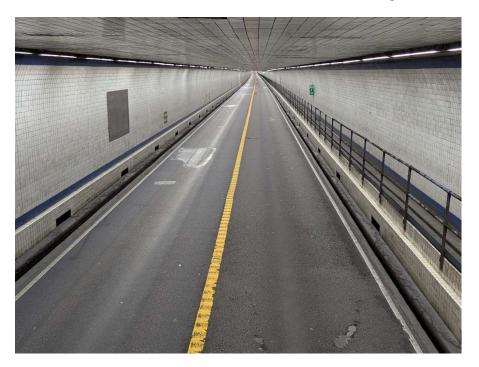


Photo 82: Tunnel Roadway – General View along Tunnel Roadway (Looking South)

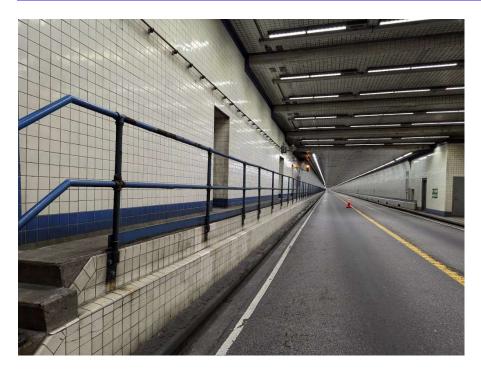


Photo 83: Tunnel Roadway – General View of the Concrete Barrier (Lined with Tile), Curb, Sidewalk, Pedestrian Railing, and Tunnel Liner Wall (Looking North)



Photo 84: Tunnel Ceiling Slab – General View of Random Cracked Tiles in Tunnel Roadway Ceiling (Looking West)



Photo 85: Tunnel Ceiling Slab – Isolated Area with More Significant Delaminated Concrete and Tile; and Missing Tiles in Tunnel Roadway Ceiling (Sta. 504+80 Looking South)



Photo 86: Fresh Air Duct – General View along Tunnel Liner and Invert (Looking North)



Photo 87: Fresh Air Duct – General View of Delamination and Spalling in the Tunnel Liner (Looking East)



Photo 88: Fresh Air Duct – General View of Cracking with Efflorescence in the Invert Slab (Looking East)



Photo 89: Exhaust Air Duct – General View along Tunnel Liner and Ceiling Slab (Looking North)



Photo 90: Exhaust Air Duct – General View of Random Transverse Cracking with Significant Efflorescence and Leakage in the Tunnel Liner (Looking West)



Photo 91: Exhaust Air Duct – General View of Random Spalling with Exposed and Corroded Reinforcement Steel in the Tunnel Liner (Looking West)



Photo 92: Fire Protection System – Typical Fire Extinguisher in Tunnel Roadway (Looking West)

FY2025 Facility Annual Inspection Report



Photo 93: Tunnel Lighting Fixture – Loose anchorage/mounting hardware at East Station 469+60 (Blue Arrow) (similar at East Station 469+30)





10.15 General Facility



Photo 94: Approach Roadway (North); Asphalt with alligator cracking up to ¼" wide throughout.



Photo 95: Approach Roadway (North) near FIB Bridge; Asphalt with alligator cracking up to 1/4" wide throughout.



Photo 96: Approach Roadway (South) near Trestle ASB; Asphalt with alligator cracking up to 1/4" wide throughout.



Photo 97: Approach Roadway (South) headed north approx. 675' south of gate; pothole and concrete pavement settled up to 1" with up to 14" wide transverse crack.







Photo 98: Approach Roadway (South); impact damage to guardrail pushed back up to 1' back (non CBBT property)

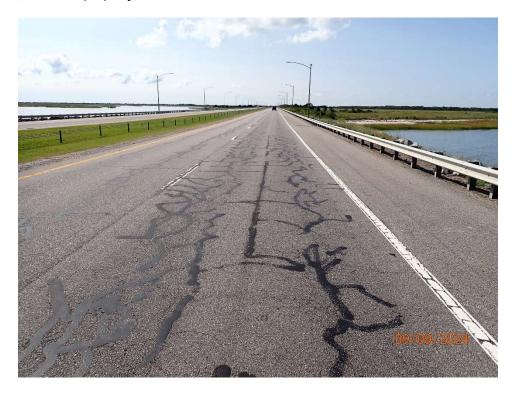


Photo 99: FIB Causeway looking south; Asphalt with up to 1/8" alligator cracking with majority sealed.



Photo 100: FIB Causeway headed south; Pothole/sinkhole at shoulder/guardrail with riprap. Sinkhole measures 42" wide x 24" long x 3" deep.



Photo 101: FIB Causeway headed north (looking south); Typical shoreline stabilization.





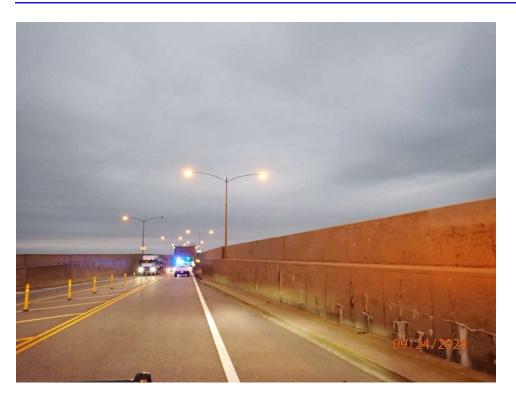


Photo 102: Portal Island 3 Approach Roadway and Approach Wall; vertical and transverse cracks in approach wall with efflorescence.



Photo 103: Portal Island 3 Approach Roadway; corrosion to rub rail throughout with isolated delaminations to sidewalk/curb.

CHESAPEAKE BAY BRIDGE-TUNNEL







Photo 104: Portal Island 3 Open Approach Wall (east face); spall with exposed reinforcement with up to 100% section loss. Spalls noted at isolated locations and typically in the top 8' of wall.

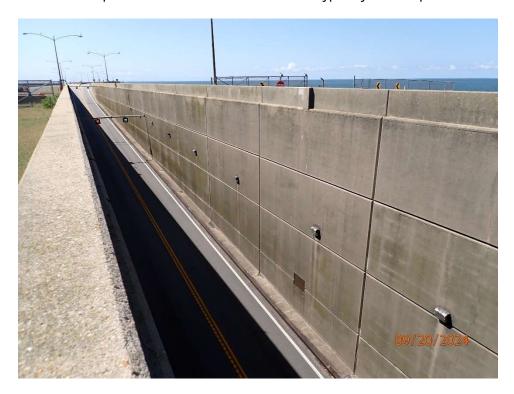


Photo 105: Portal Island 3 Open Approach Wall (west face); typical condition of approach wall.





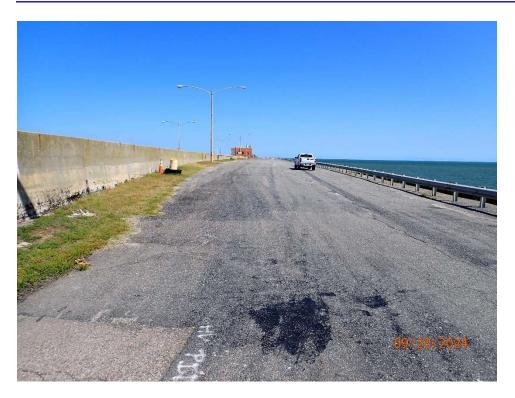


Photo 106: Portal Island 3; Surface; Typical pavement condition on east side of island.



Photo 107: Portal Island 3: Surface; Typical pavement condition on west side of island.







Photo 108: Portal Island 3; concrete light poles typically exhibit vertical cracking and spalls with exposed reinforcement.







Photo 109: Portal Island 4; Open Approach Roadway



Photo 110: Portal Island 4; Open Approach Walls; hairline crack with rust staining and minor efflorescence.



Photo 111: Portal Island 4: Open Approach Roadway and Walls; typical condition.



FY2025 Facility Annual Inspection Report moffatt & nichol



Photo 112: Portal Island 4 Splash Wall; Repair to previous sinkhole with urethane material out of joints.



Photo 113: Portal Island 4 Splash Wall; Repair to previous sinkhole with urethane material. Sinkhole is still present in roadway with void under asphalt at time of inspection. The District noted in December 2024 that repair was made to this location.







Photo 114: Portal Island 4 Splash Wall; Minor rotation to splash wall just south of sinkhole repair.

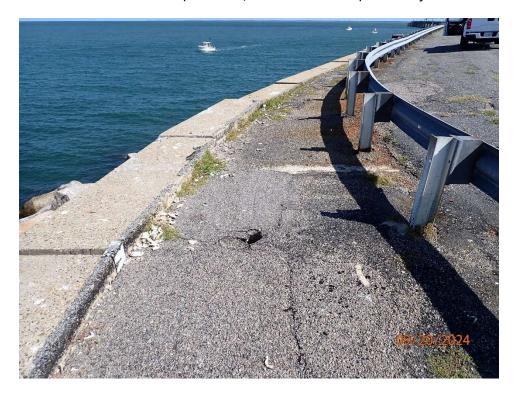


Photo 115: Portal Island 4 Splash Wall; newly forming sinkhole behind guardrail on west side of island. Depth of sinkhole could not be verified.







Photo 116: Portal Island 4 Surface; typical asphalt condition on portal island.



Photo 117: Portal Island 4 Concrete Light Pole; typical condition of poles with areas of spalls with exposed reinforcement up to 20% section loss of reinforcement.



Photo 118: South Toll Plaza; Toll Office Building



Photo 119: South Toll Plaza; Toll Office Building Roof in good condition with no notable defects.







Photo 120: South Toll Plaza; Toll Booth Plaza in good condition.



Photo 121: South Toll Plaza; Toll Booth Plaza in good condition.



Photo 122: South Toll Plaza; Garage in good condition.



Photo 123: South Toll Plaza Parking Lot; Pavement has isolated alligator cracking up to 1/8" wide; however, overall, in good condition.







Photo 124: North Toll Plaza, Toll Office Building



Photo 125: North Toll Plaza, Toll Office Building roof in good condition.







Photo 126 North Toll Plaza, Toll Office Building with water heater with minor leak.



Photo 127: North Toll Plaza, Toll Booth and Canopy with roadway approach in good condition.







Photo 128: North Toll Plaza, Toll Booth and Canopy with roof in good condition.



Photo 129: North Toll Plaza, Toll Booth and Canopy, isolated pothole 18" diameter x 2" deep.







Photo 130: North Toll Plaza, Administration Building in good condition.

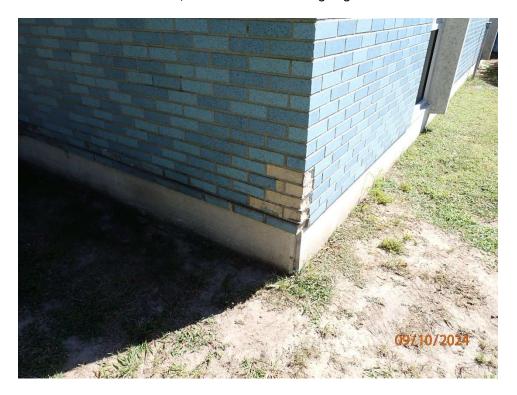


Photo 131: North Toll Plaza, Administration Building brick with minor deterioration at isolated locations. Overall, in good condition.







Photo 132: North Toll Plaza, Administration Building roof in good condition.



Photo 133: North Toll Plaza, Maintenance Building in overall in good condition.







Photo 134: North Toll Plaza, Garage & Generator Building in overall in good condition.



Photo 135: North Toll Plaza, Garage & Generator Building with interior in good condition with signs of minor leakage on backwall.







Photo 136: North Toll Plaza, Fence is in good condition with several post top for 3-wire barb off at isolated locations.



Photo 137: North Toll Plaza, Equipment Storage Building in very good condition. Minor damage noted to Bay 2 trim on north storage structure.



Photo 138: North Toll Plaza, Storage Yard and Parking in good condition with alligator cracking throughout up to $\frac{1}{2}$ wide. Isolated low areas with ponding water.



Photo 139: North Toll Plaza, Rest Room Facility in good condition with alligator cracking noted throughout the parking lot.





11. Bay Bottom Profiles

Original copies of Hydrographic Survey data are on file with CBBT.





12. Tunnel Inspection Back Up Data

SNTI Element	Component	Item Name, Description or Location	Condition / Operation	Inspection Findings	Detail	Thermal Reading °F	Condition State (1-4)	Photo	Maintenance or Repair Recommendation	Repair Priority	Date Inspected MM/DD/YYYY
10200 Ventilation System	Damper BSB1	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good							9/17/2024
10200 Ventilation System	Damper BSB1	Damper Chains	Condition	Well lubricated.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB1	Damper Drive	Condition	Good paint. Reducer is leaking oil from reducer manual drive shaft seal. Thin oil film on motor rear. Reducer oil level sight glass is opaque.			1		Replace reducer oil level sight glass		9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB1	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB2	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good							9/17/2024
10200 Ventilation System	Damper BSB2	Damper Chains	Condition	Well lubricated.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB2	Damper Drive	Condition	Good paint. Reducer is leaking oil from reducer input shaft seal. Thin oil film on motor rear. Reducer oil level sight glass is opaque.			1		Replace reducer oil level sight glass		9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB2	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB3	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good							9/17/2024
10200 Ventilation System	Damper BSB3	Damper Chains	Condition	Well lubricated.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB3	Damper Drive	Condition	Good paint. Thin oil film on motor rear. Moderate Paint failure on motor coupling. Reducer oil level sight glass is opaque.			1		Replace reducer oil level sight glass		9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSB3	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has Very isolated paint failure and Very minor surface rust.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024
10200 Ventilation System	Damper BSE1	Damper Door	Condition	Good			1				9/17/2024
			Operation	Good							9/17/2024
10200 Ventilation System	Damper BSE1	Damper Chains	Condition	Well lubricated.			1				9/17/2024
			Operation	Good. Smooth.							9/17/2024

10200 Ventilation System	Damper BSE1	D	Condition	Good paint. Thin oil film on motor front and rear. Reducer		Replace reducer oil level sight glass	9/17/2024
		Damper Drive		oil level sight glass is opaque. Reducer is leaking oil from shaft seal plates.	1		
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE1	Damper Drive Rod and Bearings	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE2	Damper Door	Condition	Good	1		9/17/2024
			Operation	Good			9/17/2024
10200 Ventilation System	Damper BSE2	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE2	Damper Drive	Condition	Good paint. Thin oil film on motor front and rear. Reducer oil level sight glass is opaque. Reducer is leaking oil from shaft seal plates and input and output shaft seals.	2	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE2	Damper Drive Rod and Bearings	Condition	Well lubricated.	1		9/17/2024
		and Bearings	Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE3	Damper Door	Condition	Good	1		9/17/2024
			Operation	Good			9/17/2024
10200 Ventilation System	Damper BSE3	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE3	Damper Drive	Condition	Reducer oil level sight glass is opaque.	1	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BSE3	Damper Drive Rod and Bearings	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB1	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024
			Operation	Good			9/17/2024
10200 Ventilation System	Damper BNB1	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB1	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque.	1	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB1	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has isolated paint failure and minor surface rust.	1		9/17/2024
		and bearings	Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB2	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024
			Operation	Good			9/17/2024

10200 Ventilation System	Damper BNB2	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB2	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque.	1	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.	1		9/17/2024
10200 Ventilation System	Damper BNB2	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has isolated paint failure and minor surface rust.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB3	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024
			Operation	Good	1		9/17/2024
10200 Ventilation System	Damper BNB3	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNB3	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque. Reducer is leaking oil from manual drive shaft seal plate and output shaft seal.	2	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	•	Damper Drive Rod and Bearings	Condition	Well lubricated. Support has isolated paint failure and minor surface rust.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE1	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024
			Operation	Good			9/17/2024
10200 Ventilation System	Damper BNE1	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE1	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque. Reducer is leaking oil from input shaft seal	1	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE1	and Bearings	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE2	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024
			Operation	Good			9/17/2024
10200 Ventilation System	Damper BNE2	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE2	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque.	1	Replace reducer oil level sight glass	9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	•	Damper Drive Rod and Bearings	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE3	Damper Door	Condition	Very isolated paint failure and Very minor surface rust.	1		9/17/2024

			Operation	Good			9/17/2024
10200 Ventilation System	Damper BNE3	Damper Chains	Condition	Well lubricated.	1		9/17/2024
			Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE3	Damper Drive Motor	Condition	Good paint. Reducer oil level sight glass is opaque. Reducer	1	Replace reducer oil level sight glass	9/17/2024
			Operation	is leaking oil from input shaft seal Good. Smooth.			9/17/2024
10200 Ventilation System	Damper BNE3		Condition	Well lubricated.	1		9/17/2024
		and Bearings	Operation	Good. Smooth.			9/17/2024
10200 Ventilation System	CO Monitor	North Exhaust Duct	Condition	Device looked to be turned off.	3	Have the monitoring equipment checked out.	9/17/2024
			Operation	Device looked to be turned off.	3		9/17/2024
10200 Ventilation System	CO Monitor	South Exhaust Duct	Condition	Good	1		9/17/2024
			Operation	Good, reading of 14 ppm was taken			9/17/2024
10200 Ventilation System	Exhaust Air Port	North Exhaust Duct	Condition	Inspected By Structural - Insert More Lines as needed for each port with a Defect			
10200 Ventilation System	Exhaust Air Port	South Exhaust Duct	Condition	Inspected By Structural - Insert More Lines as needed for each port with a Defect			
10200 Ventilation System	Fresh Air Flue	North Exhaust Duct	Condition	Inspected By Structural - Insert More Lines as Needed for each flue with a Defect			
10200 Ventilation System	Fresh Air Flue	South Exhaust Duct	Condition	Inspected By Structural - Insert More Lines as Needed for each flue with a Defect			
				eden nac with a Berece			
10201 Fans	Fan BSB1	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff	1		9/17/2024
		Fan Motor Controller	Operation	Good	1		9/17/2024
		Fan Motor Controller E Stop	Operation	Good	1		9/17/2024
		Fan Shaft	Condition	Good	1		9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level. Minor surface rust on bearing split bolt.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Drive Side	Condition	Good. Typical concrete surface cracks.	1		9/17/2024
			Condition	Good. Typical concrete surface cracks.	1		9/17/2024

		Fan Drive System	Condition	Good	1	9	9/17/2024
			Operation	Good	1	S	9/17/2024
		Fan Wheel	Condition	Mior paint failure and surface corrosion.	1	S	9/17/2024
		Fan Housing	Condition	Mior paint failure and surface corrosion.	1	2	9/17/2024
		Conduits	Condition	Good	1	g	9/17/2024
10201 Fans	Fan BSB2	Fan Motor	Condition	Good	1	S	9/17/2024
		Fan Motor	Operation Speed 1	Good, per maintenance staff	1	Ç	9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff	1	Ç	9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff	1	S	9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff	1	C	9/17/2024
		Fan Motor Controller	Operation	Good	1	9	9/17/2024
		Fan Motor Controller E Stop		Good	1	9	9/17/2024
			Condition	Minor paint failure and surface rust at sprocket	1	9	9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level.	1	Ş	9/17/2024
			Operation	Good	1	9	9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level.	1	Ş	9/17/2024
			Operation	Good	1	9	9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good	1	9	9/17/2024
			Condition	Good	1	2	9/17/2024
			Condition	Good	1	S	9/17/2024
			Operation	Good	1	9	9/17/2024
		Fan Wheel	Condition	Mior paint failure and surface corrosion.	1	9	9/17/2024
		Fan Housing	Condition	Mior paint failure and surface corrosion.	1	9	9/17/2024
		Conduits	Condition	good	1	9	9/17/2024
						9	9/17/2024
10201 Fans	Fan BSB3	Fan Motor	Condition	Good	1	g	9/17/2024
		Fan Motor	Operation Speed 1	Good. 44A	1	9	9/17/2024
		Fan Motor	Operation Speed 2	Good. 107A	1	9	9/17/2024
		Fan Motor	Operation Speed 3	Good. 75A	1	9	9/17/2024
		Fan Motor	Operation Speed 4	Good. 175A	1	g	9/17/2024

		Fan Motor Controller	Operation	Good. Voltage ok and balanced	1	9/17/2024
		Fan Motor Controller E Stop	Operation	Good	1	9/17/2024
		Fan Shaft	Condition	Minor paint failure and surface rust at sprocket	1	9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level. Moderate bearing seal leak.	1	9/17/2024
			Operation	Good	1	9/17/2024
			Condition	Good. Good oil level.	1	9/17/2024
		Non-Drive Side	Operation	Good	1	9/17/2024
			Condition	Good. Typical concrete surface cracks.	1	9/17/2024
			Condition	Good. Typical concrete surface cracks.	1	9/17/2024
		Non-Drive Side Fan Drive System	Condition	Good	1	9/17/2024
			Operation	Good	1	9/17/2024
		Fan Wheel	Condition	Mior paint failure and surface corrosion.	1	9/17/2024
		Fan Housing	Condition	Mior paint failure and surface corrosion.	1	9/17/2024
		Conduits	Condition	Good	1	9/17/2024
						9/17/2024
10201 Fans	Fan BSE1	Fan Motor	Condition	Good	1	9/17/2024
		Fan Motor	Operation Speed 1	Good. 44A	1	9/17/2024
		Fan Motor	Operation Speed 2	Good. 103A	1	9/17/2024
		Fan Motor	Operation Speed 3	Good. 100A	1	9/17/2024
		Fan Motor	Operation Speed 4	Good. 244A	1	9/17/2024
		Fan Motor Controller	Operation	Good. Voltage ok and balanced	1	9/17/2024
		Fan Motor Controller E Stop		Good	1	9/17/2024
		Fan Shaft	Condition	Good	1	9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level.	1	9/17/2024
			Operation	Good	1	9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level.	1	9/17/2024
			Operation	Good	1	9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good	1	9/17/2024
			Condition	Good	1	9/17/2024
			Condition	Good	1	9/17/2024
			Operation	Good	1	9/17/2024

		Fan Wheel	Condition	Good	1		9/17/2024
		Fan Housing	Condition	Good	1		9/17/2024
		Conduits	Condition	Good	1		9/17/2024
							9/17/2024
10201 Fans	Fan BSE2	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good. 45A	1		9/17/2024
		Fan Motor	Operation Speed 2	Good. 110A	1		9/17/2024
		Fan Motor	Operation Speed 3	Good. 96A	1		9/17/2024
		Fan Motor	Operation Speed 4	Good. 220A	1		9/17/2024
		Fan Motor Controller	Operation	Good. Voltage ok and balanced	1		9/17/2024
		Fan Motor Controller E Stop	Operation	Good	1		9/17/2024
		Fan Shaft	Condition	Good	1		9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Wheel	Condition	Good	1		9/17/2024
		Fan Housing	Condition	Good	1		9/17/2024
		Conduits	Condition	Good	1		9/17/2024
							9/17/2024
10201 Fans	Fan BSE3	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good, per maintenance staff			9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff			9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff			9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff			9/17/2024
		Fan Motor Controller	Operation	Good	1		9/17/2024
		Fan Motor Controller E Stop	Operation	Good	1		9/17/2024

		Fan Shaft	Condition	Minor paint failure.	1		9/17/2024
			Condition	Good. Good oil level.	1		9/17/2024
		Drive Side	Operation	Good			9/17/2024
			Condition	Good. Good oil level.	1		9/17/2024
		Non-Drive Side			1		
			Operation	Good	1		9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Wheel	Condition	Good			9/17/2024
			Condition	Good	1		9/17/2024
					1		
		Conduits	Condition	Good	1		9/17/2024
							9/17/2024
10201 Fans	Fan BNB1	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff	1		9/17/2024
		Fan Motor Controller	Operation	Good	1		9/17/2024
		Fan Motor Controller E Stop		Good	1		9/17/2024
			Condition	Good	1		9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good Paint. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
			Condition	Good Paint. Good oil level.	2		9/17/2024
		Non-Drive Side	Operation	Good	1		9/17/2024
			Condition	Good Paint	1		9/17/2024
		Drive Side Bearing Pedestal -	Condition	Good Paint			9/17/2024
		Non-Drive Side	Condition	Good	1		9/17/2024
		·	Operation	Good	1		
					1		9/17/2024
			Condition	Good	1		9/17/2024
		Fan Housing	Condition	Isolated paint failure and minor corrosion.	1		9/17/2024

		Conduits	Condition	Good	1	9/17/202
						9/17/2024
10201 Fans	Fan BNB2	Fan Motor	Condition	Good	1	9/17/202
		Fan Motor	Operation Speed 1	Good, per maintenance staff	1	9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff		9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff		9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff		9/17/2024
		Fan Motor Controller	Operation	Good	1	9/17/2024
		Fan Motor Controller		Good	1	9/17/2024
		E Stop Fan Shaft	Condition	Good	1	9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good Paint. Good oil level. Minor oil leak.	2	9/17/2024
			Operation	Good	1	9/17/2024
			Condition	Good Paint. Good oil level.	1	9/17/2024
		Non-Drive Side	Operation	Good	1	9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good Paint	1	9/17/202
		Bearing Pedestal -	Condition	Good Paint	1	9/17/202
		Non-Drive Side Fan Drive System	Condition	Good	2	9/17/202
			Operation	Good		9/17/2024
			Condition	Good	1	9/17/202
		Fan Housing	Condition	Isolated paint failure and minor corrosion.	1	9/17/202
			Condition	Good	1	9/17/202
		Conduits	Condition	9000	1	
						9/17/2024
10201 Fans	Fan BNB3		Condition	Good	2	9/17/2024
		Fan Motor	Operation Speed 1	Good. 46A	1	9/17/202
		Fan Motor	Operation Speed 2	Good. 105A	1	9/17/202
		Fan Motor	Operation Speed 3	Good. 75A	1	9/17/202
			Operation Speed 4	Good. 180A	1	9/17/2024
		Fan Motor Controller	Operation	Good. Voltage ok and balanced	1	9/17/2024
		Fan Motor Controller E Stop		Good	1	9/17/202
			Condition	Good	1	9/17/202
		Fan Shaft Bearing Drive Side	Condition	Minor paint failure and surface corrosion.	1	9/17/2024

			Operation	Good	1		9/17/2024
			Condition	Good Paint. Good oil level.	1		9/17/2024
		Non-Drive Side	Operation	Good	1		9/17/2024
			Condition	Good Paint	1		9/17/2024
			Condition	Good Paint	1		9/17/2024
		Non-Drive Side Fan Drive System	Condition	Good	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Wheel	Condition	Good	1		9/17/2024
		Fan Housing	Condition	Isolated paint failure and minor corrosion.	1		9/17/2024
		Conduits	Condition	Good, freshly painted	1		9/17/2024
							9/17/2024
10201 Fans	Fan BNE1	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 2	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 3	Good, per maintenance staff	1		9/17/2024
		Fan Motor	Operation Speed 4	Good, per maintenance staff	1		9/17/2024
		Fan Motor Controller	Operation	Good	1		9/17/2024
		Fan Motor Controller	Operation	Good	1		9/17/2024
		E Stop Fan Shaft	Condition	Good	1		9/17/2024
			Condition	Good. Good oil level. Minor surface rust on bearing	1		9/17/2024
		Drive Side	Operation	mounting bolts. And cap bolts. Good	1		9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level. Very minor paint failure.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Bearing Pedestal - Drive Side	Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Wheel	Condition	Good	1		9/17/2024
		Fan Housing	Condition	Good. Water on room floor.	1		9/17/2024
		Conduits	Condition	Good	1		9/17/2024
							9/17/2024

10201 Fans	Fan BNE2	Fan Motor	Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good. 44A	1		9/17/2024
		Fan Motor	Operation Speed 2	Good. 103A			9/17/2024
		Fan Motor	Operation Speed 3	Good. 102A			9/17/2024
		Fan Motor	Operation Speed 4	Good. 233A			9/17/2024
		Fan Motor Controller	Operation	Good. Voltage ok and balanced	1		9/17/2024
		Fan Motor Controller E Stop	Operation	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
		Fan Shaft Bearing Drive Side	Condition	Good. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level. Minor paint failure.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Drive Side	Condition	Good	1		9/17/2024
		Bearing Pedestal - Non-Drive Side	Condition	Good	1		9/17/2024
		Fan Drive System	Condition	Good	1		9/17/2024
			Operation	Good	1		9/17/2024
			Condition	Good	1		9/17/2024
			Condition	Good. Water on room floor.	1		9/17/2024
		Conduits	Condition	Good	1		9/17/2024
							9/17/2024
10201 Fans	Fan BNE3		Condition	Good	1		9/17/2024
		Fan Motor	Operation Speed 1	Good. 47A	1		9/17/2024
			Operation Speed 2	Good. 109A	1		9/17/2024
			Operation Speed 3	Good. 94A	1		9/17/2024
			Operation Speed 4	Good. 230A	2		9/17/2024
		Fan Motor Controller		Good. Voltage ok and balanced	1		9/17/2024
		Fan Motor Controller E Stop		Good	1		9/17/2024
			Condition	Good	1		9/17/2024
		Drive Side	Condition	Good. Good oil level.	1		9/17/2024
			Operation	Good	1		9/17/2024
		Fan Shaft Bearing Non-Drive Side	Condition	Good. Good oil level.	1		9/17/2024

Chesapeake Tunnel - Ventilation System

	Operation	Good		1		9/17/2024
Bearing Pedestal - Drive Side	Condition	Good		1		9/17/2024
Bearing Pedestal - Non-Drive Side	Condition	Good		1		9/17/2024
Fan Drive System	Condition	Good		1		9/17/2024
	Operation	Good		1		9/17/2024
Fan Wheel	Condition	Good		1		9/17/2024
Fan Housing	Condition	Good. Very minor corrosion. Water on room floor.		1		9/17/2024
Conduits	Condition	Good		1		9/17/2024

															Ceiling Tile
eiling Slab Date	Inspector	Weather	Photo	Location	Defect Location	Condition	Comments	Length	Width	Area	Condition State	1 1	2	3 4	Square Cracked De
	Шэрессоі	Weather	riioto	Location	Defect Location	Condition	Comments	Length	Width	Aica	condition state				Oracked De
9/17/2024	WRB, JBB, TMB	Rain and Wind	JBB5299	Tunnel - Chesapeake - Exhaust Duct at Sta. 473+97	Floor	Transverse Crack 9 linear ft. and Width = HL		9	1	9	2	0	9	0 0	_
9/17/2024	WRB, JBB, TMB		JBB5310	Tunnel - Chesapeake - Exhaust Duct at Sta. 474+98	Haunch Beam	Delamination of Length 1ft. by 1ft.	at hanger rod	1	1	1	2	0	1	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 475+20 Tunnel - Chesapeake - Exhaust Duct at Sta. 475+35	Haunch Beam Haunch Beam	Delamination of Length 2ft. by 1ft. Delamination of Length 1ft. by 1ft.	at hanger rod	2 1	1	2	2	0	2	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 475+60	Floor	Longitudinal Crack 7 linear ft. and Width = HL		7	1	7	2	ō	7	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB		JBB5313	Tunnel - Chesapeake - Exhaust Duct at Sta. 475+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 483+75	Floor	Transverse Crack 4 linear ft. and Width = HL		4	1 2	4 2	2 3	0	4	0 0 2 0	
9/17/2024	WRB, JBB, TMB WRB, JBB, TMB		JBB5313	Tunnel - Chesapeake - Exnaust Duct at Sta. 483+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 484+45	Exhaust Air Port Floor	Spall length 1ft. by width 2ft. Longitudinal Crack 8 linear ft. and Width = HL with Efflorescence and Drips per min		8	1	8	2	0	8	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 484+45	Floor	Transverse Crack 20 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min	Corrosion for 3 LF	22	1	22	2/3	0	19	3 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 484+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 484+60	Light Floor	Non-Functional Delamination of Length 1ft. by 3ft.	at hanger rod	0	0 3	0	- 2	0	0	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 486+75	Floor	Delamination of Length 1ft. by 4ft.	at langer too	1	4	4	2	ő	4	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 486+90	Floor	Delamination of Length 2ft. by 3ft.		2	3	6	2	0	6	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 486+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 487+60	Floor Floor	Transverse Crack 20 linear ft. and Width = HL with Efflorescence and Drips per min Transverse Crack 20 linear ft. and Width = HL with Efflorescence and Drips per min	Corrosion for 6 LF	22 22	1	22 22	2 2/3	0	22 16	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 487+85	Floor	Transverse Crack 20 linear ft. and Width = HL		22	1	22	2	0	22	0 0	
9/17/2024	WRB, JBB, TMB WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 488+20	Floor	Delamination of Length 2ft. by 2ft.	2 Asses assessing Tabel	2	2	4	2	0	4	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 489+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 489+80	Haunch Beam Floor	Delamination of Length 5ft. by 1ft. Delamination of Length 3ft. by 2ft.	3 Areas equaling Total	3	2	6	2	0	5 6	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 498+75	Haunch Beam	Delamination of Length 4ft. by 1ft.	at hanger rod	4	1	4	2	0	4	0 0	
9/17/2024	WRB, JBB, TMB WRB, JBB, TMB		JBB5321	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+25 Tunnel - Chesapeake - Exhaust Duct at Sta. 499+52	Light	Non-Functional		0	0	0 8	-	0	0	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB		JBB3321	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+52 Tunnel - Chesapeake - Exhaust Duct at Sta. 499+95	Haunch Beam Floor	Spall length 8ft. by width 1 ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars Delamination of Length 2ft. by 2ft.		2	2	4	2	0	4	0 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 500+40	Haunch Beam	Delamination of Length 5ft. by 2ft.		5	2	10	2	0	10	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB		JBB5322	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 502+60	Floor Floor	Delamination of Length 6ft. by 5ft. Delamination of Length 4ft. by 4ft.		6	5 4	30 16	2	0	30 16	0 0	1
9/19/2024	WRB, JBB	Clear	JBB5383		Exhaust Air Port	Spall length 1ft. by width 1 ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars		1	1	1	3	0	0	1 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 506+50	Exhaust Air Port	Delamination of Length 1ft. by 3ft.		1	3	3	2	0	3	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 506+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 506+50	Exhaust Air Port Exhaust Air Port	Delamination of Length 1ft. By 2ft. Delamination of Length 1ft. By 2ft.		1	2	2	2	0	2	0 0	1
9/17/2024 9/17/2024	WRB, JBB, TMB		JBB5324	Tunnel - Chesapeake - Exhaust Duct at Sta. 506+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 512+65	Exhaust Air Port	Spall length 1ft. by width 3ft.		1	3	3	3	0	0	3 0	1
9/17/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30	Floor	Spall length 1ft. by width 2ft.	at hanger rod	1	2	2	3	0	0	2 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB, JBB, TMB		JBB5125	Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60	Exhaust Air Port Exhaust Air Port	Spall length 1ft. by width 3ft. Delamination of Length 1ft. by 4ft.		1 4	3	3 8	3	0	0 8	3 0 0	1
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 521+90	Haunch Beam	Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars	at hanger rod	1	2	2	3	0	0	2 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 522+50	Floor	Delamination of Length 2ft. by 4ft.		2	4	8	2	0	8	0 0	
9/17/2024 9/17/2024	WRB, JBB, TMB WRB. JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 522+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 522+50	Floor Floor	Longitudinal Crack 10 linear ft. and Width = HL with Efflorescence and 1 Drips per min Hairline cracking Over 5% of area from 522+50 to 523+50		10 0	0	10 130	3	0	0 130	10 0	
9/17/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Exhaust Duct at Sta. 523+00	Floor	Transverse Crack 6 linear ft. and Width = HL		6	1	6	2	0	6	0 0	
														32 0	
												0	370	52 0	
oadway												0	370 # Tiles		
oadway 9/19/2024	WRB, JBB	Clear	JBB5347		Ceiling	(Cracked Tiles Square=4)(Delaminated Tiles Square = 8)		0	0	8	2	0	# Tiles	0 0	4
9/19/2024	WRB, JBB	Clear	JBB5347	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00	Ceiling	(Delaminated Tiles Square = 6)		0 0	0	6	2 2	0 0	# Tiles 8 6	0 0 0 0	4
			JBB5347					0 0 0	-		2 2 2 2 2	0 0 0 0 0	# Tiles 8 6	0 0 0 0	4 12
9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear Clear	JBB5347	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 469+00	Ceiling Ceiling Ceiling Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square=12)(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6)	Loose pieces were removed	0 0 0 0	0 0 0 0	6 16 10 6	2 2 2 2 2	0 0	# Tiles 8 6 16 10 0	0 0 0 0 0 0 0 0 0 0	4 12 6
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear	JBB5347	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 469+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10) (Delaminated Tiles Square = 10) (Cracked Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4)	Loose pieces were removed	0 0 0 0 0	0 0	6 16 10 6 4	2 2 2 2 2 1 1 1	0 0	# Tiles 8 6 16 10	0 0 0 0 0 0 0 0	4 12 6 4 12
9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 469+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75	Ceiling Ceiling Ceiling Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square=12)(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6)		0 0 0 0 0	0 0 0 0 0	6 16 10 6	2 2 2 2 1 1 1 2 2	0 0	# Tiles 8 6 16 10 0	0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 479+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Cracked Tiles Square=4)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Cracked Tiles Square=1)	Loose pieces were removed Corrosion Staining and Efflorescence	0 0 0 0 0	0 0 0 0 0	6 16 10 6 4 50 12	1	0 0 0 0 6 4 0	# Tiles 8 6 16 10 0 0 50 12 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 469+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 12) (Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4) (Cracked Tiles Square=2)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=12) (Delaminated Tiles Square = 50)- (Cracked Tiles Square=4) (Delaminated Tiles Square = 12)		0 0 0 0 0 0	0 0 0 0 0	6 16 10 6 4 50	2 2 2 2 1 1 2 2 1 2 3	0 0	# Tiles 8 6 16 10 0 0 50 12	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Delaminated Tiles Square=1) (Stained Tiles Square=1) (Stained Tiles Square=1) (Cracked Tiles Square=20)	Corrosion Staining and Efflorescence	0 0 0 0 0 0	0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20	1	0 0 0 0 6 4 0	# Tiles 8 6 16 10 0 50 12 0 12 0	0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Cracked Tiles Square=4)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Delaminated Tiles Square=1) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Square=4)(Delaminated Tiles Square = 4)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence	0 0 0 0 0 0 0	0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4	1	0 0 0 0 6 4 4 0 0	# Tiles 8 6 16 10 0 0 50 12 0 12 0 14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=6) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Delaminated Tiles Square=1) (Stained Tiles Square=1) (Stained Tiles Square=1) (Cracked Tiles Square=20)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20	1 2 3 3 3	0 0 0 0 6 4 0	# Tiles 8 6 16 10 0 50 12 0 12 0	0 0 0 0 0 0	4 12 6 4
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 479+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+77 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=12) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Stained Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=15)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0	# Tiles 8 6 16 10 0 50 12 0 12 0 0 4 4 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=21)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=1)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Delaminated Tiles Square=8)—	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8	1 2 3 3 3	0 0 0 0 6 4 0 0 1 1 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 0 0 0 0 0 8 8	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1 1 20 4 30 15
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 479+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+77 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)(Delaminated Tiles Square = 50) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=12) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Stained Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=15)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area	0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 0 0 0 0 0 8 8	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Cracked Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)—	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0	# Tiles 8 6 16 10 0 0 12 0 12 0 12 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1 20 4 30 15
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 50) (Cracked Tiles Square=2)-(Delaminated Tiles Square = 50) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30)- (Cracked Tiles Square=30 (Cracked Tiles Square=30) (Cracked Tiles Square=6)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 1 12 2 20 4 3 0 15 8 30 10 9 6	1 2 3 3 3	0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1 2 0 4 1 30 15 30 10 9 6
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=2)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=2)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=1)—(Delaminated Tiles Square = 12)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=1)— (Cracked Tiles Square=1)— (Cracked Tiles Square=3)—	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 1 12 2 20 4 3 0 15 8 30 10 10 6 6 4 7 7 7 8 7 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=6)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=1)— (Delaminated Tiles Square=1)— (Delaminated Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)—	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 1 2 0 4 1 30 15 30 10 9 6
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+60 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 50) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30) (Cracked Tiles Square=15) (Delaminated Tiles Square=8) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 15' Long area Over 15' Long area	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 1 12 2 20 4 3 0 15 8 30 10 10 6 6 4 7 7 7 8 7 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=6)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=1)— (Delaminated Tiles Square=1)— (Delaminated Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)—	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9	1 2 3 3 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 12 0 0 4 0 0 0 8 0 10 0 6 36 0 0 9	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)- (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16)- (Delaminated Tiles Square = 10)- (Cracked Tiles Square=6) (Cracked Tiles Square=4)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 50) (Cracked Tiles Square=12)-(Delaminated Tiles Square = 12) (Cracked Tiles Square=11 (Delaminated Tiles Square=12) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30) (Cracked Tiles Square=15) (Delaminated Tiles Square=8) (Cracked Tiles Square=10) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=30) (Cracked Tiles Square=9) (Cracked Tiles Square=30) (Delaminated Tiles Square = 6) (Delaminated Tiles Square = 108) (Delaminated Tiles Square = 108)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)-(Delaminated Tiles Square = 16) (Delaminated Tiles Square = 10) (Cracked Tiles Square=4)(Delaminated Tiles Square = 4) (Cracked Tiles Square=2) (Delaminated Tiles Square = 50) (Cracked Tiles Square=2) (Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 6 4 4 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 12 0 0 4 0 0 0 8 0 10 0 6 36 0 0 9	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 10)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=6)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=1)— (Delaminated Tiles Square=1)— (Delaminated Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)—(Delaminated Tiles Square = 36)— (Cracked Tiles Square=3)—(Delaminated Tiles Square = 9)— (Delaminated Tiles Square = 9)— (Delaminated Tiles Square = 9)— (Delaminated Tiles Square=28)—(Delaminated Tiles Square=20)—(Delaminated Tiles Square=30)—(Delaminated Tiles Square=30)—(Delaminate	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 500 12 1 112 2 200 4 3 0 15 8 300 10 9 6 36 32 6 48 9 108 28	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 112 0 0 122 0 0 0 10 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 10)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 48)— (Delaminated Tiles Square = 108)— (Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Squ	Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 15' Long area Over 15' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 500 12 1 112 2 200 4 3 0 15 8 30 10 9 6 36 36 32 6 48 9 108 28 20 6 9	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 12 0 0 0 4 0 0 0 8 0 10 0 6 36 0 0 9 0 0 20	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)- (Delaminated Tiles Square = 10)- (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=2) (Delaminated Tiles Square = 5) (Cracked Tiles Square=2) (Delaminated Tiles Square = 12) (Stained Tiles Square=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Square=4) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=6) (Delaminated Til	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 1 12 2 20 4 3 0 15 8 8 30 10 9 6 36 32 6 48 9 108 28 20 6	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 112 0 0 122 0 0 0 10 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 473+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+57 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 10)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 48)— (Delaminated Tiles Square = 108)— (Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Squ	Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 15' Long area Over 15' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 12 0 0 12 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5354 JBB5355 JBB5356	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)- (Delaminated Tiles Square = 10)- (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Square=6) (Delaminated	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 16 10 0 0 12 0 0 12 0 0 12 0 0 14 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+95 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=30)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square=8)—(Cracked Tiles Square=10)= (Cracked Tiles Square=6)— (Delaminated Tiles Square=6)— (Delaminated Tiles Square=6)— (Delaminated Tiles Square=6)— (Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Squar	Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 15' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 50 12 0 0 12 0 0 0 4 0 0 0 8 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 12 4 1 20 4 30 15 30 10 9 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+80 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)- (Delaminated Tiles Square = 10)- (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Square = 108) (Cracked Tiles Square=3) (Delaminated Tiles Square=5) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=5) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Til	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 22 24 16	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 16 10 0 0 12 0 0 12 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10) (Cracked Tiles Square = 10) (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 50) (Cracked Tiles Square=4) (Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30) (Cracked Tiles Square=30) (Cracked Tiles Square=30) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=20) (Delaminated Tiles Square=6) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Cracked Tiles Square=10)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 500 12 1 112 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 8 9 108 28 6 9 6 5 20 100 22 24 16 10	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 16 10 0 0 12 0 0 12 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 482+05 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 12)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square=8)—(Cracked Tiles Square=10)— (Cracked Tiles Square=8)—(Delaminated Tiles Square=20)—(Delaminated Tiles Square=5)— (Cracked Tiles Square=6)— (Delaminated Tiles Square=6)— (Delaminated Tiles Square=8)—(Delaminated Tiles Square=10)— (Cracked Tiles Square=8)—(Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracke	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 22 24 16 10 18	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 1 1 0 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 16 10 0 0 12 0 0 12 0 0 12 0 0 0 13 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10) (Cracked Tiles Square = 10) (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 50) (Cracked Tiles Square=4) (Delaminated Tiles Square = 12) (Cracked Tiles Square=1) (Delaminated Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30) (Cracked Tiles Square=30) (Cracked Tiles Square=30) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=20) (Delaminated Tiles Square=6) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Cracked Tiles Square=10)	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 500 12 1 112 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 8 9 108 28 6 9 6 5 20 100 22 24 16 10	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 16 10 0 0 12 0 0 0 0 0 0 0 0 0 0 0 0 18	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357 JBB5358 JBB5359 JBB5360 JBB5361	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 482+25 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 10)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Stained Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square=8)—(Delaminated Tiles Square = 20)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=6)— Spall length Ift. by width Ift. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars (Cracked Tiles Square=6)— (Cracked Tiles Square=8)—(Delaminated Tiles Square=10)—(Cracked Tiles Square=20)—(Cracked Tiles Square=20)—(Cracked Tiles Square=20)—(Cracked Tiles Square=10)—(Cracked Tiles S	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 22 24 16 10 18 20 4 4	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 6 16 10 0 0 0 12 0 0 0 0 0 0 0 0 0 0 0 18 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+95 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+95 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+95 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10)- (Delaminated Tiles Square = 10)- (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=1) (Delaminated Tiles Square = 12) (Stained Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Square = 6) (Delaminated Tiles Square = 6) (Delaminated Tiles Square= 108) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Delaminated Tiles Square=5) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=10) (Cracked Tiles Square=20) (Cracked Tiles Square=20) (Cracked Tiles Square=21) (Delaminated Tiles Square=10) (Cracked Tiles Square=21) (Delaminated Tiles Square=21) (Delaminated Tiles Square=21) (Delaminated Tiles Square=10) (Cracked Tiles Square=21) (Delaminated Tiles Square=21) (Cracked Tiles Square=21) (Delaminated Tiles Square=21) (Cracked Tiles Square=21) (Delaminated Tiles Square=21) (Cracked Tiles Square=21) (Delaminated Tiles Square=21) (Cracked Tiles Square=21) (Cracked Tiles Square=21) (Cracked Tiles Squ	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 22 24 16 10 18 20 4 4 108	1 2 3 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3 2 3	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 16 10 0 0 12 0 0 12 0 0 12 0 0 14 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357 JBB5358 JBB5359 JBB5360 JBB5361	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+55 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 482+25 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6)— (Cracked Tiles Square = 10)—(Delaminated Tiles Square = 16)— (Delaminated Tiles Square = 10)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 4)— (Cracked Tiles Square=4)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 50)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Cracked Tiles Square=12)—(Delaminated Tiles Square = 12)— (Stained Tiles Square=2)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=30)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Delaminated Tiles Square = 6)— (Delaminated Tiles Square=8)—(Delaminated Tiles Square = 20)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Cracked Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=8)—(Delaminated Tiles Square=6)— Spall length Ift. by width Ift. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars (Cracked Tiles Square=6)— (Cracked Tiles Square=8)—(Delaminated Tiles Square=10)—(Cracked Tiles Square=20)—(Cracked Tiles Square=20)—(Cracked Tiles Square=20)—(Cracked Tiles Square=10)—(Cracked Tiles S	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 22 24 16 10 18 20 4 4	1 2 3 3 2 2 3 3 2 2 2 3 3 1 1 2 2 2 3 3 2 2 2 2	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	# Tiles 8 6 6 16 10 0 0 0 12 0 0 0 0 0 0 0 0 0 0 0 18 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 1 30 15 30 10 9 6 20 32 28 8 6 20
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5352 JBB5353 JBB5355 JBB5356 JBB5357 JBB5358 JBB5359 JBB5360 JBB5361	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 470+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 471+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 472+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 474+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 475+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 478+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 481+90 Tunnel - Chesapeake - Roadway Level	Ceiling	(Delaminated Tiles Square = 6) (Cracked Tiles Square = 10) (Cracked Tiles Square = 10) (Cracked Tiles Square=4) (Delaminated Tiles Square = 4) (Cracked Tiles Square=4) (Delaminated Tiles Square = 5) (Cracked Tiles Square=4) (Delaminated Tiles Square = 50) (Cracked Tiles Square=1) (Delaminated Tiles Square = 12) (Stained Tiles Square=1) (Delaminated Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=30) (Cracked Tiles Square=10) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=5) (Cracked Tiles Square=20) (Delaminated Tiles Square=5) (Cracked Tiles Square=20) (Delaminated Tiles Square=5) (Cracked Tiles Square=20) (Delaminated Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=10) (Delaminated Tiles Square=10) (Cracked Tiles Square=10) (Cracked Tiles Square=10) (Cracked Tiles Square=10) (Delaminated Tiles Square=10) (Cracked Tiles Square=20) (Cracked Tiles Square=3) (Cracked T	Corrosion Staining and Efflorescence Corrosion Staining and Efflorescence Over 16' long area with Corrosion Staining and Efflorescence 4x4 area Corrosion Staining and Efflorescence Over 15' x 15' area with Corrosion Staining and Efflorescence Over 12' Long area Over 15' Long x 12' Wide area with Corrosion Staining and Isolated Delaminated Tiles Over 2' Long x 4' Wide Delaminated area with Corrosion Staining At Vent / Over 2' Long x 6' Wide Delaminated area with Corrosion Staining Over 15x12 area At Vent / Loose pieces were removed Over 20' Length with Corrosion Staining Over 10' x 10' area with Corrosion Staining Over 12' Length Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	6 16 16 10 6 4 50 12 1 12 2 20 4 3 0 15 8 30 10 9 6 36 32 6 48 9 108 28 20 6 9 6 5 20 100 120 121 12 2 2 2 4 16 10 18 20 4 4 4 108 3	1 2 3 3 2 2 3 3 2 2 2 3 3 1 1 2 2 2 3 3 2 2 2 2	0 0 0 0 0 6 4 4 0 0 0 0 0 0 0 0 0 0 0 0	#Tiles 8 6 6 16 10 0 0 12 0 0 12 0 0 0 12 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	4 12 6 4 12 4 11 20 4 30 15 30 10 9 6 20 32 28 8 8 6 20 22 10 4

Chesapeake Tunnel - Ceiling Slab

0/40/	14/DD :	CI.		The state of the s	6	(Conduct The Control 2)						0				0
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 489+80	Ceiling	(Cracked Tiles Square=2)		0 0	2	1	2	U	U	0		2
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 489+90	Ceiling	(Cracked Tiles Square=2)		0 0	2	1	2	0	0	0		2
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 491+10	Ceiling	(Delaminated Tiles Square = 4)		0 0	4	2	0	4	0	0		4
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 491+70	Ceiling	(Delaminated Tiles Square = 4)		0 0	4	2	0	4	0	0		4
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 491+80	Ceiling	(Delaminated Tiles Square = 54)	2' Long x 3' Wide Delaminated area with Corrosion Staining	0 0	54	3	0	0	54	0		54
9/19/2024	WRB, JBB	Clear	JBB5386-53	891 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 492+60	Ceiling	Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss		0 0	36	3	0	0	36	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 492+70	Ceiling	(Delaminated Tiles Square = 20)		0 0	20	2	0	20	0	0		20
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 493+05	Ceiling	(Cracked Tiles Square=3)		0 0	3	1	9	20	0	0		20
								0 0	-	1	3	0		0		3
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 496+00	Ceiling	(Cracked Tiles Square=54)	2' Long x 3' Wide area with Corrosion Staining	0 0	54	3	0	0	54	0		54
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 496+28	Ceiling	(Stained Tiles Square=5)	Corrosion Staining	0 0	5	3	0	0	5	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 496+40	Ceiling	(Cracked Tiles Square=10)		0 0	10	1	10	0	0	0		10
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 496+60	Ceiling	(Cracked Tiles Square=45)	Corrosion Staining	0 0	45	3	0	0	45	0		45
9/19/2024	WRB, JBB	Clear	JBB5370	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 499+35	Ceiling	(Cracked Tiles Square=27)	Corrosion Staining	0 0	27	3	0	0	27	0		27
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 500+00	Ceiling	(Cracked Tiles Square=12)		0 0	12	1	12	0	0	0		12
9/19/2024	WRB, JBB	Clear	JBB5371		Ceiling	(Stained Tiles Square=30)	Corrosion Staining	0 0	30	2	0	0	30	0		30
			JBB55/1				Corrosion Staining	0 0		3	0	0		-		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 500+20	Ceiling	(Delaminated Tiles Square = 6)		0 0	6	2	Ü	ь	0	0		6
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 500+50	Ceiling	(Cracked Tiles Square=27)	Corrosion Staining	0 0	27	3	0	0	27	0		27
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 500+60	Ceiling	(Cracked Tiles Square=36)		0 0	36	1	36	0	0	0		36
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+15	Ceiling	(Stained Tiles Square=4)	Corrosion Staining	0 0	4	3	0	0	4	0		
9/19/2024	WRB. JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+20	Ceiling	(Stained Tiles Square=2)	Corrosion Staining	0 0	2	3	0	0	2	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+22	Ceiling	(Stained Tiles Square=3)	Corrosion Staining	0 0	3	3	0	0	3	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+23	Ceiling	(Stained Tiles Square=4)	Corrosion Staining	0 0	4	2	0	0	4	0		
9/19/2024	WRB, JBB				8	(20000000000000000000000000000000000000		0 0	20	3		0	20		I	
		Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+25	Ceiling	(Stained Tiles Square=20)	Corrosion Staining	0 0	-	3	U	U	20	0	I	
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+30	Ceiling	(Stained Tiles Square=9)	Corrosion Staining	0 0	9	3	0	0	9	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+60	Ceiling	(Cracked Tiles Square=16)	20' Length area	0 0	16	1	16	0	0	0		16
9/19/2024	WRB, JBB	Clear	JBB5385	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+85	Ceiling	(Stained Tiles Square=6)	Corrosion Staining	0 0	6	3	0	0	6	0		
9/19/2024	WRB. JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+85	Ceiling	(Cracked Tiles Square=4)		0 0	8	1	8	0	0	0		8
9/19/2024	WRB. IBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+90	Ceiling	(Stained Tiles Square=8)	Corrosion Staining	0 0	8	2	0	0	8	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 501+90	Ceiling	(Stained Tiles Square=4)	corresion staming	0 0	4	2	0	0	4	0		
9/19/2024	WRB, JBB	Clear			Ceiling	· · ·		0 0	4	3		0	4	-		
-,,	,			Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+02	8	(Delaminated Tiles Square = 4)		0 0		1	4	0	0	0		4
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+20	Ceiling	(Stained Tiles Square=2)	Corrosion Staining	0 0	2	3	0	0	2	0		
9/19/2024	WRB, JBB	Clear	JBB5373	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+25	Ceiling	(Stained Tiles Square=4)	Corrosion Staining	0 0	4	3	0	0	4	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+27	Ceiling	(Stained Tiles Square=3)	Corrosion Staining	0 0	3	3	0	0	3	0		
9/19/2024	WRB. JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+50	Ceiling	(Stained Tiles Square=5)	Corrosion Staining	0 0	5	3	0	0	5	0		
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 502+60	Ceiling	(Cracked Tiles Square=10)		0 0	10	1	10	0	0	0		10
				, , , ,	_			0 0				-	Ü	-		10
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 503+25	Ceiling	(Cracked Tiles Square=10)	Anbandonded Electrical Box - Corrossion and Moisture Leakage Affecting Surrounding Tiles	0 0	10	1	10	0	0	0		10
							3' Long x 2' Wide Delaminated area with 1/4" Gan: Corrosion Staining Vishle Area Flexs									
9/19/2024	WRB, JBB	Clear	JBB5380-53	382 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 504+80	Ceiling	(Delaminated Tiles Square = 162)	3' Long x 2' Wide Delaminated area with 1/4" Gap; Corrosion Staining Visble, Area Flexs,	0 0	162	3	0	0	162	0		162
			JBB5380-53				Notified CBBT Maintenance for Removal			3	-	0				162 6
9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss	Notified CBBT Maintenance for Removal Loose pieces were removed	1 1	1	3	0	0	1	0		162 6
9/19/2024 9/19/2024	WRB, JBB WRB, JBB	Clear Clear	JBB5380-53	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80	Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed	1 1 0 0	1 9	3 1	0	0 0 0	1 0	0		6 9
9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80	Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=15)	Notified CBBT Maintenance for Removal Loose pieces were removed	1 1 0 0 0 0 0	1 9 15	3 1 1	0 9 15	0 0 0	1 0 0	0 0 0		162 6 9 15
9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40	Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=15)— (Cracked Tiles Square=3)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed	1 1 0 0 0 0 0 0 0 0	1 9 15 3	3 1	0 9 15 3	0 0 0 0	1 0	0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed	1 1 0 0 0 0 0	1 9 15 3 6	3 1 1	0 9 15	0 0 0 0	1 0 0	0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40	Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=15)— (Cracked Tiles Square=3)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed	1 1 0 0 0 0 0 0 0 0	1 9 15 3	3 1 1	0 9 15 3	0 0 0 0 0	1 0 0	0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed	1 1 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6	3 1 1	0 9 15 3 0	0 0 0 0 0	1 0 0	0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB WRB, JBB WRB, JBB WRB, JBB WRB, JBB WRB, JBB	Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7	3 1 1	0 9 15 3 0	0 0 0 0 0 0	1 0 0	0 0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8	3 1 1	0 9 15 3 0	0 0 0 0 0 0	1 0 0	0 0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8	3 1 1	0 9 15 3 0 0 0	0 0 0 0 0 0 0	1 0 0	0 0 0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Stained Tiles Square=8) (Stained Tiles Square=3) (Stained Tiles Square=3)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2	3 1 1	0 9 15 3 0 0 0 0	0 0 0 0 0 0 0	1 0 0 0 6 6 7 8 0	0 0 0 0 0 0		6 9 15 3 6 6 7 2 2
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-00	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)—(Delaminated Tiles Square = 36)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38	3 1 1 1 3 3 3 3 1 1 3	0 9 15 3 0 0 0 0 2	0 0 0 0 0 0 0 0	1 0 0 0 6 6 7 8 0 4 38	0 0 0 0 0 0 0 0		6 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-16	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=3) (Cracked Tiles Square=1) (Stained Tiles Square=1)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25	3 1 1	0 9 15 3 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0	1 0 0 0 6 6 7 8 0 4 38	0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-15	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles S	Notified CBST Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25 33	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 0 0 0 6 6 7 8 0 4 38	0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-16	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=3) (Cracked Tiles Square=1) (Stained Tiles Square=1)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25	3 1 1 1 3 3 3 3 1 1 3	0 9 15 3 0 0 0 0 2		1 0 0 0 6 6 7 8 0 4 38	0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-15	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles S	Notified CBST Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25 33	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0		1 0 0 0 6 6 7 8 0 4 38	0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+15 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-25	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=19) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=8) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=10	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25 33 5	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0		1 0 0 0 6 6 7 8 0 4 38 0 0 5	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 7 2 2 2 36 8 25
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=6) (Stained Tiles Square=8) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=6)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25 33 5 10 12	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0	1 0 0 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 0 5 10 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2 2 36 8 25 10 6 6 6
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=25) (Cracked Tiles Square=6) (Delaminated Tiles Square=12)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 8 2 4 38 25 33 5 10 12 12	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0	1 0 0 0 0 0 6 6 6 7 8 0 0 4 4 38 0 0 0 5 10 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2 2 36 8 25 10 6 6 6 12
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+35 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)— (Cracked Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)—(Delaminated Tiles Square = 36)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 2 4 38 25 33 5 10 12 12 8	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 0 0 6 6 6 7 8 0 4 4 38 0 0 5 5 110 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 2 2 2 36 8 25 10 6 6 6 12 8
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=15) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=8) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=8) (Cracked Tiles Square=10)- (Cracked Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=12)	Notified CBST Maintenance for Removal Lose pieces were removed Lose pieces were removed Lose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 2 4 38 25 5 100 12 12 8 12	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 0 5 5 10 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2 2 36 8 25 10 6 6 6 12
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear		Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)— (Cracked Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)—(Delaminated Tiles Square = 36)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 15 3 6 6 6 7 8 8 2 4 4 38 25 33 5 10 12 12 8 12 360	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 6 6 6 7 8 0 4 38 0 0 0 5 10 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 2 2 2 36 8 25 10 6 6 6 12 8
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+40 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)— (Cracked Tiles Square=8)— (Cracked Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 36)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 6)— (Cracked Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 12)— (Delaminated Tiles Square = 12)— (Cracked Tiles Square=8)— (Cracked Tiles Square=8)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 2 4 38 25 33 5 10 12 8 12 360 8	3 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 0 6 6 6 7 8 8 0 0 4 4 38 0 0 0 5 5 10 0 0 0 360 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 36 8 25 10 6 6 12 8 12 360 8
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=8) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=10 (Delaminated Tiles Square=8) (Delaminated Tiles Square=12) (Cracked Tiles Square=360	Notified CBST Maintenance for Removal Lose pieces were removed Lose pieces were removed Lose pieces were removed Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 15 3 6 6 6 7 8 8 2 4 4 38 25 33 5 10 12 12 8 12 360	3 1 1 1 1 3 3 3 3 3 1 1 3 3 2 2 1 1 1 1	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 6 6 6 7 8 0 4 38 0 0 0 5 10 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 2 2 2 36 8 25 10 6 6 6 12 8
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+15 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-75	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=8)— (Cracked Tiles Square=8)— (Cracked Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 36)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 6)— (Cracked Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square = 12)— (Delaminated Tiles Square = 12)— (Cracked Tiles Square=8)— (Cracked Tiles Square=8)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 2 4 38 25 33 5 10 12 8 12 360 8	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 0 6 6 6 7 8 8 0 0 4 4 38 0 0 0 5 5 10 0 0 0 360 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 36 8 25 10 6 6 12 8 12 360 8
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=19) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=8) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=36) (Cracked Tiles Square=36) (Cracked Tiles Square=37(Delaminated Tiles Square=9) (Cracked Tiles Square=27)(Delaminated Tiles Square=9) (Cracked Tiles Square=27)(Delaminated Tiles Square=9) (Cracked Tiles Square=27)(Delaminated Tiles Square=9)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 2 4 38 25 5 100 12 12 8 12 360 8 366	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0	1 0 0 0 6 6 6 7 8 0 4 38 0 0 5 10 0 0 0 0 360 0 0 27	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=1) (Cracked Tiles Square=8) (Delaminated Tiles Square=5) (Cracked Tiles Square=8) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=12) (Cracked Tiles Square=6) (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=36) (Cracked Tiles Square=36) (Cracked Tiles Square=30) (Cracked Tiles Square=90)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 7 8 2 4 4 38 25 33 5 10 12 12 8 12 360 8 36 366 366 90	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 8 0 0 4 4 38 0 0 5 5 10 0 0 0 0 0 27 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 7 2 2 2 36 8 25 10 6 6 12 8 12 360 8 27 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=19)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=8) (Stained Tiles Square=8) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=8) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=8) (Cracked Tiles Square=9) (Delaminated Tiles Square=90) (Delaminated Tiles Square=90) (Delaminated Tiles Square=90 (Delaminated Tiles Square=90 (Delaminated Tiles Square=90 (Delaminated Tiles Square=90	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 7 8 2 4 38 25 33 5 10 12 12 360 8 8 36 36 36 90 9 9	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 0 360 0 0 27 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5374	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=8) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=1) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=10 (Cracked Tiles Square=10 (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=3) (Delaminated Til	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 36 90 9 9	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 360 0 0 27 0 0 0 9	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Stained Tiles Square=1) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=10 (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=3) (Cracked Tiles Square=27) (Delaminated Tiles Square=3) (Delaminated Tiles Square=3) (Delaminated Tiles Square=9)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 5 5 10 12 2 8 12 360 8 36 36 90 9 9 9 4	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 0 360 0 0 27 0 0 0 0 9 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=8) (Stained Tiles Square=8) (Cracked Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=1) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=10 (Cracked Tiles Square=10 (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=8) (Delaminated Tiles Square=3) (Delaminated Til	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 36 90 9 9	3 1 1 1 3 3 3 3 3 1 3 3 2 2 2 2 3 3 3 3	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 360 0 0 27 0 0 0 9	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+65 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+10 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Stained Tiles Square=7) (Stained Tiles Square=2) (Stained Tiles Square=2) (Stained Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Stained Tiles Square=1) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Cracked Tiles Square=8) (Cracked Tiles Square=10 (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=12) (Cracked Tiles Square=8) (Delaminated Tiles Square=8) (Delaminated Tiles Square=3) (Cracked Tiles Square=27) (Delaminated Tiles Square=3) (Delaminated Tiles Square=3) (Delaminated Tiles Square=9)	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 5 5 10 12 2 8 12 360 8 36 36 90 9 9 9 4	3 1 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2 2 3 3 2 2 2 3 3 2 2 1 1 1 2 2 3 3 2 2 2 3 3 1	0 9 15 3 0 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 0 360 0 0 27 0 0 0 0 9 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=8)— (Stained Tiles Square=8)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 36)— (Stained Tiles Square=2)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 6)— (Cracked Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square= 12)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)— (Delaminated Tiles Square = 8)— (Cracked Tiles Square=8)— (Cracked Tiles Square=8)— (Cracked Tiles Square=8)— (Cracked Tiles Square=36)— (Delaminated Tiles Square = 36)— (Delaminated Tiles Square = 9)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 7 8 2 4 38 25 33 5 10 12 12 360 8 8 36 36 36 36 90 9 9 9 4 9	3 1 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2 2 3 3 2 2 2 3 3 2 2 1 1 1 2 2 3 3 2 2 2 3 3 1	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 6 6 6 7 8 8 0 4 4 38 0 0 5 10 0 0 0 360 0 0 7 7 0 0 0 9 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=7)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=10— (Cracked Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square= 2)— (Delaminated Tiles Square= 2)— (Cracked Tiles Square=360— (Cracked Tiles Square=37)—(Delaminated Tiles Square=9)— (Delaminated Tiles Square=4)— (Delaminated Tiles Square=6)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 36 90 9 9 4 9 9 4	3 1 1 1 1 3 3 3 3 3 1 1 3 3 2 2 2 2 3 3 2 2 2 3 3 2 2 1 1 1 2 2 3 3 2 2 2 3 3 1	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 360 0 0 27 0 0 0 9 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 515+05 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 515+05 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Cracked Tiles Square=8)— (Stained Tiles Square=8)— (Stained Tiles Square=8)— (Cracked Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Cracked Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=12)— (Delaminated Tiles Square=12)— (Cracked Tiles Square=36)— (Delaminated Tiles Square=36)— (Cracked Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=9)— (Delaminated	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 7 8 2 4 38 25 33 5 10 12 12 8 12 360 8 8 36 36 36 90 9 9 9 4 6 6 9	3 1 1 1 1 3 3 3 3 3 1 1 3 3 3 2 2 2 2 3 3 3 2 2 1 1 1 2 2 2 2	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9	1 0 0 0 0 6 6 6 7 8 8 0 0 4 4 38 8 0 0 5 10 0 0 0 0 360 0 0 9 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 524-150 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 524-50 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3)- (Cracked Tiles Square=3)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Stained Tiles Square=8)- (Stained Tiles Square=2)- (Stained Tiles Square=2)- (Stained Tiles Square=2)- (Cracked Tiles Square=2)- (Cracked Tiles Square=1)- (Cracked Tiles Square=8)- (Delaminated Tiles Square=5)- (Cracked Tiles Square=1)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Delaminated Tiles Square=8)- (Delaminated Tiles Square=8)- (Delaminated Tiles Square=8)- (Cracked Tiles Square=8)- (Delaminated Tiles Square=9)-	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 36 90 9 9 4 9 9 4 6 6 9 6	3 1 1 1 1 3 3 3 3 3 3 1 1 3 3 2 2 1 1 1 1	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 0 8 9 36 99 0 0 0	1 0 0 0 0 6 6 6 7 8 0 0 4 38 0 0 5 10 0 0 0 360 0 0 9 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square= 12)— (Cracked Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=30— (Delaminated Tiles Square=30— (Delaminated Tiles Square=3)— (Delaminated Tiles Square=3)— (Delaminated Tiles Square=4)— (Delaminated Tiles Square=6)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 7 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 90 9 9 4 9 9 4 6 6 9 9 6 6 9	3 1 1 1 1 3 3 3 3 3 1 1 3 3 3 2 2 2 2 3 3 3 2 2 1 1 1 2 2 2 2	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 0 12 0 0 0 0 0 8 9 36 99 9 0 0 0	1 0 0 0 0 6 6 6 7 8 8 0 4 338 0 0 5 5 10 0 0 0 0 27 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 12 360 90 9 9 4 6 9 9 6 9 9
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+75 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+16 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+28 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 524-150 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 524-50 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)- (Cracked Tiles Square=3)- (Cracked Tiles Square=3)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Stained Tiles Square=8)- (Stained Tiles Square=2)- (Stained Tiles Square=2)- (Stained Tiles Square=2)- (Cracked Tiles Square=2)- (Cracked Tiles Square=1)- (Cracked Tiles Square=8)- (Delaminated Tiles Square=5)- (Cracked Tiles Square=1)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=6)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Cracked Tiles Square=10)- (Delaminated Tiles Square=8)- (Delaminated Tiles Square=8)- (Delaminated Tiles Square=8)- (Cracked Tiles Square=8)- (Delaminated Tiles Square=9)-	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 6 7 8 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 36 90 9 9 4 9 9 4 6 6 9 6	3 1 1 1 1 3 3 3 3 3 1 1 3 3 3 2 2 2 2 3 3 3 2 2 1 1 1 2 2 2 2	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 12 0 0 0 0 0 0 8 9 36 9 9 0 0 0 0 9 9 4 6 9 9 9 9 9 9 9 9 9 9 9 9 9	1 0 0 0 0 6 6 6 7 8 8 0 0 4 4 38 0 0 5 10 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		6 9 15 15 3 6 6 6 7 7 2 2 2 2 3 6 8 25 10 6 6 6 12 8 12 360 8 27 9 36 90 9 9 4 6 6 9 6 9 13
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	WRB, JBB	Clear	JBB5375 JBB5378	Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+85 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 510+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 511+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+80 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 512+20 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 513+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+00 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+25 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+30 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514+90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level - Ceiling Tiles at Sta. 514-90 Tunnel - Chesapeake - Roadway Level	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars // 10%< Section Loss (Cracked Tiles Square=9)— (Cracked Tiles Square=3)— (Cracked Tiles Square=6)— (Cracked Tiles Square=6)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Stained Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=2)— (Cracked Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=8)—(Delaminated Tiles Square = 25)— (Stained Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square=6)—(Delaminated Tiles Square = 6)— (Delaminated Tiles Square= 12)— (Cracked Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=36)— (Delaminated Tiles Square=30— (Delaminated Tiles Square=30— (Delaminated Tiles Square=3)— (Delaminated Tiles Square=3)— (Delaminated Tiles Square=4)— (Delaminated Tiles Square=6)—	Notified CBBT Maintenance for Removal Loose pieces were removed Loose pieces were removed Loose pieces were removed Corrosion Staining Corrosion Staining Corrosion Staining Corrosion Staining on Cracked Tiles Mostly 4 sf Corrosion Staining	1 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 9 15 3 6 6 7 7 8 2 4 4 38 25 33 3 5 10 12 12 8 12 360 8 36 90 9 9 4 9 9 4 6 6 9 9 6 6 9	3 1 1 1 1 3 3 3 3 3 3 1 1 3 3 2 2 1 1 1 1	0 9 15 3 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	33 0 12 0 0 0 0 0 0 8 9 36 9 9 0 0 0 0 9 9 4 6 9 9 9 9 9 9 9 9 9 9 9 9 9	1 0 0 0 0 6 6 6 7 8 8 0 4 338 0 0 5 5 10 0 0 0 0 27 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 1	6 9 15 3 6 6 6 7 7 2 2 2 2 36 8 25 10 6 6 6 12 8 12 8 12 360 8 27 9 9 9 9 4 6 9 9 6 9 9

Length Width Area Condition State 1 ransverse Crack 10 linear ft. and Width = N with Efflorescence and No Active Lea WRB, JBB, TMB Rain and Wir unnel - Chesapeake - Supply Duct at Sta. 469+25 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 469+50 Ceiling Transverse Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 469+55 Transverse Crack 13 linear ft. and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Transverse Crack 15 linear ft. and Width = N with Efflorescence and No Active Leak Ceiling 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 469+65 Tunnel - Chesapeake - Supply Duct at Sta. 469+70 Tunnel - Chesapeake - Supply Duct at Sta. 469+80 Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Ceiling Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 469+90 Ceiling Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Transverse Crack 12 linear ft. and Width = HL Transverse Crack 40 linear ft. and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 470+20 Ceiling Tunnel - Chesapeake - Supply Duct at Sta. 470+20 Tunnel - Chesapeake - Supply Duct at Sta. 470+35 Longitudinal Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 8 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Ceiling Ceiling WRB, JBB, TMB Rain and Wind 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind JBB5259 Tunnel - Chesapeake - Supply Duct at Sta. 470+41 Ceiling Ceiling Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 3 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 470+68 WRB, JBB, TMB Rain and Wind Ceiling 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 470+68 Tunnel - Chesapeake - Supply Duct at Sta. 470+70 Tunnel - Chesapeake - Supply Duct at Sta. 470+75 9/16/2024 WRR IRR TMR Rain and Wind Ceiling Ceiling Transverse Crack 17 linear ft, and Width = HL with Efflorescence and No Active Leal Transverse Crack 10 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 470+75 Ceiling Longitudinal Crack 15 linear ft, and Width = N with Efflorescence and No Active Leak Tunnel - Chesapeake - Supply Duct at Sta. 470+95 ransverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 470+95 Ceiling Longitudinal Crack 8 linear ft, and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+00 Ceiling Ceiling Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak Longitudinal Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 471+05 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+10 Ceiling Ceiling Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Win Tunnel - Chesapeake - Supply Duct at Sta. 471+12 Fransverse Crack 24 linear ft. and Width = N Transverse Crack 18 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+15 Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+15 Ceiling Longitudinal Crack 5 linear ft. and Width = HL Transverse Crack 4 linear ft, and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+15 Ceiling Previously accounted for in Liner 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+20 Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+20 Ceiling Transverse Crack 24 linear ft, and Width = N WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+30 Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+40 Ceiling Longitudinal Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, IBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+50 Tunnel - Chesapeake - Supply Duct at Sta. 471+55 Ceiling Ceiling Transverse Crack 2 linear ft. and Width = HL with Efflorescence and 0 Drips per min Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+55 Ceiling Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Previously accounted for in Liner 12 Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+60 Ceiling 9/16/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+70 Ceiling Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Previously accounted for in Line 9/16/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+75 Ceiling Ceiling Transverse Crack 12 linear ft and Width = N 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+75 Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+80 Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Longitudinal Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+85 Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+90 Ceiling Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leal 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 471+97 Ceiling Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Ceiling Fransverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak unnel - Chesapeake - Supply Duct at Sta. 472+00 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+10 Ceiling Transverse Crack 2 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 1 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+10 Ceiling WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+15 Ceiling 9/16/2024 9/16/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+20 Ceiling Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and 1 Drins per min WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+20 Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+20 Ceiling Longitudinal Crack 2 linear ft, and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+20 Ceiling Transverse Crack 20 linear ft. and Width = HL with Efflorescence and No Active Leal 9/16/2024 WRB, IBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+25 Ceiling Ceiling Transverse Crack 13 linear ft. and Width = HL with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+25 Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 Longitudinal Crack 20 linear ft. and Width = N 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+25 Ceiling WRB, JBB, TMB Rain and Win Tunnel - Chesapeake - Supply Duct at Sta. 472+30 Ceiling 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+48 Ceiling Transverse Crack 20 linear ft. and Width = HL with Efflorescence and No Active Leak Previously accounted for in Liner WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+50 Transverse Crack 24 linear ft. and Width = HL with Efflorescence and 1 Drips per min 9/16/2024 Ceiling 9/16/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+50 Ceiling Transverse Crack 3 linear ft, and Width = N with Efflorescence and No Active Leak Transverse Crack 2 linear ft. and Width = HL with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+58 Ceiling Previously accounted for in Liner 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+62 Transverse Crack 24 linear ft. and Width = N with Corrosion and Efflorescence and No Active Leak 9/16/2024 Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesaneake - Sunnly Duct at Sta 472+62 Ceiling Transverse Crack 4 linear ft, and Width = HI, with Efflorescence and No Active Leak Previously accounted for in Liner WRB, JBB, TMB Rain and Wind Transverse Crack 24 linear ft. and Width = N Tunnel - Chesapeake - Supply Duct at Sta. 472+65 9/16/2024 Ceiling 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+70 Ceiling Ceiling Transverse Crack O linear ft. and Width = N with Efflorescence and No Active Leak Tunnel - Chesapeake - Supply Duct at Sta. 472+70 Transverse Crack 4 linear ft. and Width = HL 9/16/2024 /RB, JBB, TM Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+75 Ceiling Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 VRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+80 Ceiling WRB, JBB, TMB Rain and Wind 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 472+85 Ceiling Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+90 Ceiling WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 472+98 Ceiling 9/16/2024 9/16/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+04 Ceiling Ceiling Transverse Crack 24 linear ft, and Width = HI, with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+15 Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+20 Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+22 Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leal 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 473+30 Ceiling 9/16/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+50 Ceiling Ceiling Transverse Crack O linear ft. and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+60 Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+60 Ceiling Transverse Crack 4 linear ft, and Width = HL with Efflorescence and No Active Leak Previously accounted for in Line Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+65 Ceiling Tunnel - Chesapeake - Supply Duct at Sta. 473+70 Tunnel - Chesapeake - Supply Duct at Sta. 473+75 Transverse Crack 30 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Ceiling Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+80 Ceiling Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Win Tunnel - Chesapeake - Supply Duct at Sta. 473+85 Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 473+85 Ceiling Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak Tunnel - Chesapeake - Supply Duct at Sta. 473+95 Tunnel - Chesapeake - Supply Duct at Sta. 473+95 Transverse Crack 3 linear ft. and Width = N with Efflorescence and No Active Leak Longitudinal Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Ceiling Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+03 Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and No Active Leak Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leal WRB, JBB, TMB Rain and Wind Ceiling Tunnel - Chesapeake - Supply Duct at Sta. 474+10 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+15 Ceiling Transverse Crack 24 linear ft, and Width = HL with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+30 Ceiling Ceiling Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+40 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+42 Transverse Crack 3 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 eviously accounted for in Line 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+60 Ceiling Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+62 Transverse Crack 2 linear ft. and Width = N with Efflorescence and No Active Leak WRB, JBB, TMB Rain and Wind Transverse Crack 8 linear ft. and Width = HL with Efflorescence and No Active Leak 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 474+70 Ceiling Tunnel - Chesapeake - Supply Duct at Sta. 474+75 Tunnel - Chesapeake - Supply Duct at Sta. 474+80 9/16/2024 WRB, JBB, TMB Rain and Wind Ceiling Ceiling Transverse Crack 20 linear ft, and Width = N with Efflorescence and No Active Leak Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak 9/16/2024 WRB, JBB, TMB Rain and Wind WRB. JBB. TMB Rain and Wind 9/16/2024 Tunnel - Chesapeake - Supply Duct at Sta. 474+85 Transverse Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak Previously accounted for in Line Ceiling 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 474+90 Ceiling Transverse Crack 20 linear ft, and Width = N WRB, JBB, TMB Rain and Wind Transverse Crack 24 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 3 linear ft. and Width = HL with Efflorescence and No Active Leak Tunnel - Chesapeake - Supply Duct at Sta. 475+00 9/16/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Supply Duct at Sta. 475+03

9/16/2024

0/46/2024	MOD IDD TAID Date and Mind	Turnel Character County Burst at Char 475 OF	C-III	Delevisor of Leady Africa Office		2	0 27	2		27	•	
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+05	Ceiling	Delamination of Length 3ft. by 9ft.		20	9 27 1 20	2	0	27	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+15	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak	Drawingly accounted for in Lines	20	1 20	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+15	Ceiling Ceiling	Transverse Crack 4 linear ft. and Width = NL with Efflorescence and No Active Leak Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak	Previously accounted for in Liner	4	1 0	2	0	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+20	Ceiling	Transverse Crack 8 linear ft, and Width = N with Efflorescence and No Active Leak Transverse Crack 8 linear ft, and Width = HL with Efflorescence and No Active Leak		0	1 0	2	, c		0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+30 Tunnel - Chesapeake - Supply Duct at Sta. 475+70	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak		6	1 6	2	Č	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+75	Ceiling	Transverse Crack 24 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	č	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+80	Ceiling	Transverse Crack 12 linear ft, and Width = HL with Efflorescence and No Active Leak		12	1 12	2	č	12	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+85	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak	Previously accounted for in Liner	6	1 6	2	c	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+90	Ceiling	Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak	•	18	1 18	2	c	18	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+92	Ceiling	Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	č	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+00	Ceiling	Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak		12	1 12	2	Ċ	12	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+06	Ceiling	Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	c	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+10	Ceiling	Transverse Crack 30 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	c	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+12	Ceiling	Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak	Previously accounted for in Liner	4	1 4	2	c	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+25	Ceiling	Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	c	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+25	Ceiling	Longitudinal Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak		4	1 4	2	c	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+25	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak	Previously accounted for in Liner	10	1 10	2	c	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+30	Ceiling	Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	c	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+30	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	c	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+40	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+45	Ceiling	Transverse Crack 18 linear ft. and Width = N		18	1 18	2	0	18	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+50	Ceiling	Transverse Crack 8 linear ft. and Width = HL with Efflorescence and No Active Leak		8	1 8	2	0	8	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+52	Ceiling	Transverse Crack 12 linear ft. and Width = HL		12	1 12	2	0	12	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+70	Ceiling	Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak		18	1 18	2	0	18	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+70	Ceiling	Transverse Crack 2 linear ft. and Width = HL with Efflorescence and No Active Leak	Previously accounted for in Liner	2	1 2	2	0	2	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+72	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+72	Ceiling	Transverse Crack 10 linear ft. and Width = N		10	1 10	2	0	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+75	Ceiling	Transverse Crack 0 linear ft. and Width = N with Efflorescence and No Active Leak	2 fw & 1 3/4	55	1 55	2	0	55	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+80	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+82	Ceiling	Transverse Crack 8 linear ft. and Width = N with Efflorescence and No Active Leak		8	1 8	2	0	8	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+90	Ceiling	Transverse Crack 10 linear ft. and Width = HL		10	1 10	2	0	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+92	Ceiling	Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak		18	1 18 1 20	2	0	18	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+95	Ceiling	Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+08	Ceiling	Transverse Crack 24 linear ft. and Width = HL with Efflorescence and No Active Leak		20	1 20	2	0	20	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+15 Tunnel - Chesapeake - Supply Duct at Sta. 477+20	Ceiling Ceiling	Transverse Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 20 linear ft. and Width = N with Efflorescence and No Active Leak		4	1 4	2	0	20	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind		Ceiling	Longitudinal Crack 9 linear ft. and Width = N with Efflorescence and No Active Leak		20	1 20	2	, c	20	0	0
	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+20				18	1 18	2	0	18	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+25 Tunnel - Chesapeake - Supply Duct at Sta. 477+30	Ceiling Ceiling	Transverse Crack 18 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 1 linear ft. and Width = N with Efflorescence and No Active Leak		10	1 40	2	, c	40	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+30 Tunnel - Chesapeake - Supply Duct at Sta. 478+62	Ceiling	Transverse Crack 3 linear ft. and Width = HL with Efflorescence and 1 Drips per min		40	1 40	2	Č	0	3	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 470-02	Ceiling	Transverse Crack 7 linear ft. and Width = with Efflorescence and No Active Leak		7	1 7	2	č	7	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+60	Ceiling	Transverse Crack 10 linear ft. and Width = with Efflorescence and No Active Leak		10	1 10	2	č	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+91	Ceiling	Transverse Crack 6 linear ft. and Width = with Efflorescence and No Active Leak		6	1 6	2	č	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+94	Ceiling	Transverse Crack 3 linear ft. and Width = with Efflorescence and No Active Leak		3	1 3	2	č	3	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+20	Ceiling	Transverse Crack 6 linear ft. and Width = with Efflorescence and No Active Leak		6	1 6	2	č	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+25	Ceiling	Transverse Crack 4 linear ft. and Width = with Efflorescence and No Active Leak		4	1 4	2	Ċ	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+91	Ceiling	Transverse Crack 20 linear ft. and Width = N	Corrosion Staining	20	1 20	3	C	0	20	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+30	Ceiling	Transverse Crack 3 linear ft. and Width = N with Efflorescence and No Active Leak	-	3	1 3	2	· · · · · ·	3	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+40	Ceiling	Transverse Crack 3 linear ft. and Width = with Efflorescence and No Active Leak		3	1 3	2	0	3	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+22	Ceiling	Transverse Crack 7 linear ft. and Width = with Efflorescence and No Active Leak		7	1 7	2	0	7	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+33	Ceiling	Transverse Crack 4 linear ft. and Width = N with Efflorescence and No Active Leak		4	1 4	2	0	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+80	Ceiling	Transverse Crack 8 linear ft. and Width = with Efflorescence and No Active Leak		8	1 8	2	0	8	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 488+52	Ceiling	Transverse Crack 16 linear ft. and Width = N with Efflorescence and No Active Leak		16	1 16	2	0	16	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+70	Ceiling	Transverse Crack 4 linear ft. and Width = with Efflorescence and No Active Leak		4	1 4	2	0	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+90	Ceiling	Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak		6	1 6	2	0	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+20	Ceiling	Transverse Crack 4 linear ft. and Width = with Efflorescence and No Active Leak		4	1 4	2	0	4	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+20	Ceiling	Transverse Crack 5 linear ft. and Width = With Efflorescence and No Active Leak		5 15	1 15	2	Ü	15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+85	Ceiling Ceiling	Transverse Crack 15 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 3 linear ft. and Width = N with Efflorescence and No Active Leak		15	1 13	2	, c	15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 495+52 Tunnel - Chesapeake - Supply Duct at Sta. 496+37	Ceiling	Transverse Crack 13 linear ft. and Width = N with Efflorescence and No Active Leak		13	1 13	2	Č	13	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+75	Ceiling	Transverse Crack 9 linear ft, and Width = N with Efflorescence and 1 Drips per min		9	1 9	2	č	9	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+80	Ceiling	Transverse Crack 4 linear ft. and Width = with Efflorescence and 1 Drips per min		4	1 4	2	č	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+15	Ceiling	Transverse Crack 3 linear ft. and Width = with Efflorescence and No Active Leak		3	1 3	2	č	3	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+40	Ceiling	Transverse Crack 6 linear ft. and Width = with Efflorescence and No Active Leak		6	1 6	2	Ċ	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+45	Ceiling	Transverse Crack 0 linear ft. and Width = Delamination of Length 18ft. by 2ft.	Corrosion Staining	18	1 18	3	C	0	18	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+90	Ceiling	Transverse Crack 14 linear ft. and Width = with Efflorescence and No Active Leak		14	1 14	2	C	14	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+30	Ceiling	Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak	all previous transverse	16	1 16	2	c	16	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+32	Ceiling	Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak		16	1 16	2	0	16	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+33	Ceiling	Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak		16	1 16	2	0	16	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+14	Ceiling	Transverse Crack 9 linear ft. and Width = HL with Efflorescence and No Active Leak		9	1 9	2	0	9	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+02	Ceiling	Transverse Crack 6 linear ft. and Width = with Efflorescence and 1 Drips per min		6	1 6	2	0	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+09	Ceiling	Transverse Crack 1 linear ft. and Width = HL		1	1 1	2	0	1	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+20	Ceiling	Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak	Corrosion Staining	12	1 12	3	0	0	12	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+55	Ceiling	Transverse Crack 10 linear ft. and Width = with Efflorescence and 0 Drips per min		10	1 10	2	0	10	U O	U
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+00	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and 0 Drips per min+		6	1 6 1 7	2	0	6	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+65 Tunnel - Chesapeake - Supply Duct at Sta. 504+35	Ceiling Ceiling	Transverse Crack 7 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak		15	1 7 1 15	2	0	7 15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind		Ceiling	Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak		15	1 15	2	0	15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+70 Tunnel - Chesapeake - Supply Duct at Sta. 505+29	Ceiling	Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak		15	1 15	2	,	15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+29 Tunnel - Chesapeake - Supply Duct at Sta. 505+45	Ceiling	Transverse Crack 15 linear ft. and Width = HL with Efforescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak		17	1 15	2	,	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+69	Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescence and No Active Leak		5	1 5	2	č	5	0	0
9/16/2024	WRB. JBB. TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+05 Tunnel - Chesapeake - Supply Duct at Sta. 509+45	Ceiling	Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak		4	1 4	2	r	4	o o	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 509+50	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and 0 Drips per min		6	1 6	2	Č	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 509+65	Ceiling	Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak		4	1 4	2	c	4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 509+80	Ceiling	Transverse Crack 1 linear ft. and Width = HL with Efflorescence and No Active Leak		1	1 1	2	c	1	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 510+18	Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescence and No Active Leak		5	1 5	2	c	5	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 511+05	Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 0 Drips per min		5	1 5	2	C	5	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 513+50	Ceiling	Transverse Crack 2 linear ft. and Width = HL with Efflorescence and No Active Leak		2	1 2	2	0	2	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 513+50	Ceiling	Longitudinal Crack 1 linear ft. and Width = N with Efflorescence and No Active Leak	Corrosion Staining	1	1 1	3	0	0	1	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+15	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak		10	1 10	2	0	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+45	Ceiling	Transverse Crack 13 linear ft. and Width = HL with Efflorescence and No Active Leak		13	1 13	2	0	13	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+50	Ceiling	Transverse Crack 2 linear ft. and Width = HL with Efflorescence and 0 Drips per min		2	1 2	2	0	2	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+90	Ceiling	Transverse Crack 7 linear ft. and Width = HL with Efflorescence and No Active Leak		7	1 7	2	0	7	0	U
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+91	Ceiling	Transverse Crack 7 linear ft. and Width = HL with Efflorescence and No Active Leak		7	1 7	2	0	7	U	U
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 515+00	Ceiling	Transverse Crack 15 linear ft. and Width = with Efflorescence and No Active Leak		15	1 15	2	0	15	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 515+30	Ceiling	Transverse Crack 13 linear ft. and Width = HL with Efflorescence and No Active Leak		13	1 13	2	0	13	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 515+85	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and 0 Drips per min		10	1 10	2	0	10	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 516+05	Ceiling	Transverse Crack 7 linear ft. and Width = HL with Efflorescence and No Active Leak		7	1 7	2	C	7	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 517+05	Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescence and No Active Leak		5	1 5	2	c	5	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 517+25	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak		6	1 6	2	, c	6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 517+25	Ceiling	Transverse Crack 8 linear ft. and Width = HL with Efflorescence and No Active Leak		0	1 8	2	0	0	0	0
9/16/2024			Ceiling			14	1 14	2		1.4	0	0
	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 518+55	•	Transverse Crack 14 linear ft. and Width = HL with Efflorescence and 0 Drips per min				2		14	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 518+70	Ceiling	Transverse Crack 17 linear ft. and Width = HL with Efflorescence and No Active Leak		17	1 17	2	0	17	U	U

Chesapeake Tunnel - Invert Slab

9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 518+85 Tunnel - Chesapeake - Supply Duct at Sta. 519+14 Tunnel - Chesapeake - Supply Duct at Sta. 519+29 Tunnel - Chesapeake - Supply Duct at Sta. 529+80 Tunnel - Chesapeake - Supply Duct at Sta. 520+11 Tunnel - Chesapeake - Supply Duct at Sta. 521+10 Tunnel - Chesapeake - Supply Duct at Sta. 521+25 Tunnel - Chesapeake - Supply Duct at Sta. 521+54 Tunnel - Chesapeake - Supply Duct at Sta. 521+68 Tunnel - Chesapeake - Supply Duct at Sta. 521+78 Tunnel - Chesapeake - Supply Duct at Sta. 521+78 Tunnel - Chesapeake - Supply Duct at Sta. 522+18 Tunnel - Chesapeake - Supply Duct at Sta. 522+50	Ceiling	Transverse Crack 11 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 14 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 10 linear ft. and Width = HL with Efflorescence and 0 Drips per min Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 15 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 11 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 13 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 13 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 16 linear ft. and Width = HL with Efflorescence and No Active Leak		11 1 14 1 10 1 15 1 15 1 10 1 11 1 11 1 12 1 13 1 12 1 16 1 12 1	11 14 10 15 15 10 11 12 13 12 16	2 2 2 2 2 2 2 2 2 2 2 2 2 2	0 11 0 14 0 10 0 15 0 15 0 10 0 11 0 12 0 13 0 12 0 16 0 0 0	0 0 12	0 0 0 0 0 0 0
									0 2434	4 101	40
Delamination/Spalling & 0 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 469+00	Ceiling	Spall length 2ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	F#2 Passageway Area	2 5	10	3	0 0		0
9/16/2024 9/17/2024	WRB, JBB, TMB Rain and Wind JBB5256 WRB, JBB, TMB Rain and Wind JBB5278	Tunnel - Chesapeake - Supply Duct at Sta. 469+00 Tunnel - Chesapeake - Supply Duct at Sta. 523+00	Ceiling Ceiling	Spall length 2ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. Spall length 4ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	F#3 Passageway Area F#2 Passageway Area	2 5 2 4	10 8	3	0 0	10 8	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 469+25	Ceiling	Delamination of Length 1ft. by 2ft.	· ·· · · · · · · · · · · · · · · · · ·	1 2	2	2	0 2	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 469+50 Tunnel - Chesapeake - Supply Duct at Sta. 470+30	Ceiling Ceiling	Spall length 4ft. by width 4ft. // Max Depth in. with 2 Exp. Trans. Bars Delamination of Length 8ft. by 8ft.		4 4 8 8	16 64	2	0 0 0 64	16 . 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 470+48	Ceiling	Delamination of Length 4ft. by 16ft.		4 16	64	2	0 64	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 470+58 Tunnel - Chesapeake - Supply Duct at Sta. 471+25	Ceiling Ceiling	Delamination of Length 3ft. by 15ft. Delamination of Length 6ft. by 4ft.		3 15 6 4	45 24	2	0 45	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+30 Tunnel - Chesapeake - Supply Duct at Sta. 471+40	Ceiling Ceiling	Delamination of Length 4ft. by 8ft. Delamination of Length 3ft. by 6ft.		4 8	32 18	2	0 32	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+40 Tunnel - Chesapeake - Supply Duct at Sta. 471+55	Ceiling	Delamination of Length 1ft. by 3ft.		1 3	3	2	0 3	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+65 Tunnel - Chesapeake - Supply Duct at Sta. 471+65	Ceiling Ceiling	Delamination of Length 3ft. by 12ft. Delamination of Length 17ft. by 2ft.	With Cracking Corrosion Staining for 2sf	3 12 17 2	36 34	3 2/3	0 0	36	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+75	Ceiling	Delamination of Length 3ft. by 12ft.	CONTOSION Stalling for 251	3 12	36	2	0 36	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+80 Tunnel - Chesapeake - Supply Duct at Sta. 471+85	Ceiling Ceiling	Delamination of Length 2ft. by 6ft. Delamination of Length 2ft. by 2ft.		2 6 2 2	12 4	2	0 12 0 4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+85	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0 4	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+85 Tunnel - Chesapeake - Supply Duct at Sta. 471+90	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 4ft. by 20ft.	d cr patch	4 20	4 80	2	0 4	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 471+92 Tunnel - Chesapeake - Supply Duct at Sta. 472+01	Ceiling Ceiling	Delamination of Length 4ft. by 20ft. Delamination of Length 1ft. by 1ft.	Corrosion Staining for 2sf	4 20	80	2/3	0 78	2	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 472+40	Ceiling	Delamination of Length 15ft. by 2ft.		15 2	30	2	0 30	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 472+60 Tunnel - Chesapeake - Supply Duct at Sta. 472+62	Ceiling Ceiling	Delamination of Length 4ft. by 18ft. Delamination of Length 4ft. by 20ft.	Corrosion Staining for 10sf	4 18 4 20	72 80	2 2/3	0 72	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 472+62	Ceiling	Delamination of Length 3ft. by .5ft.		3 0.5	1.5	2	0 1.5	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 472+90 Tunnel - Chesapeake - Supply Duct at Sta. 472+95	Ceiling Ceiling	Delamination of Length 1ft. by 1ft. Delamination of Length 9ft. by 3ft.	Repair	1 1 9 3	1 27	2	0 1 0 27	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 472+95 Tunnel - Chesapeake - Supply Duct at Sta. 473+00	Ceiling Ceiling	Delamination of Length 4ft. by 4ft. Delamination of Length 3ft. by 1ft.	Repair	0 0	16 3	2	0 16	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+00 Tunnel - Chesapeake - Supply Duct at Sta. 473+05	Ceiling	Delamination of Length Srt. by 1rt. Delamination of Length 1ft. by 3ft.	керан	1 3	3	2	0 3	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 473+35 Tunnel - Chesapeake - Supply Duct at Sta. 474+10	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 3ft. by 12ft.		2 2 3 12	4 36	2	0 4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 474+10	Ceiling	Delamination of Length 3ft. by 15ft.	Corrosion Staining for 1sf	3 15	45	2/3	0 44	1	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind JBB5329	Tunnel - Chesapeake - Supply Duct at Sta. 474+15 Tunnel - Chesapeake - Supply Duct at Sta. 474+30	Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars 3' Width By 4' Length Area of Corrosion and Efflorescence		1 1 3 4	1 12	3	0 0	1 12	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind JBB5262	Tunnel - Chesapeake - Supply Duct at Sta. 474+40	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. 15' Width by 5' Length Repair area with Map Cracking		2 2 15 5	4 75	2	0 4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 474+40 Tunnel - Chesapeake - Supply Duct at Sta. 474+56	Ceiling	Delamination of Length 2ft. by 18ft.	Corrosion Staining for 4 sf	2 18	36	2/3	0 32	4	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 474+85 Tunnel - Chesapeake - Supply Duct at Sta. 475+00	Ceiling Ceiling	Delamination of Length 3ft. by 24ft. Delamination of Length 1ft. by 1ft.		3 20 1 1	60 1	2	0 60 0 1	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+40 Tunnel - Chesapeake - Supply Duct at Sta. 475+65	Ceiling Ceiling	Delamination of Length 3ft. Delamination of Length 3ft. by 20 ft.		3 3 3 20	9 60	2	0 9	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+75	Ceiling	Delamination of Length 2ft. by 3ft.		2 3	6	2	0 6	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+75 Tunnel - Chesapeake - Supply Duct at Sta. 475+80	Ceiling Ceiling	Delamination of Length 3ft. by 4ft. Delamination of Length 2ft. by 3ft.		3 4 2 3	12 6	2	0 12 0 6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+85	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0 4	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind JBB5330 WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 475+90 Tunnel - Chesapeake - Supply Duct at Sta. 475+90	Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars Delamination of Length 3ft. by 3ft.	Around Access Door	3 3	9	2	0 9	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+15 Tunnel - Chesapeake - Supply Duct at Sta. 476+15	Ceiling Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 3ft. by 3ft.		2 3	6	2	0 6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+30	Ceiling	Delamination of Length 2ft. by 5ft. with Corrosion Staining		2 5	10	3	0 0	10	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+32 Tunnel - Chesapeake - Supply Duct at Sta. 476+50	Ceiling Ceiling	Delamination of Length 24ft. by 3ft. Delamination of Length 6ft. by 6ft.		20 3 6 6	60 36	2	0 60 0 36	. 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+54	Ceiling	Delamination of Length 4ft. by 4ft.		4 4	16 4	2	0 16	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+55 Tunnel - Chesapeake - Supply Duct at Sta. 476+65	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 2ft. by 20ft.		2 2	40	2	0 40	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+66 Tunnel - Chesapeake - Supply Duct at Sta. 476+70	Ceiling Ceiling	Delamination of Length 4ft. by 3ft. Delamination of Length 6ft. by 6ft.		4 3	12 36	2	0 12	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+80	Ceiling	Delamination of Length 12ft. by 5ft.		12 5	60	2	0 60	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+80 Tunnel - Chesapeake - Supply Duct at Sta. 476+85	Ceiling Ceiling	Delamination of Length 1ft. by 3ft. Delamination of Length 2ft. by 2ft.		1 3 2 2	3 4	2 2	0 3	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+90	Ceiling	Delamination of Length 4ft. by 12ft.		4 12	48 4	2	0 48	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 476+92 Tunnel - Chesapeake - Supply Duct at Sta. 477+08	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 1ft. by 1ft.		1 1	1	2	0 1	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+15 Tunnel - Chesapeake - Supply Duct at Sta. 477+20	Ceiling Ceiling	Delamination of Length 3ft. by 3ft. Delamination of Length 2ft. by 3ft.		3 3	9	2	0 9	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+20	Ceiling	Delamination of Length 5ft. by 5ft.		5 5	25	2	0 25	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+25 Tunnel - Chesapeake - Supply Duct at Sta. 477+30	Ceiling Ceiling	Delamination of Length 3ft. by 15ft. Delamination of Length 20ft. by 3ft.		3 15 20 3	45 60	2	0 45	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 477+60 Tunnel - Chesapeake - Supply Duct at Sta. 477+75	Ceiling Ceiling	Delamination of Length 20ft. by 4ft. Delamination of Length 4ft. by 20ft.	Repair	20 4 4 20	80 80	2	0 80	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+10	Ceiling	Delamination of Length 2ft. by 8ft.		2 8	16	2	0 16	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+30 Tunnel - Chesapeake - Supply Duct at Sta. 478+33	Ceiling Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 0ft. by 0ft.	Repair	2 3 0 0	6	2	0 6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+40	Ceiling	Delamination of Length 4ft. by 6ft.	·	4 6	24 60	2	0 24	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+45 Tunnel - Chesapeake - Supply Duct at Sta. 478+58	Ceiling Ceiling	Delamination of Length 3ft. by 20ft. Delamination of Length 1ft. by 1ft.		3 20 1 1	1	2/3 2	0 54	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+67 Tunnel - Chesapeake - Supply Duct at Sta. 478+70	Ceiling Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 5ft. by 5ft.		2 3	6 25	2	0 6	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 478+95	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0 4	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 479+10 Tunnel - Chesapeake - Supply Duct at Sta. 479+15	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 2ft. by 4ft.		2 2 2 4	4 8	2 2	0 4 0 8	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 479+15	Ceiling	Delamination of Length 3ft. by 6ft.		3 6	18	2	0 18	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 479+80 Tunnel - Chesapeake - Supply Duct at Sta. 479+85	Ceiling Ceiling	Delamination of Length 2ft. by 5ft. Delamination of Length 1ft. by 6ft.		2 5 1 6	10 6	2	0 10	0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 480+00 Tunnel - Chesapeake - Supply Duct at Sta. 480+22	Ceiling Ceiling	Deteriorated Repair Delamination of Length 2ft. by 4ft.		2 2	4 8	2	0 4	0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 480+32	Ceiling	Spall length 1ft. by width 4ft. // Max Depth in. with 1 Exp. Trans. Bars		1 4	4	3	0 0	4	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 480+40	Ceiling	Delamination of Length 3ft. by 20ft.		3 20	60	2	υ 60	0	U

Chesapeake Tunnel - Invert Slab

9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 480+40	Ceiling	Delamination of Length 20ft. by 4ft.	Corrosion Staining for 1sft	20 4 4 20	80 80	2/3	0	79 1 80 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 480+75 Tunnel - Chesapeake - Supply Duct at Sta. 481+02	Ceiling Ceiling	Delamination of Length 4ft. by 24ft. Delamination of Length 3ft. by 15ft.	Corrosion Staining	3 15	45	3	0	0 45	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+15	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	ō	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+55	Ceiling	Delamination of Length 4ft. by 24ft.		4 20	80	2	0	80 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+60	Ceiling	Delamination of Length 3ft. by 4ft.		3 4	12	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 481+60	Ceiling	Delamination of Length 4ft. by 3ft.		4 3	12 12	2	0	12 0 12 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+10 Tunnel - Chesapeake - Supply Duct at Sta. 482+15	Ceiling Ceiling	Delamination of Length 2ft. by 6ft. Delamination of Length 2ft. by 18ft.		2 18	36	2	0	36 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+30	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	ő	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+75	Ceiling	Delamination of Length 1ft. by 3ft.		1 3	3	2	0	3 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 482+95	Ceiling	Delamination of Length 4ft. by 3ft.		4 3	12	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+15	Ceiling	Delamination of Length 3ft. by 24ft.		3 20	60	2	0	60 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+18	Ceiling	Delamination of Length 2ft. by 2ft.	Repair	2 2	4	2	0	4 0 92 8	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+30 Tunnel - Chesapeake - Supply Duct at Sta. 483+48	Ceiling Ceiling	Delamination of Length 5ft. by 20ft. Delamination of Length 2ft. by 2ft.	Corrosion Staining for 8sf	5 20	100 4	2/3	0	92 8	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 463+48 Tunnel - Chesapeake - Supply Duct at Sta. 483+50	Ceiling	Delamination of Length 5ft. by 5ft.	Repair	5 5	25	2	0	25 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+55	Ceiling	Delamination of Length 4ft. by 5ft.	Repair	4 5	20	2	ő	20 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+62	Ceiling	Delamination of Length 15ft. by ft.	Repair	15 1	15	2	0	15 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 483+85	Ceiling	Delamination of Length 16ft. by 3ft.	Corrosion Staining for 1sf	16 3	48	2/3	0	47 1	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+12	Ceiling	Delamination of Length 3ft. by 10ft.		3 10	30	2	0	30 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+40	Ceiling	Delamination of Length 3ft. by 24ft.		3 20	60	2	0	60 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+45 Tunnel - Chesapeake - Supply Duct at Sta. 484+50	Ceiling Ceiling	Delamination of Length 3ft. by 4ft. Delamination of Length 12ft. by 2ft.		12 2	12 24	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+50	Ceiling	Delamination of Length 12ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+67	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	ő	2 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+75	Ceiling	Delamination of Length 3ft. by 6ft.		3 6	18	2	Ō	18 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+10	Ceiling	elamination of Length 7ft. by 3ft.	Corrosion Staining	7 3	21	3	0	0 21	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 484+98	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+15	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+22 Tunnel - Chesapeake - Supply Duct at Sta. 485+28	Ceiling Ceiling	Delamination of Length 2ft. by 6ft. Delamination of Length 4ft. by 15ft.	Corrosion Staining	4 15	12 60	2	0	12 0 0 60	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+26 Tunnel - Chesapeake - Supply Duct at Sta. 485+35	Ceiling	Delamination of Length 41t. by 45t.	Repair	15 8	120	2	0	120 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+50	Ceiling	Delamination of Length 3ft. by 4ft.		3 4	12	2	ō	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+55	Ceiling	Delamination of Length 2ft. by 8ft.		2 8	16	2	0	16 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 485+65	Ceiling	Delamination of Length 2ft. by 6ft.		2 6	12	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+08	Ceiling	Delamination of Length 3ft. by 3ft.		3 3	9	2	0	9 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+15	Ceiling	Delamination of Length 6ft. by 20ft.		6 20	120	2	0	120 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+20 Tunnel - Chesapeake - Supply Duct at Sta. 486+20	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 3ft. by 6ft.		2 2	4 18	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+45	Ceiling	Delamination of Length 3ft. by 4ft. Delamination of Length 3ft. by 10ft.		3 10	30	2	0	30 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+55	Ceiling	Delamination of Length 2ft. by 4ft.		2 4	8	2	ō	8 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+60	Ceiling	Spall length 2ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars		2 3	6	3	0	0 6	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+60	Ceiling	Delamination of Length 3ft. by 4ft.		3 4	12	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+80	Ceiling	Delamination of Length 2ft. by 3ft.		2 3	6	2	0	6 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 486+85	Ceiling	Delamination of Length 3ft. by 4ft.	Danais	3 4	12 28	2	0	12 0 28 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+35 Tunnel - Chesapeake - Supply Duct at Sta. 487+45	Ceiling Ceiling	Delamination of Length 7ft. by 4ft. Delamination of Length 3ft. by 2ft.	Repair	7 4	6	2	0	6 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+50	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+75	Ceiling	Delamination of Length 3ft. by 2ft.		2 3	6	2	ō	6 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+75	Ceiling	Spall length 3ft. by width 3ft. // Max Depth in. with 1 Exp. Long. Bars and 4 Exp. Trans. Bars. // Approx. 25% Section Loss		3 3	9	4	0	0 0	9
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 487+80	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 488+00	Ceiling	Delamination of Length 3ft. by 20ft.	Corrosion Staining p 8sf d	3 20	60	2/3	0	59 1	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 488+00	Ceiling	Delamination of Length 3ft. by 24ft.		3 20 3 20	60 60	2	0	60 0 60 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 488+70 Tunnel - Chesapeake - Supply Duct at Sta. 488+80	Ceiling Ceiling	Delamination of Length 3ft. by 24ft. Delamination of Length 3ft. by 3ft.		3 20	9	2	0	9 O	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 488+90	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+03	Ceiling	Delamination of Length 5ft. by 24ft.		5 20	100	2	ō	100 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+05	Ceiling	Delamination of Length 4ft. by 20ft.	Corrosion Staining d p e	4 20	80	3	0	0 80	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+32	Ceiling	Delamination of Length 2ft. by 5ft.	Corrosion Staining d p e	2 5	10	3	0	0 10	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+50	Ceiling	Delamination of Length 4ft. by 8ft.		4 8	32	2	0	32 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+70 Tunnel - Chesapeake - Supply Duct at Sta. 489+80	Ceiling Ceiling	Delamination of Length 2ft. by 4ft. Delamination of Length 1ft. by 2ft.		2 4	8 2	2	0	2 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+80	Ceiling	Delamination of Length 1rt. by 2rt.		2 2	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 489+95	Ceiling	Delamination of Length 20ft. by 1ft.	Repair	20 1	20	2	0	20 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 490+13	Ceiling	Delamination of Length 14ft. by 3ft.	Corrosion Staining for 3sf	14 3	42	2/3	0	3 39	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 490+30	Ceiling	Delamination of Length 3ft. by 7ft.		3 7	21	2	0	21 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 490+40	Ceiling	Delamination of Length 2ft. by 18ft.		2 18 2 6	36 12	2	0	36 0 12 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 490+65 Tunnel - Chesapeake - Supply Duct at Sta. 490+70	Ceiling Ceiling	Delamination of Length 2ft. by 6ft. Delamination of Length 2ft. by 6ft.		2 6	12	2	0	12 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 490+90		Delamination of Length 2ft. by 18ft.		4 18	72	2	ő	72 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+02		Delamination of Length 2ft. by 3ft.		2 3	6	2	Ō	6 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+25	Ceiling	Delamination of Length 5ft. by 4ft.		5 4	20	2	0	20 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+50	Ceiling	Delamination of Length 15ft. by 2ft.	Corrosion Staining	15 2	30	3	0	0 30	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+65 Tunnel - Chesapeake - Supply Duct at Sta. 491+70	Ceiling Ceiling	Delamination of Length 3ft. by 12ft. Spall length 1ft. by width 1ft. // Max Depth in. with Approx. % Section Loss		3 12 1 1	36 1	2	0	30 0 0 1	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+70 Tunnel - Chesapeake - Supply Duct at Sta. 491+78	Ceiling	Delamination of Length 7ft. by 2ft.	Repair	7 2	14	2	0	14 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+78	Ceiling	Delamination of Length 1ft. by 2ft.	•	1 2	2	2	0	2 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 491+90	Ceiling	Delamination of Length 2ft. by 3ft.		2 3	6	2	0	6 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+00	Ceiling	Delamination of Length 2ft. by 18ft.		2 18	36	2	0	36 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+08	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+10 Tunnel - Chesapeake - Supply Duct at Sta. 492+20	Ceiling Ceiling	Delamination of Length 3ft. by 6ft. Delamination of Length 3ft. by 15ft.		3 6 3 15	18 45	2	0	18 0 45 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+20 Tunnel - Chesapeake - Supply Duct at Sta. 492+38	Ceiling	Delamination of Length 3rt. by 13rt. Delamination of Length 2ft. by 21ft. Spall length 2ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars		2 3	6	3	0	0 6	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+38	Ceiling	Delamination of Length 2ft. by 21ft. Spall length 2ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars		2 20	34	2	0	34 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+45	Ceiling	Delamination of Length 2ft. by 15ft.		2 15	30	2	0	30 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+46	Ceiling	Delamination of Length 3ft. by 3ft.	Repair	3 3	9	2	0	9 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+50	Ceiling	Delamination of Length 1ft. by 4ft. Polamination of Length 2ft, by 2ft.		1 4	4	2	0	4 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+65 Tunnel - Chesapeake - Supply Duct at Sta. 492+80	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 2ft. by 3ft.		2 2	6	2	n	4 U	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 492+80 Tunnel - Chesapeake - Supply Duct at Sta. 492+85	Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 3ft. by 7ft.		3 7	21	2	0	21 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+00	Ceiling	Delamination of Length 1ft. by 4ft.		1 4	4	2	0	4 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+05	Ceiling	Delamination of Length 3ft. by 7ft.		3 7	21	2	0	21 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+20	Ceiling	Delamination of Length 3ft. by 12ft.		3 12	36	2	0	36 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+25	Ceiling	Delamination of Length 1ft. by 3ft.	Consolar Stalalar	1 3	3	2	0	3 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+34	Ceiling	Delamination of Length 8ft. by 2ft. Delamination of Length 8ft. by 6ft.	Corrosion Staining	8 2	16	3	0	0 16	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+40 Tunnel - Chesapeake - Supply Duct at Sta. 493+70	Ceiling Ceiling	Delamination of Length 3ft. by 6ft. Delamination of Length 1ft. by 10ft.		3 6 1 10	18 10	2	0	18 0 10 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+70 Tunnel - Chesapeake - Supply Duct at Sta. 493+80	Ceiling	Delamination of Length 1ft. by 10ft. Delamination of Length 3ft. by 20ft.	Corrosion Staining for 8 sf	3 20	60	2/3	0	52 8	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+80	Ceiling	Delamination of Length 3ft. by 12ft.		3 12	36	2	0	36 0	Ö
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 493+95	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+00	Ceiling	Delamination of Length 3ft. by 6ft.		3 6	18	2	0	18 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+15	Ceiling	Delamination of Length 2ft. by 10ft.		2 10	20	2	0	20 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+26	Ceiling	Delamination of Length 2ft. by 6ft.	Correction Statistics	2 6	12	2	0	12 0	0
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+32 Tunnel - Chesapeake - Supply Duct at Sta. 494+35	Ceiling Ceiling	Delamination of Length 2ft. by 8ft. Delamination of Length 3ft. by 12ft.	Corrosion Staining	2 8 3 12	16 36	3	0	0 16 36 0	0
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+35 Tunnel - Chesapeake - Supply Duct at Sta. 494+45	Ceiling	Delamination of Length 3rt. by 12rt. Delamination of Length 2ft. by 5ft.		2 5	10	2	0	10 0	0
		• • • • •	· ·								

9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+50	Ceiling	Delamination of Length 16ft. by 2ft.	Repair	16 2 2	2 2 0 0	,
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+60	Ceiling	Delamination of Length 10tt. by 2ft. Delamination of Length 2ft. by 3ft with Transverse Crack and 4 Drips per min	первії	2 3 6	0 0 0 6	<u>ز</u>
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+75	Ceiling	Delamination of Length 2ft. by 14ft.	Corrosion Staining	2 14 28	8 2 0 28 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 494+90	Ceiling	Delamination of Length 2ft. by 8ft.	Repair	2 8 10		1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 495+07	Ceiling	Delamination of Length 4ft. by 4ft.		4 4 16		•
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 495+35 Tunnel - Chesapeake - Supply Duct at Sta. 495+53	Ceiling Ceiling	Delamination of Length 3ft. by 18ft. Delamination of Length 1ft. by 4ft.		3 18 54 1 4 4		1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 495+60	Ceiling	Delamination of Length 2ft. by 9ft.		2 9 18	8 2 0 18 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 495+70	Ceiling	Delamination of Length 2ft. by 18ft.		2 18 36		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+25	Ceiling	Delamination of Length 2ft. by 4ft.		2 4 8		
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+30 Tunnel - Chesapeake - Supply Duct at Sta. 496+40	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 2ft. by 2dft.	Corrosion Staining for 1sf	2 2 4	· · · · · · · · · · · · · · · · · · ·)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+70	Ceiling	Delamination of Length 2ft. by 20ft.	Repair	3 20 60		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+87	Ceiling	Delamination of Length 20ft. by 2ft.	Repair	20 2 40	0 2 0 40 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+90	Ceiling	Delamination of Length 3ft. by 6ft.		3 6 18		į.
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 496+95 Tunnel - Chesapeake - Supply Duct at Sta. 497+20	Ceiling	Delamination of Length 4ft. by 4ft.		4 4 16 3 15 49		•
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+20 Tunnel - Chesapeake - Supply Duct at Sta. 497+26	Ceiling Ceiling	Delamination of Length 3ft. by 15ft. Delamination of Length 2ft. by 15ft.	Repair	2 15 30)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+40	Ceiling	Delamination of Length 3ft. by 24ft.		3 20 60		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+50	Ceiling	Delamination of Length 4ft. by 4ft.		4 4 16	6 2 0 16 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+70	Ceiling	Delamination of Length 3ft. by 3ft.		3 3 9		
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 497+80 Tunnel - Chesapeake - Supply Duct at Sta. 497+95	Ceiling Ceiling	Delamination of Length 3ft. by 18ft. Delamination of Length 3ft. by 6ft.		3 18 5 ⁴ 3 6 18)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+00	Ceiling	Delamination of Length 1ft. by 1ft.		1 1 1	2 0 1 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+10	Ceiling	Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+10	Ceiling	Delamination of Length 2ft. by 5ft.		2 5 10		1
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+15	Ceiling Ceiling	Delamination of Length 17ft. by 2ft.	Repair Repair	17 2 34 3 20 60		,
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+19 Tunnel - Chesapeake - Supply Duct at Sta. 498+55	Ceiling	Delamination of Length 3ft. by 20ft. Delamination of Length 2ft. by 2ft.	керан	2 2 4)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+70	Ceiling	Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 498+75	Ceiling	Delamination of Length 2ft. by 7ft.		2 7 14		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+02	Ceiling	Delamination of Length 2ft. by 10ft.		2 10 20		1
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+06 Tunnel - Chesapeake - Supply Duct at Sta. 499+15	Ceiling Ceiling	Delamination of Length 1ft. by 7ft. Delamination of Length 1ft. by 2ft.		1 / /	2 0 7 0 0 2 0 0	1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+20	Ceiling	Delamination of Length 2ft. by 4ft.		2 4 8	2 0 8 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+40	Ceiling	Delamination of Length 1ft. by 4ft.		1 4 4	2 0 4 0 0	1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+40	Ceiling	Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0	,
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+50 Tunnel - Chesapeake - Supply Duct at Sta. 499+55	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 3ft. by 2ft.		2 2 4	2 0 4 0 0	
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+65	Ceiling	Delamination of Length 3ft. by 2ft. Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+74	Ceiling	Delamination of Length 3ft. by 20ft.	Corrosion Staining	3 20 60	0 2 0 60 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+85	Ceiling	Delamination of Length 1ft. by 1ft.		1 1 1	2 0 1 0 0	i
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+87	Ceiling	Delamination of Length 1ft. by 1ft.		1 1 1	2 0 1 0 0	
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+98 Tunnel - Chesapeake - Supply Duct at Sta. 499+99	Ceiling Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 1ft. by 3ft.		2 2 4	0 4 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 499+99 Tunnel - Chesapeake - Supply Duct at Sta. 500+05	Ceiling	Delamination of Length 1rt. by 3rt. Delamination of Length 1ft. by 2ft.		1 2 2	2 0 2 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+05	Ceiling	Delamination of Length 1ft. by 4ft.		1 4 4	2 0 4 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+15	Ceiling	Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+15	Ceiling	Delamination of Length 3ft. by 24ft.	Carradan Staining	3 20 60 19 2 38		•
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+46 Tunnel - Chesapeake - Supply Duct at Sta. 500+52	Ceiling Ceiling	Delamination of Length 19ft. by 2ft. Delamination of Length 1ft. by 2ft.	Corrosion Staining	19 2 30	0 38 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+62	Ceiling	Delamination of Length 2ft. by 5ft.		2 5 10	0 2 0 10 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+65	Ceiling	Delamination of Length 3ft. by 5ft.		3 5 19	5 2 0 15 0 0	1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+66	Ceiling	Delamination of Length 2ft. by 2ft.	Repair	2 2 4	· · · · · · · · · · · · · · · · · · ·	
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+75 Tunnel - Chesapeake - Supply Duct at Sta. 500+85	Ceiling Ceiling	Delamination of Length 2ft. by 24ft. Delamination of Length 17ft. by 4ft.	Corrosion Staining	2 20 40 17 4 68)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 500+88	Ceiling	Delamination of Length 17tt. by 4ft.	COTTOSION Staining	1 2 2		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+15	Ceiling	Delamination of Length 3ft. by 15ft.	Corrosion Staining	3 15 49	5 2 0 45 0 0	J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+40	Ceiling	Delamination of Length 1ft. by 1ft.		1 1 1	2 0 1 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+65	Ceiling	Delamination of Length 4ft. by 5ft.		4 5 20	0 2 0 0 0	•
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+75 Tunnel - Chesapeake - Supply Duct at Sta. 501+85	Ceiling Ceiling	Delamination of Length 3ft. by 3ft. Delamination of Length 3ft. by 24ft.		3 20 60	0 9 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+90	Ceiling	Delamination of Length 3ft. by 6ft.		3 6 18		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+92	Ceiling	Delamination of Length 2ft. by 6ft.		2 6 12	2 0 12 0 0	j
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+95	Ceiling Ceiling	Delamination of Length 1ft, by 3ft.		3 3 9	0 9 0 0	
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 501+99 Tunnel - Chesapeake - Supply Duct at Sta. 502+02	Ceiling	Delamination of Length 1ft. by 2ft. Delamination of Length 3ft. by 5ft.		3 5 1	5 2 0 15 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+04	Ceiling	Delamination of Length 4ft. by 6ft.		4 6 24		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+15	Ceiling	Delamination of Length 2ft. by 4ft.		2 4 8	· -	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+20	Ceiling	Delamination of Length 4ft. by 6ft.		4 6 24 2 5 10		•
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+25 Tunnel - Chesapeake - Supply Duct at Sta. 502+25	Ceiling Ceiling	Delamination of Length 2ft. by 5ft. Delamination of Length 3ft. by 8ft.		2 5 10 3 8 24		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+27	Ceiling	Delamination of Length 2ft. by 4ft.		2 4 8		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+30	Ceiling	Delamination of Length 3ft. by 5ft.		3 5 1		J.
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+35 Tunnel - Chesapeake - Supply Duct at Sta. 502+37	Ceiling Ceiling	Delamination of Length 4ft. by 6ft. Delamination of Length 2ft. by 7ft.		4 6 24 2 7 14		1
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+40	Ceiling	Delamination of Length 2ft. by 7ft. Delamination of Length 3ft. by 8ft.		3 8 24		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+42	Ceiling	Delamination of Length 3ft. by 5ft.		3 5 1		i
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+50	Ceiling	Delamination of Length 3ft. by 18ft.		3 18 54		,
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+55 Tunnel - Chesapeake - Supply Duct at Sta. 502+58	Ceiling Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 2ft. by 2ft.		2 3 6		1
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+58 Tunnel - Chesapeake - Supply Duct at Sta. 502+60	Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 20ft. by 5ft.	Corrosion Staining	2 2 4	· · · · · · · · · · · · · · · · · · ·	ر
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 502+60	Ceiling	Delamination of Length 6ft. by 24ft.		6 20 12		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+08	Ceiling	Delamination of Length 2ft. by 2ft.		2 2 4	2 0 4 0 0	J.
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+10 Tunnel - Chesapeake - Supply Duct at Sta. 503+35	Ceiling Ceiling	Delamination of Length 1ft. by 2ft. Delamination of Length 4ft. by 20ft.	Corrosion Staining for 4sf	1 2 2 4 20 80	2 2 0 0 0 0 2/3 0 76 4 0	1
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+35 Tunnel - Chesapeake - Supply Duct at Sta. 503+45	Ceiling	Delamination of Length 4rt. by 20rt. Delamination of Length 1ft. by 12ft.	COLLOSION STAINING TO 451	4 20 80 1 12 12		ر
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+47	Ceiling	Delamination of Length 3ft. by 10ft.		3 10 30	0 2 0 30 0 0	j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+50	Ceiling	Delamination of Length 4ft. by 24ft.		4 20 80		J.
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+60 Tunnel - Chesapeake - Supply Duct at Sta. 503+63	Ceiling Ceiling	Delamination of Length 2ft. by 7ft.		2 7 14 5 10 50		1
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+63 Tunnel - Chesapeake - Supply Duct at Sta. 503+70	Ceiling	Delamination of Length 5ft. by 10ft. Delamination of Length 20ft. by 3ft.	Corrosion Staining	20 3 60		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+80	Ceiling	Delamination of Length 26tt. by 16ft.	Corrosion Staining	2 16 32	2 0 32 0 0)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 503+85	Ceiling	Delamination of Length 4ft. by 6ft.		4 6 24		į.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+08	Ceiling	Delamination of Length 4ft. by 20ft.	Corrosion Staining for 2sf	4 20 80	,-	
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+18 Tunnel - Chesapeake - Supply Duct at Sta. 504+20	Ceiling Ceiling	Delamination of Length 3ft. by 5ft. Delamination of Length 2ft. by 4ft.	p nw	3 5 1! 2 4 8)
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+20 Tunnel - Chesapeake - Supply Duct at Sta. 504+20	Ceiling	Delamination of Length 2ft. by 4ft. Delamination of Length 2ft. by 4ft.	γ ·· ··	2 4 8		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+30	Ceiling	Delamination of Length 3ft. by 18ft.		3 18 54		j
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+35	Ceiling	Delamination of Length 3ft. by 6ft.		3 6 18		
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+48 Tunnel - Chesapeake - Supply Duct at Sta. 504+55	Ceiling Ceiling	Delamination of Length 4ft. by 6ft. Delamination of Length 2ft. by 4ft.		4 6 24 2 4 8		1
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+55 Tunnel - Chesapeake - Supply Duct at Sta. 504+65	Ceiling	Delamination of Length 2ft. by 4ft. Delamination of Length 2ft. by 5ft. with Corrosion Staining		2 4 8		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+78	Ceiling	Delamination of Length 3ft. by 20ft.	Corrosion Staining	3 20 60		J
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 504+88	Ceiling	Delamination of Length 3ft. by 4ft.		3 4 12		į.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+01	Ceiling	Delamination of Length 3ft. by 3ft.		3 3 9	2 0 9 0 0	,
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+08	Ceiling	Delamination of Length 3ft. by 3ft.		3 3 9	0 9 0 0	

9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+10	Ceiling	Delamination of Length 2ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+20	Ceiling	Delamination of Length 20ft. by 10ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+35	Ceiling	Delamination of Length 201t. by 101t. Delamination of Length 3ft. by 5ft.
9/16/2024	WRB. JBB. TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+45	Ceiling	Delamination of Length 4ft. by 4ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+48	Ceiling	Delamination of Length 3ft. by 20ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+65	Ceiling	Delamination of Length 2ft. by 10ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+68	Ceiling	Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+70	Ceiling	Delamination of Length 20ft. by 2ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+85	Ceiling	Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+95	Ceiling	Delamination of Length 2ft. by 2ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 505+95	Ceiling	Delamination of Length 5ft. by 8ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+05	Ceiling	Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+10	Ceiling	Delamination of Length 4ft. by 7ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+13	Ceiling	Delamination of Length 4ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+30	Ceiling	Delamination of Length 4ft. by 20ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+45	Ceiling	Delamination of Length 5ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+47	Ceiling	Delamination of Length 2ft. by 12ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+50	Ceiling	Delamination of Length 3ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+52	Ceiling	Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+60	Ceiling	Delamination of Length 3ft. by 24ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+75	Ceiling	Delamination of Length 2ft. by 24ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 506+80	Ceiling	Delamination of Length 2ft. by 15ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+00	Ceiling	Spall length 1ft. by width 2ft. // Max Depth in. with 1 Exp. Trans. Bars
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+10	Ceiling	Delamination of Length 3ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+15	Ceiling	Delamination of Length 4ft. by 18ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+30	Ceiling	Delamination of Length 2ft. by 6ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+35	Ceiling	Delamination of Length 2ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+50	Ceiling	Delamination of Length 4ft. by 15ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+75	Ceiling	Delamination of Length 4ft. by 8ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 507+80	Ceiling	Delamination of Length 2ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+00	Ceiling	Spall length 1ft. by width 20ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+10	Ceiling	Delamination of Length 2ft. by 4ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+25	Ceiling	Delamination of Length 1ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+35	Ceiling	Delamination of Length 3ft. by 5ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+35	Ceiling	Delamination of Length 4ft. by 12ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 508+40	Ceiling	Delamination of Length 4ft. by 6ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 510+50	Ceiling	Delamination of Length 17ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 510+60	Ceiling	Delamination of Length 8ft. by 2ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 510+66	Ceiling	Delamination of Length 6ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 5101+80	Ceiling	Delamination of Length 18ft. by 6ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 511+00	Ceiling	Delamination of Length 3ft. by 15ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 511+90	Ceiling	Delamination of Length 20ft. by 8ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 512+22	Ceiling	Delamination of Length 18ft. by 11ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 512+50	Ceiling	Delamination of Length 20ft. by 14ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 513+28	Ceiling	Delamination of Length 3ft. by 20ft.
9/16/2024 9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 513+65 Tunnel - Chesapeake - Supply Duct at Sta. 514+00	Ceiling Ceiling	Delamination of Length 9ft. by 4ft. Delamination of Length 6ft. by 20ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 514+60 Tunnel - Chesapeake - Supply Duct at Sta. 514+80	Ceiling	Delamination of Length 81t. by 20tt. Delamination of Length 20ft. by 7ft.
9/16/2024				
9/16/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 515+25 Tunnel - Chesapeake - Supply Duct at Sta. 515+40	Ceiling Ceiling	Delamination of Length 20ft. by 4ft. Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 515+40	Ceiling	Delamination of Length 2ft. by 3ft. Delamination of Length 3ft. by 20ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 516+20	Ceiling	Delamination of Length 3ft. by 20ft. Delamination of Length 2ft. by 14ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 516+20	Ceiling	Delamination of Length 2ft. by 2ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 516+55	Ceiling	Delamination of Length 31t. by 41t. Delamination of Length 20ft. by 7ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 516+90	Ceiling	Delamination of Length 20ft. by 7ft. Delamination of Length 20ft. by 15ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 510+50	Ceiling	Scientification of Congail 2016 by 1316
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 517+20 Tunnel - Chesapeake - Supply Duct at Sta. 517+50	Ceiling	Delamination of Length 2ft. by 10ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 517+50	Ceiling	Delamination of Length 4ft. by 12ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 518+82	Ceiling	Delamination of Length 4rt. by 12rt. Delamination of Length 2ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 519+50	Ceiling	Delamination
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 519+63	Ceiling	Spall length 1ft. by width 20ft. // Max Depth 0 1/2 in. with Exp. Long. Bars and Exp. Trans. Bars
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 520+50	Ceiling	Delamination of Length 3ft. by 3ft.
9/16/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Supply Duct at Sta. 522+50	Ceiling	Delamination of Length 2ft. by 16ft.
-, -, -	• • •	· ····································	6	•

Chesapeake Tunnel - Invert Slab

Carracian Staining for 3 of	2 20	5 10	10 200	2 2/3	0	10 198	0 2	0
Corrosion Staining for 2sf	3	5	15	2/3	0	15	0	0
	4	4	16	2	0	16	0	0
Corrosion Staining	3	20	60	2	0	60	0	0
Corrosion Staining	2	10	20	2	0	20	0	0
Corrosion Staining	2	3	6	2	0	6	0	0
Corrosion Staining	20	2	40	2	0	40	0	0
	2	3	6	2	0	6	0	0
	2	2	4	2	0	4	0	0
	5	8	40	2	0	40	0	0
	2	3 7	6	2	0	6	0	0
	4 4	5	28 20	2 2	0	28 20	0	0
	0	0	80	2	0	80	0	0
	5	5	25	2	0	25	0	0
	2	12	24	2	0	24	0	0
	3	5	15	2	0	15	0	0
	2	3	6	2	0	6	0	0
	3	20	60	2	0	60	0	0
	2	20	40	2	0	40	0	0
	2	15	30	2	0	30	0	0
	1	2	2	3	0	0	2	0
	3	3	9	2	0	9	0	0
	4	18	72	2	0	72	0	0
	2	6	12	2	0	12	0	0
	2	5	10	2	0	10	0	0
	4	15	60	2	0	60	0	0
2 about the sale and distance	4 2	8 5	32	2 2	0	32	0	0
2 similar sized delams At Joint	1	20	10 20	3	0	10 0	0 20	0
ACJOINC	2	4	8	2	0	8	0	0
	1	3	3	2	0	3	0	0
	3	5	15	2	0	15	0	0
	4	12	48	2	0	48	0	0
Marked with x	4	6	24	2	0	24	0	0
Corrosion Staining	17	3	51	2	0	51	0	0
Corrosion Staining	8	2	16	2	0	16	0	0
Corrosion Staining de 1 sft	6	3	18	2/3	0	17	1	0
Corrosion Staining de 4 sft	18	6	108	2/3	0	104	4	0
Corrosion Staining	3	15	45	2	0	45	0	0
Corrosion Staining de 1 sft	20	8	160	2/3	0	159	1	0
Corrosion Staining	18	11	198	2	0	198	0	0
Corrosion Staining de 1 sft	20 3	14 20	280	2/3	0	279 56	1 4	0
Corrosion Staining d1sf p sw nw Corrosion Staining	9	4	60 36	2/3	0	36	0	0
Corrosion Staining Corrosion Staining d8sf p e	6	20	120	2/3	0	112	8	0
Corrosion Staining de 2 sft	20	7	140	2/3	0	138	2	0
Corrosion Staining de 1 sft	20	4	80	2/3	0	79	1	0
Corrosion Staining d	2	3	6	3	ō	0	6	0
Corrosion Staining d 6sf	3	20	60	2/3	0	59	1	0
Corrosion Staining	2	14	28	2	0	28	0	0
	3	4	12	2	0	12	0	0
Corrosion Staining de 1 ft	20	7	140	2/3	0	139	1	0
Corrosion Staining	20	15	300	2	0	300	0	0
Corrosion Staining d10sf p se 60sf 10sf delam	0	0	60	2/3	0	50	10	0
Corrosion Staining	2	10	20	2	0	20	0	0
	4	12	48	2	0	48	0	0
Corrosion Staining	2	3	6	2 2	0	6	0	0
Random Areas equal Total 100SF At Joint	0 1	0 20	100 20	3	0	100 0	0 20	0
Corrosion for 1 SF	3	3	9	2/3	0	8	1	0
COTTOSION FOR I SI	2	16	32	2/3	0	32	0	0
	4	10	34	2	0	9857.5	584	15
					J		504	13

Length Width Area Condition State 1 Inspector Weather Photo Location Defect Location Condition 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+25 Ceiline Spall length 1ft, by width 3ft, // Max Depth in, with 1 Exp. Trans, Bars Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+25 Ceiling WRB, JBB, TMB Rain and Wind Spall length 6ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+30 Ceiling Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesaneake - Exhaust Duct at Sta. 470+30 Ceiling Spall length 1ft, by width 3ft, // Max Depth in, with Exp. Long, Bars and 1 Exp. Trans. Bars. Spall length 1ft. by width 9ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+35 Ceiling Spall length 1ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+35 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+35 Ceiling Delamination of Length 1ft. by 2ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+40 Ceiling Spall length 2ft, by width 8ft, // Max Depth in, with Exp. Long, Bars and 3 Exp. Trans, Bars, 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+40 Ceiling Delamination of Length 1ft. by 3ft. WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+40 Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+45 Ceiling Spall length 1ft. by 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+45 Ceiling Spall length 2ft. by 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+50 Spall length 10ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and 5 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+50 Ceiling Spall length 1ft. by width 3ft. // Max Depth in. with 1 Exp. Trans. Bars. 9/17/2024 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+65 Ceiling Delamination of Length 1ft, by 1ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+75 Ceiling Delamination of Length 2ft. by 20ft. WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+80 9/17/2024 Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+80 Ceiling Delamination of Length 2ft. by 4ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+85 Ceiling Delamination of Length 2ft. by 6ft. 12 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+90 Ceiling Delamination of Length 1ft. by 4ft 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+92 Delamination of Length 1ft. by 1ft 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+95 Ceiling Delamination of Length 1ft, by 3ft, 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 470+98 Ceiling Spall length 4ft, by width 6ft, // Max Depth in, with Exp. Long, Bars and 4 Exp. Trans, Bars, 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 471+00 Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. Ceiling WRB, JBB, TMB Rain and Wind 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 471+05 Ceiling Spall length 5ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 5 Exp. Trans. Bars 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+30 Ceiling Delamination of Length 1ft. by 15ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+40 Ceiling Delamination of Length 1ft. by 6ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+4 Delamination of Length 2ft. by 8f 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+60 Ceiling Delamination of Length 1ft. by 2ft 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+60 Ceiling Delamination of Length 1ft, by 15ft WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+65 Ceiling 9/17/2024 Delamination of Length 1ft. by 2ft. 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 471+90 Delamination of Length 1ft. by 10ft 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesaneake - Exhaust Duct at Sta 473+05 Ceiling Spall length 2ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars. Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 473+20 Ceiling 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 473+35 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 473+50 Ceiling Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 473+80 Ceiling Delamination of Length 2ft, by 2ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 474+80 Ceiling Delamination of Length 1ft. by 1ft. 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 475+10 Ceiling Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. Ceiling 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 475+35 Delamination of Length 2ft. by 2ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 475+90 Ceiling Delamination of Length 1ft. by 1ft. Tunnel - Chesapeake - Exhaust Duct at Sta. 475+90 Delamination of Length 2ft. by 2ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Ceiling WRB. JBB. TMB Rain and Wind 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 476+50 Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 476+65 Ceiling Spall length 2ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 476+80 Ceiling Spall length 2ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 476+80 Delamination of Length 2ft. by 2ft. 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+10 Ceiling Spall length 1ft. by width 1ft. // Max Depth in. with Approx. % Section Los 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+20 Ceiling Delamination of Length 1ft, by 2ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+40 Ceiling Delamination of Length 2ft. by 10ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+4 Spall length 5ft. by width 3ft. // Max Depth in. with 1 Exp. Trans. Bars 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+60 Ceiling Spall length 10ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 7 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+68 Ceiling Spall length 1ft, by width 3ft, // Max Depth in, with Exp. Long, Bars and 2 Exp. Trans. Bars. Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+70 Ceiling WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+74 9/17/2024 Delamination of Length 2ft. by 6ft. 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 479+80 Ceiling Spall length 1ft, by width 5ft, // Max Depth in, with Exp. Long, Bars and 1 Exp. Trans, Bars. Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 480+30 Ceiling 9/17/2024 9/17/2024 WRB, JBB, TMB Rain and Wind Spall length 2ft. by width 22ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. Tunnel - Chesapeake - Exhaust Duct at Sta. 481+80 Ceiling 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 481+90 Spall length 1ft. by width 2ft. // Max Depth in. with 1 Exp. Trans. Bars. 9/17/2024 WRB, IBB, TMB Rain and Wind Tunnel - Chesaneake - Exhaust Duct at Sta. 482+70 Ceiling Spall length 6ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 5 Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 483+15 Ceiling Delamination of Length 1ft. by 2ft. Spall length 1ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 483+20 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 483+20 Ceiling Delamination of Length 1ft, by 1ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 484+65 Ceiling Delamination of Length 1ft, by 4ft, With cracking and corrosion Tunnel - Chesapeake - Exhaust Duct at Sta. 484+65 Ceiling 9/17/2024 WRB, JBB, TMB Rain and Wind Delamination of Length 2ft. by 4ft. WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 484+75 Spall length 1ft. by width 2ft. // Max Depth 2 in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 Spall length 1ft. by width 4ft. // Max Depth in. with 2 Exp. Trans. Bars. 9/17/2024 WRB, IBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 485+30 Ceiling WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 485+30 Ceiling Delamination of Length 1ft. by 4ft. 9/17/2024 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 486+85 Spall length 2ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 486+95 Ceiling 9/17/2024 9/17/2024 WRB. JBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 487+60 Ceiling Spall length 2ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 489+20 Ceiling Delamination of Length 1ft. by 6ft. WRB, JBB, TMB Rain and Wind Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 489+25 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 489+25 Ceiling Delamination of Length 4ft. by 4ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 490+50 Ceiling Spall length 1ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. WRB, JBB, TMB Rain and Wind 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 491+02 Ceiling Delamination of Length 2ft. by 4ft. WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 491+02 9/17/2024 Delamination of Length 1ft. by 3ft. Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 WRB, IBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 491+40 Ceiling 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 491+50 Ceiling Spall length 1ft, by width 2ft, // Max Depth in, with Exp. Long, Bars and 1 Exp. Trans. Bars. 9/17/2024 Tunnel - Chesapeake - Exhaust Duct at Sta. 491+60 Delamination of Length 1ft. by 12ft. With cracks and Efflorescence WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 492+40 Ceiling Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 492+80 Ceiling Delamination of Length 3ft. by 3ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 492+90 Spall length 1ft. by width 2ft. // Max Depth in. with 1 Exp. Trans. Bars 9/17/2024 WRB, JBB, TMB Rain and Wind Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. 9/17/2024 WRB. IBB. TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 493+25 Ceiling Delamination of Length 1ft. by 3ft. 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 493+30 Ceiling Spall length 1ft, by width 3ft, // Max Depth in, with 1 Exp. Trans, Bars, 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 493+50 Ceiling Delamination of Length 2ft. by 8ft.Spall length 3ft. by width 8ft. // Max Depth in. with 3 Exp. Trans. Bars. Tunnel - Chesapeake - Exhaust Duct at Sta. 494+10 9/17/2024 WRB, JBB, TMB Rain and Wind 9/17/2024 WRR IRR TMR Rain and Wind Tunnel - Chesaneake - Exhaust Duct at Sta 494+60 Spall length 2ft. by width 10ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. // Approx. 10 % Section Los 9/17/2024 WRB, JBB, TMB Rain and Wind Tunnel - Chesapeake - Exhaust Duct at Sta. 495+20 Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.

									_		
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 495+25	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 3	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 495+50	Ceiling	Spall length 1ft. by width 8ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 8	8	3	0	0	8 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 496+25	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 3	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 496+30	Ceiling	Spall length 1ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 6	6	3	0	0	6 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 496+60	Ceiling	Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 2	2	3	_ 0	0	2 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 497+10	Ceiling	Delamination of Length 1ft. by 8ft.		1 8	8	2	0	8	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 497+50	Ceiling	Spall length 8ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 5 Exp. Trans. Bars.		8 4	32	3	0		32 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+30	Ceiling	Delamination of Length 1ft. by 3ft.		1 3	3	2	0	3	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+50	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+52	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+65	Ceiling	Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 2	2	3	0	0 :	2 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+20	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+40	Ceiling	Delamination of Length 3ft. by 10ft.		3 10	30	2	0	30	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+40	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+65	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+10	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Exposed Conduit	1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+20	Ceiling	Delamination of Length 1ft. by 3ft.		1 3	3	2	_ 0	3	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+40	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		1 3	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+40	Ceiling	Spall length 1ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		1 6	6	3	0	0	6 0
		Tunnel - Chesapeake - Exhaust Duct at Sta. 500+40	Ceiling	Delamination of Length 2ft. by 6ft.		2 6	12	2	0		0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+50	Ceiling	Spall Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	10 sf	0 0	10	3	0	-	10 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+95	Ceiling	Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 4	4	3	0	0	4 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+98	Ceiling	Spall length 2ft. by width 8ft. // Max Depth in. with 1 Exp. Trans. Bars.		2 8	16	3	0	-	16 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+60	Ceiling	Delamination of Length 1ft. by 5ft.		1 5	5	2	0	·	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+80	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+05	Ceiling	Spall length 2ft. by width 6ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		2 6	12	3	0		12 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+15	Ceiling	Delamination of Length 3ft. by 7ft.		3 7	21	2	0		0 0
9/17/2024		Tunnel - Chesapeake - Exhaust Duct at Sta. 502+15	Ceiling	Spall length 2ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars. // Approx. 10 % Section Loss		2 5	10	3	0	•	
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+80	Ceiling	Spall length 1ft. by width 1ft.		1 1	1	2	0	1	0 0
0/47/2024	WDD IDD TAID Date and Word	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+88 Tunnel - Chesapeake - Exhaust Duct at Sta. 504+40	Ceiling	Delamination of Length 2ft. by 2ft.		1 3	4	2	0	4	0 0
9/17/2024	WRB, JBB, TMB, Rain and Wind		Ceiling	Delamination of Length 1ft. by 3ft.			-	2	0	3 12	
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+40	Ceiling Ceiling	Delamination of Length 2ft. by 6ft. Delamination of Length 1ft. by 2ft.		2 6 1 2	12 2	2	_ 0	2	0 0
9/17/2024		Tunnel - Chesapeake - Exhaust Duct at Sta. 504+45				2 12	24	2	0	24	0 0
	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+54	Ceiling	Delamination of Length 2ft. by 12ft.		1 1	1	2	0	24	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 508+70 Tunnel - Chesapeake - Exhaust Duct at Sta. 509+40	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	_	2	0	0	1 0
9/17/2024	WRB, JBB, TMB, Rain and Wind		Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Approx. % Section Loss		1 1	1	3	0	1	0 0
9/17/2024 9/17/2024	WRB, JBB, TMB, Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 510+82	Ceiling Ceiling	Delamination of Length 1ft. by 1ft. Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 1	1	2	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 510+92 Tunnel - Chesapeake - Exhaust Duct at Sta. 511+00	Ceiling	Delamination of Length 1ft. by 1ft.Spall Max Depth in. with Exp. Long. Bars and Lexp. Trans. Bars.		1 1	1	2	0	1	0 0
9/17/2024			Ceiling			1 1	2	2	0	0	2 0
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 512+20	Ceiling	Spall length 1ft. by width 2ft. // Max Depth in. with 1 Exp. Trans. Bars. Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 4	4	3	0	0 .	4 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+20	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Approx. % Section Loss		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+00	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+10	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+10	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+10	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+80	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Long. Bars and Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 515+75	Ceiling	Delamination of Length 2ft. by 2ft.		2 2	4	2	0	-	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 515+75	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	ő	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+10	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Approx. % Section Loss		1 1	1	3	o o	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+10	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 3	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+10	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+10	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+60	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 3	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+65	Ceiling	Spall length 2ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		2 3	6	3	0	0	6 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 516+90	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind JBB5326 WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+05	Ceiling Ceiling	Spall length 1ft. by width 8ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. // Approx. 10 % Section Loss Delamination of Length 1ft. by 3ft.		1 0	8	3	0	3	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+03	Ceiling	Delamination of Length 1ft. by 3ft. Delamination of Length 1ft. by 3ft.		1 3	3	2	0	-	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30	Ceiling	Delamination of Length 1ft. by 4ft.		1 4	4	2	0	4	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30	Ceiling	Delamination of Length 2ft. by 5ft.		2 5	10	2	0	10	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+40	Ceiling	Delamination of Length 1ft. by 12ft.	At Joint	1 12	12	2	0	12	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+40	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+85	Ceiling	Delamination of Length 1ft. by 1ft.	As lains	1 1	1	2	0	T 24	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesaneake - Exhaust Duct at Sta. 517+96	Ceiling	Delamination of Length 2ft. by 12ft.	At Joint	2 12 1 3	24	2	0	24	0 0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 517+98 Tunnel - Chesapeake - Exhaust Duct at Sta. 518+15	Ceiling Ceiling	Delamination of Length 1ft. by 3ft. Delamination of Length 2ft. by 1ft.		ı 3	3	2	_ 0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 518+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 518+20	Ceiling	Delamination of Length 2ft. by 2ft. Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 518+20	Ceiling	Delamination of Length 1ft. by 24ft.	At Joint	1 24	24	2	0	24	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 519+65	Ceiling	Spall length 3ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		3 1	3	3	0	0	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 519+70	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 519+70	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	0	2	υ 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 519+90	Ceiling	Spall length 2ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		2 1	2	3	0	0	2 0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 519+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 520+25	Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Long. Bars and Exp. Trans. Bars. Delamination of Length 2ft. by 2ft.		2 2	4	2	0	4	. U
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+28	Ceiling	Delamination of Length 1ft. by 3ft.		1 3	3	2	ő	3	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+30	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars. //		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+30	Ceiling	Delamination of Length 1ft. by 1ft.		1 1	1	2	0	1	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+40	Ceiling	Spall length 2ft. by width 8ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		2 8	16	3	_ 0	0 1	16 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+40	Ceiling	Spall length 1ft. by width 8ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 8	8	3	_ 0	0	8 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+50	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars.		1 1	1	3	_ 0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+50	Ceiling	Delamination of Length 1ft. by 2ft.		1 2	2	2	_ 0	2	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+55	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+65	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars.		1 3	3	3	0	U	3 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+80	Ceiling	Spall length 2ft. by width 3ft. // Max Depth in. with 3 Exp. Trans. Bars.		2 3	6	3	0	U	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+83	Ceiling	Delamination of Length 2ft, by 2ft. Spall length 1ft, by width 1ft, // May Dooth in with 3 Eye Trans. Page		2 2	4	2	0	4	J U
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 520+85	Ceiling Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 2 Exp. Trans. Bars. Spall length 1ft. by width 1ft. // Max Depth in. with 2 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 520+86	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 2 Exp. Trans. Bars. Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 1	1	3.	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+87	Ceiling	Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars.		1 1	1	3	0	0	1 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+88	Ceiling	Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		1 2	2	3	0	0	2 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+90	Ceiling	Spall length 1ft. by width 5ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 5	5	3	0	0	5 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 520+90	Ceiling	Delamination of Length 1ft. by 4ft.		1 4	4	2	0	4	0 0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 521+10	Ceiling	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1 3	3	3	<u> </u>	0	3 0

9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch	esapeake - Exhaust Duct at Sta. 521+40 Ceiling esapeake - Exhaust Duct at Sta. 521+40 Ceiling esapeake - Exhaust Duct at Sta. 521+50 Ceiling esapeake - Exhaust Duct at Sta. 521+60 Ceiling esapeake - Exhaust Duct at Sta. 521+95 Ceiling esapeake - Exhaust Duct at Sta. 522+06 Ceiling esapeake - Exhaust Duct at Sta. 522+15 Ceiling esapeake - Exhaust Duct at Sta. 522+15 Ceiling esapeake - Exhaust Duct at Sta. 522+40 Ceiling esapeake - Exhaust Duct at Sta. 529+56 Ceiling esapeake - Exhaust Duct at Sta. 529+58 Ceiling	Spall length 1ft. by width 3ft. // Max Depth 2 in. with Exp. Long. Bars and 3 Exp. Trans. Bars. Spall length 2ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and 2 Exp. Trans. Bars. Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars. Spall length 1ft. by width 4ft. // Max Depth 1 in. with 1 Exp. Trans. Bars. Spall width 6ft. // Max Depth 1 in. with 1 Exp. Trans. Bars. Spall width 6ft. // Max Depth 1 in. with 2 Exp. Trans. Bars. Spall width 4ft. // Max Depth 1 in. with 2 Exp. Trans. Bars. Spall length 1ft. by width 1ft. // Max Depth 1/2" in. Spall length 1ft. by width 1ft. // Max Depth in. with 1 Exp. Trans. Bars. Delamination of Length 1ft. by 1ft.	1 3 3 2 2 4 1 4 4 1 4 4 3 4 12 1 6 6 1 4 4 1 1 1 1 1 1	3 0 0 3 0 0 3 0 3 0 0 0 4 0 0 0 0 4 0 0 0 0
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2	WRB, JBB, TMB Rain and Wind JBB5302 Tunnel - Ch WRB, JBB, TMB Rain and Wind JBB5307 Tunnel - Ch WRB, JBB, TMB Rain and Wind JBB5307 Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch WRB, JBB, TMB Rain and Wind Tunnel - Ch	esapeake - Exhaust Duct at Sta. 474+20 celing esapeake - Exhaust Duct at Sta. 474+25 celing esapeake - Exhaust Duct at Sta. 474+40 celing esapeake - Exhaust Duct at Sta. 474+80 celing esapeake - Exhaust Duct at Sta. 475+10 celing esapeake - Exhaust Duct at Sta. 475+35 celing esapeake - Exhaust Duct at Sta. 475+36 celing esapeake - Exhaust Duct at Sta. 478+30 celing esapeake - Exhaust Duct at Sta. 481+37 celing esapeake - Exhaust Duct at Sta. 481+37 celing esapeake - Exhaust Duct at Sta. 482+30 celing esapeake - Exhaust Duct at Sta. 482+37 celing esapeake - Exhaust Duct at Sta. 482+37 celing esapeake - Exhaust Duct at Sta. 483+36 celing esapeake - Exhaust Duc	Transperse Crast 32 lines ft., and Width - IN, with Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 12 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 12 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Corrosion and Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Corrosion and Efforescence and Efforescence and Dipsp per min Transperse Crast 22 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 22 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 2 lines ft., and Width - IN, with Efforescence and 20 pipsp per min Transperse Crast 2 lines ft., and Width - IN, with Efforescence and 20 pipsp per min Transperse Crast 2 lines ft., and Width - IN, with Efforescence and 20 pipsp per min Transperse Crast 2 lines ft., and Width - IN, with Efforescence and 20 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and 20 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and 10 pipsp per min Transperse Crast 20 lines ft., and Width - IN, with Efforescence and No Active Leak Transperse Crast 20 lines ft., and Width - IN, with Efforescence and No Active Leak Transperse Crast 20 lines ft., and Width - IN, with Efforescence and No Active Leak Transperse Crast 20 lines ft., and Width - IN, with Efforescence and No A	22	2
9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind Tunnel - Ch	esapeake - Exhaust Duct at Sta. 497+10 Ceiling esapeake - Exhaust Duct at Sta. 497+85 Ceiling esapeake - Exhaust Duct at Sta. 497+95 Ceiling	Transverse Crack 22 linear ft. and Width = HL Transverse Crack 22 linear ft. and Width = HL Transverse Crack 6 linear ft. and Width = HL	22 1 22 22 1 22 6 1 6	2 0 22 0 0 2 0 6 0 0

9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 498+40	Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL with Efflorescence and Drips per min			14 2 12 2	0	14 0 12 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+40	Ceiling	Transverse Crack 10 linear ft. and Width = HL			10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 498+80	Ceiling	Transverse Crack 5 linear ft. and Width = HL			5 2	0	5 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+25	Ceiling	Longitudinal Crack 5 linear ft. and Width = HL with Efflorescence and No Active Leak		5 1	5 2	0	5 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+25	Ceiling	Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and No Active Leak		22 1	22 3	0	0 22	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+25	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak			10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+30	Ceiling	Transverse Crack 18 linear ft. and Width = HL with Corrosion and Efflorescence and 2 Drips per min			17 4	0	0 0	17
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+40	Ceiling	Longitudinal Crack 12 linear ft. and Width = N with Efflorescence and 1 Drips per min			12 4 8 2	0	0 0	12 0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 499+55	Ceiling Ceiling	Longitudinal Crack 8 linear ft. and Width = HL with Efflorescence and Drips per min Transverse Crack 10 linear ft. and Width = HL with Efflorescence and Drips per min		-	10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+55 Tunnel - Chesapeake - Exhaust Duct at Sta. 499+85	Ceiling	Transverse Crack 17 linear ft. and Width = HL			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 499+63	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak		 	22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+20	Ceiling	Transverse Crack 8 linear ft. and Width = HL with Efflorescence and 1 Drips per min			8 4	o o	0 0	8
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+30	Ceiling	Transverse Crack 17 linear ft. and Width = N with Efflorescence and 1 Drips per min			17 4	0	0 0	17
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+40	Ceiling	Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min		22 1	22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+50	Ceiling	Transverse Crack 22 linear ft. and Width = N		22 1	22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+60	Ceiling	Transverse Crack 18 linear ft. and Width = HL			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+80	Ceiling	Transverse Crack 22 linear ft. and Width = HL			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 500+95	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and Drips per min			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+05	Ceiling	Transverse Crack 18 linear ft. and Width = HL with Corrosion and Efflorescence and No Active Leak			17 3	0	0 17	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+15	Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescence and No Active Leak			5 2	0	5 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+40	Ceiling	Transverse Crack 17 linear ft. and Width = N			17 3	0	0 17	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+56	Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 11 linear ft. and Width = N			14 2 11 2	0	14 0 11 0	0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 501+70 Tunnel - Chesapeake - Exhaust Duct at Sta. 502+15	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak		== =	22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+25	Ceiling	Longitudinal Crack 5 linear ft. and Width = HL with Efflorescence and Drips per min			5 4	ŏ	0 0	5
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+30	Ceiling	Transverse Crack 22 linear ft. and Width = N with Efflorescence and 3 Drips per min			22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+35	Ceiling	Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak			4 2	0	4 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+50	Ceiling	Transverse Crack 11 linear ft. and Width = HL with Efflorescence and No Active Leak		11 1	11 2	0	11 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+50	Ceiling	Transverse Crack 7 linear ft. and Width = N with Efflorescence and 15 Drips per min		7 1	7 4	0	0 0	7
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+56	Ceiling	Transverse Crack 6 linear ft. and Width = HL			6 2	0	6 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+60	Ceiling	Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak			12 2	0	12 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+70	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+80	Ceiling	Transverse Crack 7 linear ft. and Width = HL		, -	7 2	0	7 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+85	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak			22 2	0	22 0 22 0	0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 502+92 Tunnel - Chesapeake - Exhaust Duct at Sta. 503+00	Ceiling Ceiling	Transverse Crack 22 linear ft. and Width = HL Transverse Crack 4 linear ft. and Width = HL			22 2 2	0	4 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+00	Ceiling	Transverse Crack 22 linear ft. and Width = N with Efflorescence and 1 Drips per min			22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+20	Ceiling	Transverse Crack 22 linear ft. and Width = N with Efflorescence and No Active Leak			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+20	Ceiling	Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak		== =	6 2	0	6 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+80	Ceiling	Transverse Crack 12 linear ft. and Width = HL with Efflorescence and Drips per min		12 1	12 4	0	0 0	12
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+88	Ceiling	Transverse Crack 17 linear ft. and Width = N with Efflorescence and No Active Leak	3/4 Corrosion		17 3	0	0 17	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 503+95	Ceiling	Transverse Crack 8 linear ft. and Width = HL		8 1	8 2	0	8 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+40	Ceiling	Transverse Crack 11 linear ft. and Width = HL		11 1	11 2	0	11 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+60	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak		10 1	10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+80	Ceiling	Transverse Crack 17 linear ft. and Width = N with Efflorescence and No Active Leak			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 504+80	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescenceand No Active Leak			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 507+12	Ceiling	Transverse Crack 10 linear ft. and Width = HL			10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 507+25	Ceiling	Transverse Crack 8 linear ft. and Width = HL		• .	8 2 10 2	0	8 0	0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 507+40 Tunnel - Chesapeake - Exhaust Duct at Sta. 507+50	Ceiling Ceiling	Transverse Crack 10 linear ft. and Width = HL Transverse Crack 3 linear ft. and Width = HL with Efflorescence and No Active Leak			3 2	0	3 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 507+30	Ceiling	Transverse Crack 17 linear ft. and Width = HL Transverse Crack 17 linear ft. and Width = HL			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 508+20	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak			22 2	0	22 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 508+80	Ceiling	Transverse Crack 8 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min			8 3	0	0 8	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 508+95	Ceiling	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak		10 1	10 2	0	10 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 509+00	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and 1 Drips per min		22 1	22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 509+10	Ceiling	Transverse Crack 17 linear ft. and Width = N with Efflorescence and No Active Leak			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 509+40	Ceiling	Transverse Crack 17 linear ft. and Width = HL with Corrosion and Efflorescence and 2 Drips per min			17 4	0	0 0	17
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 510+76	Ceiling	Transverse Crack 17 linear ft. and Width = NDelamination of Length 1ft. by 4ft.			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 511+40	Ceiling	Transverse Crack 17 linear ft. and Width = N with Efflorescence and No Active Leak			17 2	0	17 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 511+50	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak			22 2	0	5 0	0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 511+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 511+60	Ceiling Ceiling	Transverse Crack 5 linear ft. and Width = HL with Efflorescenceand No Active Leak Transverse Crack 6 linear ft. and Width = HL with Efflorescence and No Active Leak			6 2	0	6 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+50	Ceiling	Transverse Crack 22 linear ft. and Width = N with Efflorescence and 1 Drips per min			22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+60	Ceiling	Transverse Crack 22 linear ft. and Width = N with Efflorescence and and Drips per min			22 4	0	0 0	22
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+65	Ceiling	Transverse Crack 22 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min		22 1	22 3	0	0 22	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+80	Ceiling	Transverse Crack 3 linear ft. and Width = HL with Efflorescence and No Active Leak			3 2	0	3 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 512+85	Ceiling	Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak			11 2	0	11 0	0
9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+65	Ceiling	Transverse Crack 22 linear ft. and Width = HL		22 1	22 2	0	22 0	0
9/17/2024			_						0 0	11
0/47/2021	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95	Ceiling	Transverse Crack 12 linear ft. and Width = N with Efflorescence and 2 Drips per min		11 1	11 4	0		14
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05	Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min		11 1 14 1	14 4	0	0 0	
9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15	Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak		11 1 14 1 11 1	14 4 11 3	0	0 11	0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15	Ceiling Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak		11 1 14 1 11 1 12 1	14 4 11 3 12 2	0 0	0 0 0 11 12 0	0
9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30	Ceiling Ceiling Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = II with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min		11 1 14 1 11 1 12 1 22 1	14 4 11 3 12 2	0 0 0 0 0	0 0 11 12 0 0 0 4 0	0 0 22 0
9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15	Ceiling Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak		11 1 14 1 11 1 12 1 22 1 4 1	14 4 11 3 12 2 22 4	0 0 0 0 0 0 0	0 0 11 12 0 0 0 4 0 0 0	0 22
9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = HL with Efflorescence and and No Active Leak		11 1 14 1 11 1 12 1 22 1 4 1 22 1	14 4 11 3 12 2 22 4 4 2	0 0 0 0 0 0	0 0 11 12 0 0 0 4 0 0 0 0 0 0	0 22 0
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60	Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 11 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = H with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack linear ft. and Width = N with Efflorescence and 1 Drips per min		11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1	14 4 111 3 12 2 2 2 4 4 2 2 2 4 8 8 4 111 4	0 0 0 0 0 0	0 0 11 12 0 0 0 4 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 22 linear ft. and Width = N with Efflorescence and 2 Drips per min		11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1	14 4 4 11 3 112 2 2 4 4 2 2 2 4 8 8 4 4 2 2 2 4 4 2 2 2 4 4 2 2 4 4 2 2 4 4 2 4 4 2 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4 4	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = HL with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 22 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 73 linear ft. and Width = HL with Efflorescence and 1 Drips per min		11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1	14 4 4 11 3 12 2 4 4 2 22 4 4 2 22 4 7 4 9 7 4	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 11 12 0 0 0 0 4 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+40 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 2 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 1 Drips per min		11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1 8 1	14 4 11 3 12 2 2 4 4 2 2 2 4 4 2 2 2 4 4 11 4 4 2 2 2 4 4 7 7 7 4 8 8 2 2	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 11 12 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 514-90 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack Rinear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 0 Active Leak Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 0 Orips per min	At Joint	11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1 8 1 22 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 22 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 0 No Active Leak Transverse Crack 6 linear ft. and Width = N with Efflorescence and 0 Ortips per min Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak	At Joint	11 1 14 1 11 1 12 1 22 1 22 1 8 1 11 1 22 1 8 1 11 1 22 1 7 1 8 1 22 1 6 1	14 4 4 11 3 112 2 2 4 4 2 2 2 4 8 8 4 4 11 4 4 2 2 2 4 7 7 4 4 8 8 2 2 2 3 6 2 2 5 6 2	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 11 12 0 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = N with Corrosion and Efflorescence and No Active Leak Transverse Crack 2 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak	At Joint	11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1 8 1 22 1 6 1 11 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = NL with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = NL with Efflorescence and No Active Leak Transverse Crack 2 linear ft. and Width = NL with Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = NL with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = NL with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = NL with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = NL with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = NL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = NL with Efflorescence and 0 Drips per min	At Joint	11 1 14 1 11 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1 8 1 22 1 6 1 11 1	14 4 4 11 3 112 2 2 4 4 2 2 2 4 8 8 4 4 11 4 4 2 2 2 4 7 7 4 4 8 8 2 2 2 3 6 2 2 5 6 2	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = N with Corrosion and Efflorescence and No Active Leak Transverse Crack 2 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 5 linear ft. and Width = N with Efflorescence and No Active Leak	At Joint	11 1 14 1 11 1 12 1 12 1 22 1 4 1 22 1 8 1 11 1 22 1 7 1 8 1 22 1 7 1 8 1 22 1 7 1 8 1 11 1 11 1 11 1 11 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 2 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = HL with Corrosion and Efflorescence and Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = HL with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 1 and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL	At Joint Corrosion and Efflorescence for 3 LF	11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 11 12 0 0 0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+46 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 515+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 2 linear ft. and Width = H with Efflorescence and no Active Leak Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 72 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = H Transverse Crack 12 linear ft. and Width = H Transverse Crack 8 linear ft. and Width = H Transverse Crack 8 linear ft. and Width = H Transverse Crack 8 linear ft. and Width = H Transverse Crack 8 linear ft. and Width = H Transverse Crack 8 linear ft. and Width = H		11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+06 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 2 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 4 linear ft. and Width = N with Efflorescence and and No Active Leak Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active Leak Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL		11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+66 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 3 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 4 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 6 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and N Active Leak Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL with Corrosion and Efflorescence and No Active Leak Longitudinal Crack 4 linear ft. and Width = HL		11	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+66 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 3 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 4 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 6 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and N Active Leak Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL with Corrosion and Efflorescence and No Active Leak Longitudinal Crack 4 linear ft. and Width = HL		11	14	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11 22 7 0 0 0 0 0
9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024 9/17/2024	WRB, JBB, TMB Rain and Wind	Tunnel - Chesapeake - Exhaust Duct at Sta. 513+95 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+05 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+15 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+66 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+60 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+75 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+80 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+85 Tunnel - Chesapeake - Exhaust Duct at Sta. 514+90 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+00 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+10 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 517+50 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30 Tunnel - Chesapeake - Exhaust Duct at Sta. 521+30	Ceiling	Transverse Crack 14 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 12 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = HL with Efflorescence and No Active Leak Transverse Crack 22 linear ft. and Width = N with Corrosion and Efflorescence and 1 Drips per min Transverse Crack 3 linear ft. and Width = HL with Efflorescence and and No Active Leak Transverse Crack 4 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = N with Efflorescence and 1 Drips per min Transverse Crack 8 linear ft. and Width = HL with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = N with Efflorescence and 2 Drips per min Transverse Crack 7 linear ft. and Width = HL with Efflorescence and 1 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 5 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 6 linear ft. and Width = N with Efflorescence and 0 Drips per min Transverse Crack 12 linear ft. and Width = N with Efflorescence and N Active Leak Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL Transverse Crack 12 linear ft. and Width = HL with Corrosion and Efflorescence and No Active Leak Longitudinal Crack 4 linear ft. and Width = HL		11	14		0 0 4 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 22 0 22 8 11 22 7 0 0 0 0 0 0 0

9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5256	Tunnel - Chesapeake - Supply Duct at Sta. 469+50	Floor	Spall length 4ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		4	4	16	3	0	0	16 0	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 471+40	Floor	Spall length 2ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		2	2	4	3	0	0	4 0	
9/16/2024	WRB, JBB, TMB		JBB5264	Tunnel - Chesapeake - Supply Duct at Sta. 478+00		Spall length 4ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to East Vent	1	2	8	2	ō	0	8 0	
9/16/2024	WRB, JBB, TMB		JBB5264		Floor	Spall length 4ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to East Vent	4	2	8	3	Ů	0		
				Tunnel - Chesapeake - Supply Duct at Sta. 478+20			The state of the s	4	_	-	3	U	Ü	0 0	
9/16/2024	WRB, JBB, TMB		JBB5264	Tunnel - Chesapeake - Supply Duct at Sta. 478+40	Floor	Spall length 4ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to East Vent	4	2	8	3	U	Ü	8 0	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 478+78	Floor	Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and 1 Exp. Trans. Bars.		1	4	4	3	0	0	4 0	
9/16/2024		Rain and Wind	JBB5256	Tunnel - Chesapeake - Supply Duct at Sta. 480+21	Floor	Spall length 3ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	At Centerline	3	1	3	3	0	0	3 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5264	Tunnel - Chesapeake - Supply Duct at Sta. 480+21	Floor	Spall length 3ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to East Vent	4	2	8	3	0	0	8 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5265	Tunnel - Chesapeake - Supply Duct at Sta. 480+65	Floor	Spall length 4ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to West Vent	4	3	12	3	0	0	12 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5266	Tunnel - Chesapeake - Supply Duct at Sta. 482+90	Floor	Delamination of Length 4ft. by 2ft.	In East Vertical Portion	4	2	8	2	0	8	0 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5267	Tunnel - Chesapeake - Supply Duct at Sta. 483+50	Floor	Spall length 3ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to West Vent	3	3	9	3	0	0	9 0	
9/16/2024	WRB, JBB, TMB		JBB5268	Tunnel - Chesapeake - Supply Duct at Sta. 486+50	Floor	Spall length 2ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	Adjacent to West Vent	2	4	8	3	0	0	8 0	
9/16/2024			JBB5269		Floor		· ·	4	3	3	3	0	0	3 0	
	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 486+50		Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	1	-	-	3	-	Ü	3 0	
9/16/2024	WRB, JBB, TMB		JBB5270	Tunnel - Chesapeake - Supply Duct at Sta. 488+75	Floor	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	1	3	3	3	0	0	3 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5272	Tunnel - Chesapeake - Supply Duct at Sta. 489+15		1ft. by width 3ft. Area with Corrosion Staining and Active Leak	In East Vertical Portion	1	3	3	4	0	0	0 3	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5271	Tunnel - Chesapeake - Supply Duct at Sta. 489+30	Floor	Spall length 1ft. by width 2ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	1	2	2	3	0	0	2 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5273	Tunnel - Chesapeake - Supply Duct at Sta. 489+80	Floor	Spall length 1ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	1	3	3	3	0	0	3 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 498+37	Floor	Non-function Light	At East Vertical Portion		-		-	0	0	0 0	
9/16/2024	WRB. JBB. TMB	Rain and Wind	JBB5290	Tunnel - Chesapeake - Supply Duct at Sta. 499+756	Floor	1ft. by width 4ft. Area with Corrosion Staining and Active Leak	In West Vertical Portion	1	4	4	4	0	0	0 4	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 503+55	Floor	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	2	3	6	3	Ö	0	6 0	
9/16/2024	WRB, JBB, TMB		JBB5289	Tunnel - Chesapeake - Supply Duct at Sta. 504+60		Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	- 4	3	3	3	ő	0	3 0	
								'	3	,			0	0 0	
9/16/2024	WRB, JBB, TMB		JBB5288	Tunnel - Chesapeake - Supply Duct at Sta. 508+20	Floor	Non-function Light	At East Vertical Portion				-		Ü	0 0	
9/16/2024	WRB, JBB, TMB		JBB5287	Tunnel - Chesapeake - Supply Duct at Sta. 511+15	Floor	1ft. by width 3ft. Area with Corrosion Staining and Active Leak	In East Vertical Portion	1	3	3	4	0	0	0 3	
9/16/2024	WRB, JBB, TMB		JBB5286	Tunnel - Chesapeake - Supply Duct at Sta. 511+70	Floor	Spall length 5ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	5	4	20	3	0	0	20 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5285	Tunnel - Chesapeake - Supply Duct at Sta. 513+00	Floor	Spall length 3ft. by width 3ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	3	3	9	3	0	0	9 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 515+65	Floor	1ft. by width 3ft. Area with Corrosion Staining and Active Leak	In East Vertical Portion	1	3	3	4	0	0	0 3	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5284	Tunnel - Chesapeake - Supply Duct at Sta. 516+85	Floor	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	5	1	5	3	0	0	5 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 517+70	Floor	Spall length 1ft. by width 3ft.	In East Vertical Portion	1	3	3	3	0	0	3 0	
9/16/2024	WRB, JBB, TMB		JBB5283	Tunnel - Chesapeake - Supply Duct at Sta. 519+50	Floor	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars. // 10%< Section Loss	In East Vertical Portion	1	1	1	3	0	0	1 0	
9/16/2024	WRB, JBB, TMB		JBB5282	Tunnel - Chesapeake - Supply Duct at Sta. 519+90	Floor	Spall length 3ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In West Vertical Portion	3	4	12	3	0	0	12 0	
9/16/2024	WRB, JBB, TMB		JBB5282 JBB5279	Tunnel - Chesapeake - Supply Duct at Sta. 520+48	Floor	Spall length 1ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.		1	1	4	3	0	0	4 0	
							In Fact Vanished Banklan	· ·	4		3	-	0		
9/16/2024	WRB, JBB, TMB		JBB5281	Tunnel - Chesapeake - Supply Duct at Sta. 521+00	Floor	Spall length 5ft. by width 4ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	5	4	20	3	0	U	20 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5280	Tunnel - Chesapeake - Supply Duct at Sta. 522+00	Floor	Spall length 1ft. by width 1ft. // Max Depth in. with Exp. Long. Bars and Exp. Trans. Bars.	In East Vertical Portion	1	1	1	3	0	0	1 0	
												0	8	223 13	
Cracks															
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 469+00	Floor	Longitudinal Crack 10 linear ft. and Width = N		10	1	10	2	0	10	0 0	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 469+00	Floor	Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active LeakLongitudinal Crack 0 linear ft. and Width = N	Delamination of Length 3ft, by 3ft	2	3	9	2	0	0	0 0	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 469+00	Floor	Transverse Crack 6 linear ft. and Width = N with Efflorescence and No Active LeakLongitudinal Crack 0 linear ft. and Width = N		5	1	6	2	0	6	0 0	
							Detainination of Length Sit. by Sit.		1	4	2	0		0 0	
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 469+05	Floor	Transverse Crack 4 linear ft. and Width = N with Efflorescence and Drips per min		4	1		2	0	4		
9/16/2024	WRB, JBB, TMB			Tunnel - Chesapeake - Supply Duct at Sta. 469+10	Floor	Map Cracking Area with Corrosion and Efflorescence Totaling 25sf		5	5	25	2	0	25	0 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 469+65	Floor	Transverse Crack 30 linear ft. and Width = HL with Efflorescence and No Active Leak		30	1	30	2	0	30	0 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind	JBB5261	Tunnel - Chesapeake - Supply Duct at Sta. 469+65	Floor	Transverse Crack 10 linear ft. and Width = HL with Efflorescence and No Active Leak		10	1	10	2	0	10	0 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 471+25	Floor	Transverse Crack 4 linear ft. and Width = HL with Efflorescence and No Active Leak		4	1	4	2	0	4	0 0	
9/16/2024	WRB, JBB, TMB	Rain and Wind		Tunnel - Chesapeake - Supply Duct at Sta. 471+30	Floor	Transverse Crack 2 linear ft. and Width = HL with Efflorescence and No Active Leak		2	1	2	2	0	2	0 0	
-,,	,,,							-	-	-	-				
													75	0 0	
												0	75	0 0	
												0	75	0 0	ī
Roadway												0	75	0 0	Cracked
Roadway Tiles	No new notes											0	75	0 0	Cracked Tiles
	No new notes	Clear		Tunnel - Chesaneake - Roadway Level - Lower Wall at Sta.	SOS: Roadway up to 6-ft		9 delam tiles	0	0	0	1	0	75	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft		9 delam tiles 8 delam tiles	0	0	0	1 1	0	0	0 0	
9/19/2024 9/19/2024	TMB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1)	8 delam tiles	0	0	0	1 1 1	0 0 0	0 0	0 0 0	
9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign	0 0 0	0	0	1 1 1 1	0	0 0 0	0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=1) (Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles	0 0 0	0 0	0 0 0	1 1 1 1	0	0 0 0 0	0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB	Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=1) (Cracked Tiles Square=1) (Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80	0	0	0 0 0 0	1 1 1 1	0 0	0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60	0 0 0	0 0 0 0	0 0 0 0	1 1 1 1 1	0 0 0 0	0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15	0 0 0	0 0	0 0 0 0 0	1 1 1 1 1 1	0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60	0 0 0	0 0 0 0	0 0 0 0	1 1 1 1 1 1	0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft	(Cracked Tiles Square=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15	0 0 0	0 0 0 0 0	0 0 0 0 0	1 1 1 1 1 1 1 1	0 0 0 0	0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE	0 0 0	0 0 0 0 0	0 0 0 0 0 0	1 1 1 1 1 1 1 1 1	0 0 0 0 0	75 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB TMB TMB JBB	Clear Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=2) (Cracked Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral	0 0 0 0 0 0	0 0 0 0 0 0	0 0 0 0 0 0	1 1 1 1 1 1 1 1 1 1	0 0 0 0 0	0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB TMB TMB JBB	Clear Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Godwalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile	0 0 0 0 0 0	0 0 0 0 0 0	0 0 0 0 0 0	1 1 1 1 1 1 1 1 1 1 1	0 0 0 0 0	75 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB JBB JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0	1 1 1 1 1 1 1 1 1 1 1 1 2	0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=9)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile 6 delam tiles, 8 tiles sta492+80	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0	1 1 1 1 1 1 1 1 1 1 1 1 1 2 1 1 1 1 1 1	0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB JBB TMB TMB JBB JBB JBB JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Delaminated Tiles Bull nose=3) (Delaminated Tiles Bull nose=7)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0	1 1	0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Delaminated Tiles Square = 3) (Cracked Tiles Square=6) (Cracked Tiles	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0	1 1 1	0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB JBB JBB JBB JBB JBB JBB JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 4894-82	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadwalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Delaminated Tiles Square = 3) (Delaminated Tiles Square = 20)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1	0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB JBB JBB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 20) (Cracked Tiles Square=6)(Delaminated Tiles Square = 20) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1	0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB JBB TMB TMB JBB TMB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20	SOS: Roadway up to 6-ft SOS: Sodewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square=3) (Cracked Tiles Square=6)(Delaminated Tiles Square=20) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose=4) (Cracked Tiles Bull nose=3)(Delaminated Tiles Bull nose=3)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB TMB TMB TMB TMB TMB TMB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 493+35 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 493+35 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadwalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose= 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Delaminated Tiles Bull nose=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Delaminated Tiles Bull nose=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80	SOS: Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Sull nose = 4) (Cracked Tiles Bull nose=1) (Delaminated Tiles Sull nose = 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Square = 2) (Cracked Tiles Square=9)(Delaminated Tiles Square = 2)(Delaminated Tiles Square =	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80	SOS: Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose= 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Delaminated Tiles Bull nose=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Delaminated Tiles Bull nose=1)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80	SOS: Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Sull nose = 4) (Cracked Tiles Bull nose=1) (Delaminated Tiles Sull nose = 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Square = 2) (Cracked Tiles Square=9)(Delaminated Tiles Square = 2)(Delaminated Tiles Square =	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500+60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500+60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500+60	SOS: Roadway up to 6-ft SOS: Roadwalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk t	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Sull nose = 4) (Cracked Tiles Bull nose=1) (Delaminated Tiles Sull nose = 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 493-35 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80	SOS: Roadway up to 6-ft SOS: Roadwalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk t	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Bull nose=4) (Delaminated Tiles Square = 20) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=3) (Delaminated Tiles Square = 3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Delaminated Tiles Square = 9)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 493+35 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511+75	SOS: Roadway up to 6-ft SOS: Godwalk to Roadway SS-Sidewalk to Roadway up to 6-ft SOS: Roadwa	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=0) (Cracked Tiles Square=0) (Cracked Tiles Square=0) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=9)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80	SOS: Roadway up to 6-ft SOS: Roadwalk to Roadway SS-Sidewalk to Roadway SS-Sidewalk to Roadway up to 6-ft SOS: Sidewalk to Roadway SS-Sidewalk to Roadway up to 6-ft SS-Sidewalk	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 4) (Cracked Tiles Square=9) (Delaminated Tiles Square = 9) (Delaminated Tiles Square = 9) (Delaminated Tiles Square = 9) (Stained Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-425 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75	SOS: Roadway up to 6-ft SOS: Roadway SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=5)(Delaminated Tiles Square = 3) (Cracked Tiles Square=5)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1) (Cracked Tiles Bull nose=3)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 4) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square=4)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam tile of tiles from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining Cerrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 504+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 20) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Square=9)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square=5) (Stained Tiles Square=2)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1) (Cracked Tiles Bull nose=1) (Cracked Tiles Bull nose=1) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=5) (Delaminated Tiles Square=4) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=18) (Cracked Tiles Square=18) (Cracked Tiles Square=18) (Cracked Tiles Square=18)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam tile of tiles from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining Cerrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Sull nose=4)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=3)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square=4) (Cracked Tiles Square=5)(Delaminated Tiles Square=5) (Delaminated Tiles Square=5) (Cracked Tiles Square=5)(Delaminated Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam tile of tiles from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining Cerrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 2 2 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 504+15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501+55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)-(Delaminated Tiles Bull nose = 3) (Cracked Tiles Square=9)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)-(Delaminated Tiles Bull nose = 3) (Cracked Tiles Square=9)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam tile of tiles from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tile, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion staining Cerrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 2 2 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB JBB JBB TMB TMB TMB JBB JBB JBB JBB JBB TMB JBB TMB TMB TMB TMB TMB TMB TMB TMB TMB T	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 504+15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501+55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)-(Delaminated Tiles Bull nose = 3) (Cracked Tiles Square=9)-(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 1 1 1 1 1 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 499-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-60 Tunn	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=6)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 1 1 1 1 1 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 484-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 503-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 504-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 501-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Delaminated Tiles Square=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=3) (Cracked Tiles Square=9) (Delaminated Tiles Square=4) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=6)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 1 1 1 1 1 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505-30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513-70 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 511-75 Tunn	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Cracked Tiles Square=6)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 1 1 1 1 1 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500+60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 505+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunn	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 1) (Cracked Tiles Square=9) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Half Bull-nose = 6) (Delaminated Tiles Half Bull-nose = 4)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490+82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502+15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502+15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+70 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunn	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Delaminated Tiles Square=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=3) (Cracked Tiles Square=9) (Delaminated Tiles Square=4) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=6)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 2 2 1 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513-96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50	SOS: Roadway up to 6-ft	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6) (Delaminated Tiles Square=3) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Delaminated Tiles Square=4) (Cracked Tiles Square=9) (Cracked Tiles Square=5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 1sf Efflorescence 1sf Efflorescence	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 2 2 1 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunn	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 4) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Half Bull-nose = 6) (Delaminated Tiles Half Bull-nose = 5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 506+60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 509+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+76 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+76 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunn	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Ceiling	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3)-(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Square=6)(Delaminated Tiles Square = 20) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=3)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Square=3) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=3) (Cracked Tiles Square=5) (Delaminated Tiles Half Bull-nose = 6) (Delaminated Tiles Half Bull-nose = 5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 2 2 1 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunn	SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway up to 6-ft SOS: Roadway up to 6-ft SS-Sidewalk to Roadway SS-Sidewalk to Ceiling	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 4) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Half Bull-nose = 6) (Delaminated Tiles Half Bull-nose = 5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear	JBB5362	Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489+25 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496+80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 506+60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 509+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+76 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513+76 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514+50 Tunn	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3)-(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Square=6)(Delaminated Tiles Square = 20) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=3)(Delaminated Tiles Bull nose = 3) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2)(Delaminated Tiles Square=3) (Cracked Tiles Square=9)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=3) (Cracked Tiles Square=5) (Delaminated Tiles Half Bull-nose = 6) (Delaminated Tiles Half Bull-nose = 5)	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear	JBB5362	Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 485+75 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-482 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 489-482 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502-15 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513-96 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=3) (Cracked Tiles Square=6) (Delaminated Tiles Square=20) (Cracked Tiles Bull nose=1) (Delaminated Tiles Square=2) (Cracked Tiles Bull nose=3) (Delaminated Tiles Square=2) (Cracked Tiles Square=3) (Cracked Tiles Square=5) (C	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB	Clear	JBB5362	Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. + Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 490-82 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 494-20 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 496-80 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 500-60 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 504-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 513-70 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-55 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 514-50 Tunn	SOS: Roadway up to 6-ft SOS: R	(Cracked Tiles Square=1) (Cracked Tiles Square=2) (Cracked Tiles Square=9) (Cracked Tiles Square=9) (Cracked Tiles Square=6)(Delaminated Tiles Square = 3)(Delaminated Tiles Bull nose = 7) (Cracked Tiles Bull nose=1)(Delaminated Tiles Bull nose = 4) (Cracked Tiles Bull nose=1)(Delaminated Tiles Square = 2) (Cracked Tiles Bull nose=1)(Delaminated Tiles Sull nose = 1) (Cracked Tiles Square=9)(Delaminated Tiles Square = 2) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5)(Delaminated Tiles Square = 4) (Cracked Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=5) (Cracked Tiles Square=6) (Delaminated Tiles Square=6) (Delaminated Tiles	8 delam tiles delam, 3690' sign delam 10 tiles 2 delam tiles sta 514+80 2 delam tiles sta 506+60 10 delam sta 505+15 delam two flues from FE 4 delam tile-5290' from poral 1 delam tile 6 delam tiles, 8 tiles sta492+80 crack with eff @ 4010' sign delam at flue - 9 tiles 4490' sign Efflorescence @ 2170' sign Corrosion Staining Corrosion Staining 5 delam tiles at rail connection sta 505+30 Efflorescence 4 tiles 2 Corrosion stained tiles sta 513+96 15f Efflorescence 1sf Efflorescence Efflorescence 1sf Corner of 1 tile broken	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	1 1 1 2 3 1 1 1 1 2 2 2 3 3 2 2 2 1 1 1 1		0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	

9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 486+70	SS-Sidewalk to Ceiling	(Delaminated Tiles Square = 45)		0	0	0	1	0	0 /	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 487+50	SS-Sidewalk to Ceiling		1sf Efflorescence	0	0	9	2	0	9 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 487+80	SS-Sidewalk to Ceiling		Efflorescence 40 tiles	0	0	40	2	0	40 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 489+50	SS-Sidewalk to Ceiling	(Delaminated Tiles Square = 20)		0	0	0	1	a 0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 490+00	SS-Sidewalk to Ceiling		Efflorescence 14 tiles	0	0	14	2	0	14 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 493+15	SS-Sidewalk to Ceiling		Efflorescence 12 tiles	0	0	12	2	0	12 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 493+40	SS-Sidewalk to Ceiling		Efflorescence 45 tiles	0	0	45	2	0	45 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 493+50		(Cracked Tiles Square=9)	Emorescence as thes	0	0	0	1	0	0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 494+20		(Cracked Tiles Square=3)		0	0	0	1	0	0	0 0	
9/19/2024	TMB	Clear						0	0	0	1	0	0 (0 0	
				Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 494+50		(Cracked Tiles Square=4)		0	0	0		0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 494+70	-	(Cracked Tiles Square=6)(Delaminated Tiles Square = 12)		0	-	-	1	_	0 (
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 495+10	-	(Cracked Tiles Square=4)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear	JBB5366	Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 496+00	-	Missing Station Marker		0	0	0	4	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 496+00	-	(Cracked Tiles Square=6)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 496+28	-	(Stained Tiles Square=4)	Efflorescence	2	1	2	2	0	2 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 496+30	SS-Sidewalk to Ceiling	•	3sf Efflorescence	0	0	81	2	0	81 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 496+50	SS-Sidewalk to Ceiling		Efflorescence 30 tiles	0	0	30	2	0	30 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 497+10	SS-Sidewalk to Ceiling	(Delaminated Tiles Square = 30)		0	0	0	1	0	0 /	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 497+90	SS-Sidewalk to Ceiling	(Cracked Tiles Square=7)(Cracked Tiles Half Bull-nose=2)		0	0	0	1	0	0 /	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 498+10	SS-Sidewalk to Ceiling	(Delaminated Tiles Half Bull-nose = 4)		0	0	0	1	0	0 /	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 499+30		(Delaminated Tiles Half Bull-nose = 4)		0	0	0	1	0	0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 500+00	-	(Cracked Tiles Square=3)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 500+60	SS-Sidewalk to Ceiling		Efflorescence 30 tiles	0	0	30	2	0	30 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 500+70	-	(Delaminated Tiles Square = 24)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 501+15		(Delaminated Tiles Square = 33)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 501+15	SS-Sidewalk to Ceiling	(Detailillated files signale - 33)	Efflorescence 75 tiles	0	0	75	2	0	75 (0 0	
				Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 503+50 Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 503+50	-			0			2	0	10 (0 0	
9/19/2024	TMB	Clear		•	SS-Sidewalk to Ceiling		Efflorescence 10 tiles	0	0	10	2				
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 504+20		(Cracked Tiles Square=2)	FED	0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 504+85	SS-Sidewalk to Ceiling		Efflorescence 12 tiles	U	0	12	2	0	12 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 505+15		(Delaminated Tiles Half Bull-nose = 4)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 505+20		(Cracked Tiles Square=3)		0	0	0	1	0	0 (0 د	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 507+80		(Delaminated Tiles Square = 20)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 509+30	SS-Sidewalk to Ceiling	(Cracked Tiles Square=14)(Delaminated Tiles Square = 26)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 509+50	SS-Sidewalk to Ceiling	(Cracked Tiles Square=7)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 509+80		(Cracked Tiles Square=2)		0	0	0	1	0	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 510+15	SS-Sidewalk to Ceiling	(Cracked Tiles Square=10)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 510+30	SS-Sidewalk to Ceiling	<u>.</u>	Efflorescence 24 tiles	0	0	24	2	0	24 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 511+30	-	(Cracked Tiles Square=4)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 513+30	-	(Cracked Tiles TBR Square=2)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 513+50		(Cracked Tiles Square=14)(Cracked Tiles Half Bull-nose=3)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 513+70	SS-Sidewalk to Ceiling	(claused lies square-14) (cracked lies han builtose-3)	Efflorescence 10 tiles	0	0	10	2	0	10 (, ,	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 514+00	SS-Sidewalk to Ceiling		Efflorescence 20 tiles	0	0	20	2	0	20 (0 0	
					-	Constant Title Course 42)	Efflorescence 20 tiles	0			2	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 514+20		(Cracked Tiles Square=13)		0	0	0	1	•			
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 516+20		(Cracked Tiles Square=13)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 516+50		(Delaminated Tiles Square = 10)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 517+00	-	(Delaminated Tiles Half Bull-nose = 4)		0	0	0	1	0	0 (J 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 518+30	SS-Sidewalk to Ceiling	(Cracked Tiles Square=5)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 518+80	SS-Sidewalk to Ceiling	(Cracked Tiles Square=8)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 519+30	SS-Sidewalk to Ceiling	(Cracked Tiles Square=8)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 520+90	SS-Sidewalk to Ceiling	(Delaminated Tiles Square = 4)		0	0	0	1	0	0 (0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Main Wall at Sta. 521+20	SS-Sidewalk to Ceiling	(Cracked Tiles Square=10)		0	0	0	1	0	0 /	0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 469+90	SOS: Above 6'	(Delaminated Tiles Square = 9)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 471+75	SOS: Above 6'	(Delaminated Tiles Square = 8)(Delaminated Tiles Half Bull-nose = 8)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 472+50	SOS: Above 6'	(Delaminated Tiles Square = 8)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 475+10	SOS: Above 6'		Efflorescence 1sf	0	0	1	2	0	1	0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 475+50	SOS: Above 6'	(Delaminated Tiles Square = 3)(Delaminated Tiles Half Bull-nose = 6)	Emoreoscine 131	0	0	0	1	0	· `	, o	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 476+30	SOS: Above 6'	- Colaminated Thes Square - 57 (Scianinated Thes Train Bull Hose - 67	Efflorescence 6sf	0	0	6	2	0	6	0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 476+60	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 30)	top row	0	0	0	1	0	0 (0 0	
	JBB	Clear			SOS: Above 6'	(Defailiffaced Tiles Trail Bull-Tilose – 30)		0	0	0		0	0 (0 0	
9/19/2024				Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 476+60		Table 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1	476+60 to 486+00 approx 15% top row blue tiles delam	0	-	-		•			
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 477+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 1)	top row delam from last defect to here. approx 50'	0	0	0	1	0		0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 480+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 20)		0	0	0	1	0	0 () 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 483+05	SOS: Above 6'	-	Efflorescence 12sf	0	0	12	2	0	12 (0 ر	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 483+60	SOS: Above 6'		Efflorescence 40 tiles	0	0	40	2	0		0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 484+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 1)	50LF delam top row to either side	0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 485+70	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 20)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear	JBB5364	Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 489+40	SOS: Above 6'	-	Efflorescence 60 tiles	0	0	60	2	0	60 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 491+70	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 8)		0	0	0	1	0	0 /	0 0	
9/19/2024	JBB	Clear	JBB5372	Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 501+30	SOS: Above 6'		Efflorescence 75 tiles	0	0	75	2	0	75 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 501+50	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 2)		0	0	0	1	0	0	0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+10	SOS: Above 6'		2'h 8'L delam Corrosion photo e	2	8	16	3	0	0 1	16 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+15	SOS: Above 6'		Corrosion stain 50 tiles	0	0	50	3	ő		50 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+20	SOS: Above 6'		Efflorescence 3tiles floor to bottom of blue tiles	n	0	93	2		93 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+20 Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+30	SOS: Above 6'		Efflorescence 10 tiles	0	0	93 10	2	0		0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 502+50 Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 507+20	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 20)	Error escende 10 tiles	0	0	0	1	0	0 (0 0	
								0		-	1	•			
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 512+20	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 10)	FIRE CONTRACTOR OF THE CONTRAC	0	0	0	1	0		0 0	
9/19/2024	JBB	Clear	JBB5376	Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 514+86	SOS: Above 6'	(Delevelanted Tiles Half Dell account)	Efflorescence 239 tiles; Corrosion staining 4 tiles	U	0	243	2	0	239 4	. 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 515+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 1)	20LF either side of sta. top row only	0	0	0	1	0	0 (0 0	
9/19/2024	JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 515+20	SOS: Above 6'		Efflorescence 243 tiles	0	0	243	2	0	243 (
		Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 516+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 15)		0	0	0	1	0	0 (0 0	
9/19/2024	JBB			Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 517+80	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 20)		0	0	0	1	0		0 0	
9/19/2024 9/19/2024	JBB JBB	Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 519+00	SOS: Above 6'	(Delaminated Tiles Half Bull-nose = 10)		0	0	0	1	0	0 (0 0	
		Clear					Corrosion staining 3 tiles wide floor to ceiling	0				<u>-</u>		P	
9/19/2024	JBB			Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00	SOS: Above 6'			U	0	93	3	0	0 9	93 0	
9/19/2024 9/19/2024	JBB JBB	Clear			SOS: Above 6'			U	0	93	3	0	0 §	93 0	
9/19/2024 9/19/2024	JBB JBB	Clear			SOS: Above 6'			Ü	0	93	3 Tiles	0		93 0 33 0	. 0
9/19/2024 9/19/2024	JBB JBB	Clear			SOS: Above 6'			Ü	0	93					0
9/19/2024 9/19/2024 9/19/2024	JBB JBB	Clear			SOS: Above 6'			0	0	93					0
9/19/2024 9/19/2024 9/19/2024 /all Non-Tile & Niches	JBB JBB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00		-		0	0	93					0
9/19/2024 9/19/2024 9/19/2024 Vall Non-Tile & Niches 9/19/2024	JBB JBB JBB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27	SOS: Roadway up to 6-ft		Efflorescence next to niche	0	0	93 1 1			1315 33		0
9/19/2024 9/19/2024 9/19/2024 Vall Non-Tile & Niches 9/19/2024 9/19/2024	JBB JBB JBB TMB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling		Efflorescence next to niche 1 linear ft of Niche Concrete Cracked	0 1	0	1 1 1			1315 33	33 0	0
9/19/2024 9/19/2024 9/19/2024 Vall Non-Tile & Niches 9/19/2024 9/19/2024 9/19/2024	JBB JBB JBB TMB TMB TMB	Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling		Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking	0 1 0 2	0 1 0	93 1 1 1		0 0 0	1315 33	33 0	0
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	JBB JBB JBB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 474+44	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling	- - - -	Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking 3 linear ft of Niche Concrete Cracked	0 1 0 3 3	0	93 1 1 1 3		0 0 0 0 0	1315 33	33 0	0
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	JBB JBB JBB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 474+44 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484+64	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling	- - - - -	Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking 3 linear ft of Niche Concrete Cracked 3 linear ft of Niche Concrete Cracked	0 1 0 3 3	0 1 0 1 1	93 1 1 3 3		0 0 0 0 0	1315 33	0 0 0 0 0 0 0 0 0 0	0
9/19/2024 9/19/2024 9/19/2024 Vall Non-Tile & Niches 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	JBB JBB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear Clear	_	Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 474+44 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484+64	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling	- - - - -	Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking 3 linear ft of Niche Concrete Cracked 5 linear ft of Niche Concrete Cracked Evidence of past Efflorescence	0 1 0 3 3	0 1 0 1 1	1 1 1 3 3		0 0 0 0 0 0 0 0 0	1315 33 1 (1 (1 (3 (3 (3 (1 (0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	JBB JBB JBB TMB TMB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 474+44 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484+64 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484-64 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484-64 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484-64 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484-64	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling		Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking 3 linear ft of Niche Concrete Cracked 3 linear ft of Niche Concrete Cracked Evidence of past Efflorescence Evidence of past Efflorescence	0 1 0 3 3 0	0 1 0 1 1 0 0	1 1 1 3 3 1		0 0 0 0 0	1315 33 1 (1 (1 (3 (3 (3 (1 (0 0 0 0 0 0 0 0 0 0 0 0 0 0	0
9/19/2024 9/19/2024 9/19/2024 /all Non-Tile & Niches 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	JBB JBB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Upper Wall at Sta. 523+00 Tunnel - Chesapeake - Roadway Level - Fire Extiguisher at Sta. 499+27 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 469+99 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 472+95 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 474+44 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 484+64	SOS: Roadway up to 6-ft SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling SS-Sidewalk to Ceiling		Efflorescence next to niche 1 linear ft of Niche Concrete Cracked 1 sf of Cracking 3 linear ft of Niche Concrete Cracked 5 linear ft of Niche Concrete Cracked Evidence of past Efflorescence	0 1 0 3 3	0 1 0 1 1	93 1 1 1 3 3 1 1		0 0 0 0 0 0 0 0 0	1315 33 1 (1 (1 (3 (3 (3 (1 (0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0

9/19/2024	TMB	Clear		Tunnel Chasenselia Beadurey level Fire Futinguisher at Ste 402:42	SS-Sidewalk to Ceiling		14 of Efficience Courseign n140	0	0 14	2	0 14	0 0	
9/19/2024	TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 493+42 Tunnel - Chesapeake - Roadway Level - Fire Extinguisher at Sta. 496+34	SS-Sidewalk to Ceiling		14 sf Efflorescence Corrosion p140 10 sf		0 10	2	0 14	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 471+54	SOS: Roadway up to 6-1		Evidence of past Efflorescence	-	0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 473+00	SOS: Roadway up to 6-1		Evidence of past Efflorescence		0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 475+93	SOS: Roadway up to 6-1		Evidence of past Efflorescence, Corrosion on valve stem	0	0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 477+39	SOS: Roadway up to 6-1		Evidence of past Efflorescence	0	0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 478+85	SOS: Roadway up to 6-f		Evidence of past Efflorescence	0	0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 480+31	SOS: Roadway up to 6-1		Evidence of past Efflorescence	0	0 2	2	0 2	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 483+24	SOS: Roadway up to 6-1		Corrosion Staining and Cracks		0 4	3	0 0	4 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 496+40	SOS: Roadway up to 6-f		Corrosion Staining at TC box Efflorescence and cracks at hv	-	0	2/3	0 6	1 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 499+32	SOS: Roadway up to 6-1		Corrosion stains under box next to FE		0 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 502+25	SOS: Roadway up to 6-1		Corrosion Staing and Efflorescence	-	0	2/3	0 2	3 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 508+10	SOS: Roadway up to 6-1		Corrosion on valve stem		0	2	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Hose Valve at Sta. 512+23	SOS: Roadway up to 6-1		Corrosion on valve stem	U	U	2	0 0	10 0	—
											0 30	10 0	
Barrier 0 (47) (200 4											2	2 2	
9/17/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 473+20 Tunnel - Chesapeake - Roadway Level - Curb at Sta. 473+30	SS: Roadway SS: Roadway	Gutter Curb Armor Fair 3 linear ft. of concrete cracked Spall length 3ft. by width 1ft.		3	1 3	3	0 0	3 0	
9/19/2024	TMB	Clear	TMB8608		SOS: Roadway	spall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 475+82	SOS: Roadway	ipall length 3ft. by width 1ft.		3	1 3	3	0 0	3 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 475+96	SS: Roadway	ipall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 475+97	SOS: Roadway	ipall length 1ft. by width 1ft.		1	1 1	2	0 1	0 0	
9/17/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 475+98	SS: Roadway	Spall length 2ft. by width 1ft.	With Exposed Rebar	2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 480+34	SS: Roadway	pall length 7ft. by width 1ft.		7	1 7	3	0 0	7 0	
9/19/2024	TMB	Clear	TMB8616	· · · · · · · · · · · · · · · · · · ·	SOS: Roadway	Spall length 4ft. by width 1ft.		4	1 4	3	0 0	4 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 486+48	SS: Roadway	Spall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 487+70	SOS: Roadway	ipall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 488+00	SOS: Roadway SOS: Roadway	ipall length 2ft. by width 1ft. ipall length 8ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024 9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 488+85 Tunnel - Chesapeake - Roadway Level - Curb at Sta. 490+74	SOS: Roadway SOS: Roadway	spall length 8ft. by width 1ft. Spall length 1ft. by width 1ft.		o 1	1 8	2	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 493+83	SOS: Roadway	ipall length 1ft. by width 1ft.		1	1 1	2	0 1	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 494+45	SOS: Roadway	ipall length 1ft. by width 1ft.		1	1 1	2	0 1	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 494+55	SOS: Roadway	ipall length 1ft. by width 1ft.		1	1 1	2	0 1	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 496+22	SS: Roadway	pall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/17/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 496+30	SS: Roadway	Spall length 2ft. by width 1ft.	At low point drain	2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 498+50	SS: Roadway	pall length 2ft. by width 1ft.		2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 503+00	SOS: Roadway	Spall length 7ft. by width 1ft.		7	1 7	3	0 0	7 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 504+54	SS: Roadway	pall length 3ft. by width 1ft.		3	1 3	3	0 0	3 0	
9/17/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 504+60	SS: Roadway SS: Roadway	Spall length 2ft. by width 1ft.	With Exposed Rebar	2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 505+07 Tunnel - Chesapeake - Roadway Level - Curb at Sta. 505+47	SS: Roadway	ipall length 4ft. by width 1ft. ipall length 2ft. by width 1ft.		2	1 4	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 505147	SOS: Roadway	Spall length 5ft. by width 1ft.		5	1 5	3	0 0	5 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 511+85	SOS: Roadway	ipall length 3ft. by width 1ft.		3	1 3	3	0 0	3 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 511+93	SS: Roadway	ipall length 5ft. by width 1ft.		5	1 5	3	0 0	5 0	
9/17/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 511+97	SS: Roadway	Spall length 2ft. by width 1ft.	With Exposed Rebar	2	1 2	3	0 0	2 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 512+42	SOS: Roadway	Spall length 1ft. by width 1ft.		1	1 1	2	0 1	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 514+00	SOS: Roadway	pall length 4ft. by width 1ft.		4	1 4	3	0 0	4 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 519+50	SOS: Roadway	Edging Curb Armor with Section Loss		0	0 0	-	0 0	0 0	
9/19/2024	TMB	Clear	TMB8664		SOS: Roadway	Steel Curb is misaligned up to 1-1/2"		1	1 1	3	0 0	1 0	
9/19/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Curb at Sta. 523+36	SS: Roadway	Stairs with Cracking and Corrosion staining		5 10	1 5	3	0 0	5 0 10 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at at Sta. 511+30 Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SS: Roadway SOS: Roadway	Spall length 10ft. by width 1ft.	Efflorescence above flue near 2170' sign		1 10 0 0	2	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway		Corrosion staining in crackm@ flue by 2170' sign		0 0	3	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. +	SOS: Roadway		Corrosion and Efflorescence one flue from 2170' sign	2	0 0	3	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 482+25	SOS: Roadway		Cracked grout at flue	2	0 0	2	0 0	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Lower Wall at Sta. 502+25	SS: Roadway		Loss grout in flue	3	0 0	2	0 0	0 0	
											0 6	99 0	
Asphalt Wearing Surface													
9/19/2024	ТМВ	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 468+90	SOS: Roadway	Minor Pot Hole		2	2 4	2	0 4	0 0	_
9/19/2024	ТМВ	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 469+19	SOS: Roadway	Area of Scaling		45	3 135	2	0 135	0 0	
9/19/2024	TMB	Clear	TMB8601		SOS: Roadway	Asphalt Patch		3	2 6	2	0 6	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 470+67	SS: Roadway	Asphalt Patch		4	2 8	2	0 8	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 472+62	SS: Roadway	Cracking Minor Post Hole		3	3 9	2	0 9	0 0	
9/19/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 475+14 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 475+29	SS: Roadway	Minor Pot Hole Minor Pot Hole		3	2 6 2	2 2	0 6	0 0	
9/19/2024	TMB	Clear	TMB8613		SS: Roadway SOS: Roadway	Willed Ashpalt		50	8 400	2	0 400	0 0	
9/19/2024	TMB	Clear	0023	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 477772	SS: Roadway	Asphalt Patch		8	4 32	2	0 32	0 0	
9/19/2024	ТМВ	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 479+42	SS: Roadway	Asphalt Patch		6	3 18	2	0 18	0 0	
9/19/2024	тмв	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 480+10	SS: Roadway	Area of Scaling		4	2 8	2	0 8	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 485+13	SS: Roadway	Asphalt Patch		4	3 12	2	0 12	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 485+50	SS: Roadway	Asphalt Patch		4	3 12	2	0 12	0 0	
9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 489+96	SS: Roadway	Asphalt Patch		2	2 4 4 20	2	0 4	0 0	
9/19/2024				Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 490+16	SOS: Roadway	Asphalt Patch		5	2 4	2	0 20	0 0	
9/19/2024 9/19/2024	TMB TMB	Clear Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 494+66 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 496+04	SS: Roadway SOS: Roadway	Asphalt Patch Asphalt Patch		6	4 24	2	0 4	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 496+59	SOS: Roadway	Asphalt Patch		4	4 16	2	0 16	0 0	
9/19/2024	тмв	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 496+88	SOS: Roadway	Asphalt Patch		6	3 18	2	0 18	0 0	
9/19/2024	ТМВ	Clear	TMB8635		SOS: Roadway	Minor Pot Hole		2	2 4	2	0 4	0 0	
9/19/2024	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 499+79	SOS: Roadway	Asphalt Patch		9	3 27	2	0 27	0 0	
9/19/2024	TMB	Clear	TMB8641	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 499+88	SOS: Roadway	Minor Pot Hole		3	3 9	2	0 9	0 0	
	TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 502+07	SOS: Roadway	Minor Pot Hole		4	4 16	2	0 16	0 0	
9/19/2024				Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 505+64	SS: Roadway	Minor Pot Hole		3	3 9		<u> </u>		
9/19/2024 9/19/2024	тмв	Clear			COC. F. 1			_		2	0 9	0 0	
9/19/2024 9/19/2024 9/19/2024	TMB TMB	Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63	SOS: Roadway	Asphalt Patch		4	3 12	2	0 9 0 12	0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024	ТМВ ТМВ ТМВ	Clear Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35	SOS: Roadway	Asphalt Patch Minor Pot Hole		4 3	3 9	2 2 2	0 9 0 12 0 9	0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB	Clear Clear Clear		Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94	SOS: Roadway SOS: Roadway	Asphalt Patch Minor Pot Hole Asphalt Patch		4 3 4 2	3 12 3 9 3 12 2 4	2 2 2 2 2	0 9 0 12 0 9 0 12 0 4	0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB TMB TMB	Clear Clear	TMB8660	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 514+91	SOS: Roadway	Asphalt Patch Minor Pot Hole		4 3 4 2 3	3 9	=	0 9 0 12 0 9 0 12 0 4 0 9	0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB TMB TMB TMB	Clear Clear Clear Clear	TMB8660	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 514+91	SOS: Roadway SOS: Roadway SOS: Roadway	Asphalt Patch Minor Pot Hole Asphalt Patch Asphalt Patch		4 3 4 2 3	3 9 3 12 2 4	=	0 4	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear	ТМВ8660	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 514+91 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 515+30	SOS: Roadway SOS: Roadway SOS: Roadway SOS: Roadway	Asphalt Patch Minor Pot Hole Asphalt Patch Asphalt Patch Minor Pot Hole		4 3 4 2 3 2 5	3 9 3 12 2 4	=	0 4	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB TMB TMB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear	ТМВ8660	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 514+91 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 515+30 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 517+37	SOS: Roadway SOS: Roadway SOS: Roadway SOS: Roadway SS: Roadway	Asphalt Patch Minor Pot Hole Asphalt Patch Minor Pot Hole Minor Pot Hole Minor Pot Hole		4 3 4 2 3 2 5 3	3 9 3 12 2 4 3 9 2 4 3 15 3 9	2 2 2 2 2	0 4 0 9 0 4 0 15 0 9	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	
9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024 9/19/2024	TMB TMB TMB TMB TMB TMB TMB TMB	Clear Clear Clear Clear Clear Clear Clear	TMB8660	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 511+63 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 512+35 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 513+94 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 514+91 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 515+30 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 517+37 Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 517+44	SOS: Roadway SOS: Roadway SOS: Roadway SOS: Roadway SS: Roadway SOS: Roadway	Asphalt Patch Minor Pot Hole Asphalt Patch Asphalt Patch Minor Pot Hole Minor Pot Hole Asphalt Patch Minor Pot Hole		4 3 4 2 3 2 5 3 4	3 9 3 12 2 4 3 9 2 4	2 2 2 2 2 2	0 4 0 9 0 4 0 15	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	

9/19/2024 TMB	Clear	Tunnel - Chesapeake - Roadway Level - Asphalt Wearing at Sta. 521+55	SS: Roadway	Minor Pot Hole		3	3	9	2	0	908	0	0
Steel Pedestrian Railing 9/19/2024 TMB	Clear	Tunnel - Chesapeake - Roadway Level - Railing at Sta.	SS-Sidewalk to Roadway	No notes other than paint system failure		0	0	0	1	0	0	0	0
Lighting Fixtures 9/26/2018 21:32 JEG	Clear Warm Ligh 1 - 1	Tunnel - Chesapeake - Roadway Level - Tunnel Lighting System at Sta. 46	9+8 Ceiling	Lights out were identified and addressed	half of light	0	0	0		1 0	0	0	0

SNTI Element	Component	Item Name, Description or Location	Condition / Operation	Inspection Findings	Detail	Thermal Reading ∘F	Condition State (1-4)	Photo	Maintenance or Repair Recommendation	Repair Priority	Date Inspected MM/DD/YY
10300 Drainage and Pumping System	3 Island Portal Pump Room	Main Sump Pump BSD-1 - Pump	Condition	Good			1				9/18/2024
			Operation	Not Operated			1				9/18/2024
		Main Sump Pump BSD-1 - Motor	Condition	Good			1				9/18/2024
			Operation	Not Operated			1				9/18/2024
		Main Sump Pump BSD-1 - US Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
		Main Sump Pump BSD-1 - DS Valve	Condition	Check Vavle Leaks. All valves appear to be recently painted.			2				9/18/2024
			Operation	Good			2				9/18/2024
10300 Drainage and Pumping System	3 Island Portal Pump Room	Drainage Pump BSD-2 - Pump	Condition	Good			1				9/18/2024
,	·		Operation	Good			1				9/18/2024
		Drainage Pump BSD-2 - Motor	Condition	Good			1				9/18/2024
			Operation	Good			1				9/18/2024
		Drainage Pump BSD-2 - US Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
		Drainage Pump BSD-2 - DS Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
10300 Drainage and Pumping System	3 Island Portal Pump Room	Drainage Pump BSD-3 - Pump	Condition	Good			1				9/18/2024
,	, , , , , , , , , , , , , , , , , , ,		Operation	Good.			1				9/18/2024
		Drainage Pump BSD-3 - Motor	Condition	Good			1				9/18/2024
			Operation	Good			1				9/18/2024
		Drainage Pump BSD-3 - US Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
		Drainage Pump BSD-3 - DS Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
10300 Drainage and Pumping System	3 Island Portal Pump Room	Booster Pump BSW-1 - Pump	Condition	Good			1				9/18/2024
,			Operation	Good			1				9/18/2024
		Booster Pump BSW-1 - Motor	Condition	Good			1				9/18/2024
			Operation	Good			1				9/18/2024
		Booster Pump BSW-1 - US Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024
			Operation	Good			1				9/18/2024
		Booster Pump BSW-1 - DS Valve	Condition	Good. All valves appear to be recently painted.			1				9/18/2024

10000							
			Operation	Good	1		9/18/2024
	land Portal mp Room	Booster Pump BSW-2 - Pump	Condition		1		9/18/2024
			Operation		1		9/18/2024
	E	Booster Pump BSW-2 - Motor	Condition		1		9/18/2024
			Operation		1		9/18/2024
	E	Booster Pump BSW-2 - US Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
	E	Booster Pump BSW-2 - DS Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
	land Portal mp Room	Domestic W Pump BSW-3 - Pump	Condition		1		9/18/2024
System	mp Room		Operation		1		9/18/2024
	C	Domestic W Pump BSW-3 - Motor	Condition		1		9/18/2024
			Operation		1		9/18/2024
	[Domestic W Pump BSW-3 - US Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
	[Domestic W Pump BSW-3 - DS Valves	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
	land Portal Imp Room	Hydropnuematic Tank	Condition	Good	1		9/18/2024
System Pun	mp Room		Operation	Good	1		9/18/2024
	land Portal Imp Room	Room Sump Pump	Condition	Good. Sump is dry.	1		9/18/2024
System Pun	mp Room		Operation	Good	1		9/18/2024
10300 Drainage and Pumping System Ches. N	Mid-Channel S	Sump Room	Condition	Good. Regular problem with hay preventing check valve seal, losing prime on pump. Minor sruface corrosion on pipe supports.	1		9/18/2024
			Operation	Good Good	1		9/18/2024
10300 Drainage and Pumping Ches. N	Mid-Channel [Drainage Pump BMD-1 - Pump	Condition	Good	1		9/18/2024
System			Operation	Good	1		9/18/2024
	C	Drainage Pump BMD-1 - Motor	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BMD-1 - US Valve	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BMD-1 - DS Valve	Condition	Good. All valves appear to be recently painted. Monor	1		9/18/2024
			Operation	corrosion at seal. Good	1		9/18/2024
10300 Drainage and Pumping Ches. N	Mid-Channel [Drainage Pump BMD-2 - Pump	Condition	Good	1		9/18/2024
SVCTOM			Operation	Good			9/18/2024

		Drainage Pump BMD-2 - Motor	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BMD-2 - US Valve	Condition	Good. All valves appear to be recently painted. Monor corrosion at seal.	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BMD-2 - DS Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
10300 Drainage and Pumping System	4 Island Portal Pump Room	Main Sump Pump BND-1 - Pump	Condition	Good. Cracks on concrete pedestal.	1		9/18/2024
5,555			Operation	Rear packing seal reached 180F after 5 minutes of operation.	3		9/18/2024
		Main Sump Pump BND-1 - Motor	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Main Sump Pump BND-1 - US Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
		Main Sump Pump BND-1 - DS Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
10300 Drainage and Pumping System	4 Island Portal Pump Room	Drainage Pump BND-2 - Pump	Condition	Good. Cracks on concrete pedestal.	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BND-2 - Motor	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BND-2 - US Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BND-2 - DS Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
10300 Drainage and Pumping System	4 Island Portal Pump Room	Drainage Pump BND-3 - Pump	Condition	Good. Cracks on concrete pedestal.	1		9/18/2024
•	•		Operation	Good	1		9/18/2024
		Drainage Pump BND-3 - Motor	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BND-3 - US Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
		Drainage Pump BND-3 - DS Valve	Condition	Good. All valves appear to be recently painted.	1		9/18/2024
			Operation	Good	1		9/18/2024
10300 Drainage and Pumping System	4 Island Portal Pump Room	Room Sump Pump	Condition	Good	1		9/18/2024
			Operation	Good	1		9/18/2024

SNTI Element	Component	Item Name,		Inspection Findings	Output Voltage	Thermal Reading	Condition	Photo	Maintenance or Repair Recommendation	Repair	Date
		Description or Location	Condition / Operation		Acceptable (Y/N)	∘F	State (1-4)			Priority	Inspected MM/DD/YY
10400 Emergency Generator System	3 Island Emerg. Generator	Island 3 Generator	Condition	Good. Still looks brand new. Runtime: 179.8 (from 169.4 hrs. in 2020 and 156 in 2018), Battery Voltage- 27.4 V, fuel filter date: 1/27/21, Fuel & water sperator date: 1/27/2024, Oil filter date- 7-23-2024	600V-(Y)	157	1				9/17/2024
			Operation	Good. The generator was ran, under load, for about 15 minutes.			1				9/17/2024
		Fuel Pump	Condition	Good			1				9/17/2024
			Operation	Good.							9/17/2024
		Supply Air Louvers	Condition	Fair. Not a water tight seal.			2		Recommended to put an NEC acceptable drip pan under the louvers.		9/17/2024
			Operation	Good, open and close as needed. Although not weather tight							9/17/2024
		Day Fuel Tank	Condition	Good			1				9/17/2024
		Exhaust System	Condition	Good			1				9/17/2024
10400 Emergency Generator System	4 Island Emerg. Generator	Island 4 Generator	Condition	Good. Still looks brand new. Runtime: 133.8 hrs (from 124.2 hrs. in 2020), Battery Voltage- 26.3 V, fuel filter date: 7/28/22, Fuel & water sperator date: 7/26/2021, Oil filter date- 7-22-2024	600V-(Y)	126	1				9/17/2024
			Operation	Good. The generator was ran, under load, for about 15 minutes.							9/17/2024
		Fuel Pump	Condition	Good			1				9/17/2024
			Operation	Good.							9/17/2024
		Supply Air Louvers	Condition	Fair. Not a water tight seal.			2		Recommended to put an NEC acceptable drip pan under the louvers.		9/17/2024
			Operation	Good, open and close as needed. Although not weather tight							9/17/2024
		Day Fuel Tank	Condition	Good			1				9/17/2024
		Exhaust System	Condition	Good			1				9/17/2024
											9/17/2024
											9/17/2024
10550 Emergency Distribution System	3 Island Emerg. System	ATS	Condition	Good			1				9/17/2024
			Operation	Good. Power transfer was completed in less than 10 seconds			1				9/17/2024
		UPS	Condition	Good			1				9/17/2024
			Operation	Good			1				9/17/2024
		UPS Batteries	Condition	Good			1				9/17/2024
			Operation	Good			1				9/17/2024
		Wiring / Conduit	Condition	Good			1				9/17/2024
											9/17/2024
10550 Emergency Distribution System	4 Island Emerg. System	ATS	Condition	Good			1				9/17/2024

Chesapeake Tunnel - Emergency Generator System

		Operation	Good. Power transfer was completed in less than 10 seconds		1		9/17/2024
	UPS	Condition	Good		1		9/17/2024
		Operation	Good		1		9/17/2024
	UPS Batteries	Condition	Good		1		9/17/2024
		Operation	Good		1		9/17/2024
	Wiring / Conduit	Condition	Good		1		9/17/2024

			Last Serviced	Service Due	Thermal	0			0111		Noise	Condition	Comments
SNTI Element	Component/Label	Item Name, Description or Location	DD/MM/YY	DD/MM/YY	Reading oC	Operational (Y/N)	Volts	Current	Oil Level (XFMR)	PSIG	(dB)	Condition State (1-4)	
SOUTH SHORE SUBS		,											
10500 Electrical Distribution						Y		11				2	FairThis is determined by the age vs life expectancy of the equipment.
System	FDR NB20	Medium Voltage Switchgear											FairThis is determined by the age vs life expectancy of the equipment.
	Secondary Main ACB No. 1	Medium Voltage Switchgear				Y	13,440	23				2	
	Secondary Main ACB No. 2	Medium Voltage Switchgear				Y	13,929	24				2	FairThis is determined by the age vs life expectancy of the equipment.
	Bus Tie ACB No. 3	Medium Voltage Switchgear				Υ		0				2	FairThis is determined by the age vs life expectancy of the equipment.
						Y		9				2	Open at time of inspection. FairThis is determined by the age vs life expectancy of the equipment.
	FDR NB10	Medium Voltage Switchgear	5/29/2018	5/29/2019		Y	42.440	6					Good
	Feeder Breaker 1200 Amp	Medium Voltage Switchgear				Y	13,440	ь				1	Tap changer (2 Left)
	XFMR #1 (West)	2000KVA Transformer #1			40	Y	34.5 kV-13.8 kV		30	2 left		2	
					40	.,	245111422111		30	4 left			Tap changer (4 Left)
	XFMR #2 (East)	2000KVA Transformer #2			40	Y	34.5 kV-13.8 kV		30	4 lett		2	
	AT WIN #2 (Last)	2000KVA HallStofflet #2											
				1									
	ON BUILDING #4 - CHE	SAPEAKE TUNNEL											
10500 Electrical Distribution System	MB FDR NB23	Medium Voltage Switchgear				Y	13,320	8				1	Good
,,	FB SB14	Medium Voltage Switchgear				Y	N/A	8				1	Good
	FB XFMR #2	Medium Voltage Switchgear				Υ	N/A	2				1	Good
	TIE BREAKER	Medium Voltage Switchgear				Y	N/A	0				1	Good, switched off during inspection
	MB FDR NB13	Medium Voltage Switchgear				Υ	13,440	7				1	Good
	FB NB14	Medium Voltage Switchgear				Y	N/A	7				1	Good
	FB XFMR #1	Medium Voltage Switchgear				Y	N/A	5				1	Good
	TIE BREAKER	Medium Voltage Switchgear				Y	N/A	0				1	Good, switched off during inspection
	LVSG Enclosure	Low Voltage Switchgear	4/20/2021			Y	600V					1	Good
	Trans #1	1000KVA Transformer #1	./20/2021		52	Y	13.8KV-600/347V		26	1.3		1	Good
	Trans #2	1000KVA Transformer #1 1000KVA Transformer #2			48	Υ	13.8KV-600/347V		25	1.8		1	Good
	House Bus PB 1	Distribution Panel				Y	208/120					1	Good
	House Bus PB 2	Distribution Panel				Υ	208/120					1	Good
		Distribution Panel Distribution Panel				Y	208/120					1	Good
	Essential Service PB					Y	600V					1	
	Emergency Systems	Diesel Genrator Switchboard			43	Y	600V-208/120V		25	1.6		1	Good
	HT1	House Transformer #1 - 150KVA 3PH 60HZ			47	Y	600V-208/120V		25	1.0		1	Good
	HT2	House Transformer #2 - 150KVA 3PH 60HZ			47	,	600V-208/120V		25	1		1	Good
	Outlets/Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1											Good
	Outlets/Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2											Good
	Raceways/Overhead Wiring	General Overhead Raceways and Wiring Chases within Electrical Floor											Good, some general conduit corrosion in some locations, mostly on Levels 2 and 3, but not widespread
	Disconnect Switches	Main Power Transformer											Good
SOUTH VENTILATIO	N BUILDING #3 - CHES	APEAKE TUNNEL	II.	1	l .								1
10500 Electrical Distribution		-				Y	13,440					1	Good
System	MB FDR SB12 FB NB23	Medium Voltage Switchgear Medium Voltage Switchgear				Y	N/A	10				1	Good
	FB XFMR #2	Medium Voltage Switchgear				Y	N/A	5				1	Good
	MB M1 FDR NB22 MB M1 FDR NB12	Medium Voltage Switchgear Medium Voltage Switchgear				Y	13800 13440	7				1	Good Good
	FB NB13	Medium Voltage Switchgear				Y	N/A	6				1	Good
	FB XFMR #1 MB M2 FDR SB12	Medium Voltage Switchgear Medium Voltage Switchgear				Y	N/A 13440	0				1	Good Good
						Y	600V					1	Good
	LVSG Enclosure	600 V Switchboard			51	Y	13.8KV-600/347V		24	2		1	Good
	Main Trans #1	1000KVA Transformer #1					· ·						Good
	Main Trans #2	1000KVA Transformer #2			58	Y	13.8KV-600/347V		25	2		1	
	House Bus PB 1	Distribution Panel				Y	208/120					1	Good
	House Bus PB 2	Distribution Panel				Υ	208/120					1	Good
	Essential Service PB	Distribution Panel				Y	208/120					1	Good
						Y	600V					1	Good
	Emergency Systems	Diesel Generator Switchboard			47	Y	600V-208/120		26	1.6		1	Good
	HT1	House Transformer #1 - 150KVA 3PH 60HZ											Good
	HT2	House Transformer #2 - 150KVA 3PH 60HZ			48	Y	600V-208/120		25	1.5		1	
	Outlets/Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1										1	Good
	Outlets/Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2										1	Good
		General Overhead Raceways and Wiring Chases within											Good, Minor Corrosion above panel LP2, moderate corrosion above
	Raceways/Overhead Wiring	Electrical Floor											central vacumm, mainly large conduits on right Good
	Disconnect Switches	Main Power Transformer											
	Other												LP2 panel breaker 1 is bad (not used)
											<u></u>		

Chesapeake Tunnel - Power Distribution

Chesapeake Tunnel - Power Distribution

SNTI Element	Component	Item Name, Description or Location	Condition / Operation	Inspection Findings	Detail	Thermal Reading	Condition State	Photo	Maintenance or Repair Recommendation	Repair Priority	Date Inspected
						vF	(1-4)				MM/DD/Y
10600 Tunnel Lighting System	Lights	Phyical Light Bulbs	Condition	Lighting has been replaced with LED lighting			1				9/19/202
			Operation	Good			1				9/19/202
10600 Tunnel Lighting System	Lenses	Light Fixture Cover/Lenses	Condition	Good			1				9/19/202
10600 Tunnel Lighting System	Conduit	Conduits connecting Fixtures	Condition	Good			1				9/19/202
10600 Tunnel Lighting System	Wireways	Lighting wireways powering fixtures	Condition	Good			1				9/19/202
10600 Tunnel Lighting System	PLC	Programmable Logic Controllers for Tunnel Lighting Sensors	Condition	Good			1				9/19/202
			Operation	Good			1				9/19/202
10600 Tunnel Lighting System	Luminance Met		Operation	West wall, 3 units not lit/not functioning at station 476+00			N/A				9/19/202
		portal opening		Station 469+00: SB Lane 35.24 FC, Centerline 32.51 FC, NB Lane 31.46 FC			1				9/19/2024
				Station 478+45: SB Lane 35.54 FC, Centerline 33.69 FC, NB Lane 32.9 FC			1				9/19/2024
				Station 484+22: SB Lane 32.59 FC, Centerline 34.04 FC, NB Lane 35.14 FC			1				9/19/2024
				Station 495+60: SB Lane 31.62 FC, Centerline 37.67 FC, NB Lane 38.53 FC			1				
							1				9/19/2024
				Station 501+90: SB Lane 38.74 FC, Centerline 35.25 FC, NB Lane 36.4 FC			1				9/19/2024
				Station 513+31: SB Lane 32.15 FC, Centerline 33.56 FC, NB Lane 33.57 FC			1				9/19/2024
				Station 522+54: SB Lane 33.24 FC, Centerline 31.03 FC, NB Lane 27.97 FC							9/19/2024
10600 Tunnel Lighting System	LDP3-1	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
	LDP3-2	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
	LDP3-3	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
	LDP3-4	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
10600 Tunnel Lighting System	LDP4-1	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
		-89	Operation	Good			1				9/19/2024
	LDP4-2	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				
	LDI 4-2	Lighting Distribution 1 and 1 Substation 3 (Chesapeake)					1				9/19/2024
	1004.3	Lighting Distribution Dagal Cubstation 2 (Character)	Operation	Good			1				9/19/2024
	LDP4-3	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
	LDP4-4	Lighting Distribution Panel - Substation 3 (Chesapeake)	Condition	Good			1				9/19/2024
			Operation	Good			1				9/19/2024
10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Chesapeake Tunnel) - Substation 3	Condition	Good		<150	1				9/19/2024
			Operation	Good			1				9/19/2024
	CBW-2	112.5KVA Dry Type Transformer for LDP2 (Chesapeake Tunnel) - Substation 3	Condition	Good		<150	1				9/19/2024
			Operation	Good			1				9/19/2024
	CBE-3	112.5KVA Dry Type Transformer for LDP3 (Chesapeake Tunnel) - Substation 3	Condition	Good		<150	1				9/19/2024
			Operation	Good			1				9/19/2024

	CBE-4	150KVA Dry Type Transformer for LDP4 (Chesapeake Tunnel) - Substation 3	Condition	Good	<150	1		9/19/2024
			Operation	Good		1		9/19/2024
10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Chesapeake Tunnel) - Substation 4	Condition	Good	<150	1		9/19/2024
			Operation	Good		1		9/19/2024
	CBW-2	112.5KVA Dry Type Transformer for LDP2 (Chesapeake Tunnel) - Substation 4	Condition	Good	<150	1		9/19/2024
			Operation	Good		1		9/19/2024
	CBE-3	112.5KVA Dry Type Transformer for LDP3 (Chesapeake Tunnel) - Substation 4	Condition	Good	<150	1		9/19/2024
			Operation	Good		1		9/19/2024
	CBE-4	150KVA Dry Type Transformer for LDP4 (Chesapeake Tunnel) - Substation 4	Condition	Good	<150	1		9/19/2024
			Operation	Good				9/19/2024
10601 Tunnel Lighting Fixture	Housing	Lighting Fixture Housing	Condition	All good				9/19/2024
10601 Tunnel Lighting Fixture	Connections	Connection to the Tunnel Lining	Condition	Loose anchor at East wall station 469+30 Loose anchor at East wall station 469+60		2		9/19/2024
				ILDUSE dIILIIUI di Edst Wali Statiuii 409+00				
								+

SNTI Element	Component	Item Name, Description or Location	Condition / Operation	Inspection Findings	Condition State	Photo	Maintenance or Repair Recommendation	Repair Priority	Date Inspected
					(1-4)				MM/DD/YY
10650 Fire Detection System	Ches. Tunnel CCTV	P-C01	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C17	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C02	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C03	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C04	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C05	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C06	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C07	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C08	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C09	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C10	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C11	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C12	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		F-C16	Condition	Good					9/18/2024
			Operation	Good.					9/18/2024
		P-C13 Portal Pump Room 3 Isl	Condition	Good					9/18/2024

			Operation	Good.			9/18/2024
			Condition	Good			9/18/2024
		Room	Operation	Good.			9/18/2024
		P-C15 Portal Pump	Condition	Good			9/18/2024
		Room 4 Isl	Operation	Good.			9/18/2024
10650 Fire Detection System	Ches. Tunnel	470+02	Condition	Good	1		9/18/2024
	Emerg. Tele.		Operation	Good. 331	1		9/18/2024
		472+95	Condition	Good	1		9/18/2024
			Operation	Good. 330	1		9/18/2024
		475+85	Condition	Good	1		9/18/2024
			Operation	Good. 329	1		9/18/2024
		478+80	Condition	Good	1		9/18/2024
			Operation	Good. 328	1		9/18/2024
		481+72	Condition	Good	1		9/18/2024
			Operation	Good. 327	1		9/18/2024
		484+65	Condition	Good	1		9/18/2024
			Operation	Good. 326	1		9/18/2024
		487+57	Condition	Good	1		9/18/2024
			Operation	Good. 325	1		9/18/2024
		490+47	Condition	Good	1		9/18/2024
			Operation	Did Not Function. 324	3	Restore functionality	9/18/2024
		493+45	Condition	Good	1		9/18/2024
			Operation	Good. 323	1		9/18/2024
		496+37	Condition	Good	1		9/18/2024
			Operation	Good. 322	1		9/18/2024
		499+30	Condition	Good	1		9/18/2024
			Operation	Good. 319	1		9/18/2024
		502+22	Condition	Good	1		9/18/2024
			Operation	Good. 318	1		9/18/2024
		505+15	Condition	Good	1		9/18/2024
			Operation	Good. 317	1		9/18/2024

		508+07	Condition	Good	1		0/19/2024
		308+07			1		9/18/2024
			Operation	Good. 316	1		9/18/2024
		510+67	Condition	Good	1		9/18/2024
			Operation	Good. 315	1		9/18/2024
		513+60	Condition	Good	1		9/18/2024
			Operation	Good. 314	1		9/18/2024
		516+55	Condition	Good	1		9/18/2024
			Operation	Good. 313	1		9/18/2024
		519+47	Condition	Good	1		9/18/2024
			Operation	Did Not Function. 312	3	Restore functionality	9/18/2024
		522+47	Condition	Good	1		9/18/2024
			Operation	Good. 311	1		9/18/2024
		Portal Pump Room 3 Island	Condition	Good	1		9/18/2024
		155.14	Operation	Good.	1		9/18/2024
		Mid-Channel Pump Room	Condition	Good	1		9/18/2024
			Operation	Good.	1		9/18/2024
		Portal Pump Room 4 Isl	Condition	Good	1		9/18/2024
			Operation	Good.	1		9/18/2024
10700 Fire Protection System	Hose Valve	470+05	Condition / Operation	Fair	2		9/19/2024
		471+54	Condition / Operation	Fair	2		9/19/2024
		473+00	Condition / Operation	Fair	2		9/19/2024
		474+50	Condition / Operation	Fair	2		9/19/2024
		475+93	Condition / Operation	Fair	2		9/19/2024
		477+39	Condition / Operation	Fair	2		9/19/2024
		478+85	Condition / Operation	Fair	2		9/19/2024
		480+31	Condition / Operation	Fair	2		9/19/2024
		481+77	Condition / Operation	Fair	2		9/19/2024
		483+24	Condition / Operation	Fair	2		9/19/2024
			Condition / Operation	Fair	2		9/19/2024
		486+16	Condition / Operation	Fair	2		9/19/2024
		487+62	Condition / Operation	Fair	2		9/19/2024

		1	1		T	1	
			Condition / Operation	Fair	2		9/19/2024
			Condition / Operation	Fair	2		9/19/2024
			Condition / Operation	Fair	2		9/19/2024
		493+47	Condition / Operation	Fair	2		9/19/2024
		494+94	Condition / Operation	Fair	2		9/19/2024
		496+40	Condition / Operation	Fair	2		9/19/2024
		497+86	Condition / Operation	Fair	2		9/19/2024
		499+32	Condition / Operation	Fair	2		9/19/2024
		500+79	Condition / Operation	Fair	2		9/19/2024
		502+25	Condition / Operation	Fair	2		9/19/2024
		503+72	Condition / Operation	Fair	2		9/19/2024
		505+18	Condition / Operation	Fair	2		9/19/2024
		506+63	Condition / Operation	Fair	2		9/19/2024
		508+10	Condition / Operation	Fair	2		9/19/2024
		509+43	Condition / Operation	Fair	2		9/19/2024
		510+76	Condition / Operation	Fair	2		9/19/2024
		512+23	Condition / Operation	Fair	2		9/19/2024
		513+68	Condition / Operation	Fair	2		9/19/2024
		515+15	Condition / Operation	Fair	2		9/19/2024
		516+61	Condition / Operation	Fair	2		9/19/2024
			Condition / Operation	Fair	2		9/19/2024
		519+53	Condition / Operation	Fair	2		9/19/2024
		521+00	Condition / Operation	Fair	2		9/19/2024
		522+41	Condition / Operation	Fair	2		9/19/2024
10700 Fire Protection System	rire Extiliguisher		Condition / Operation	Satisfactory	1		7/30/2024
		469+99 SOS	Condition / Operation	Satisfactory	1		6/25/2024
		471+48 SS	Condition / Operation	Satisfactory	1		6/25/2024
		471+48 SOS	Condition / Operation	Satisfactory	1		6/25/2024
		472+95 SS	Condition / Operation	Satisfactory	1		7/30/2024
						<u> </u>	

Chesapeake Tunnel - FLS

				·	reance ranner 125
472+95 SOS	Condition / Operation	Satisfactory	1		6/25/2024
474+44 SS	Condition / Operation	Satisfactory	1		6/25/2024
474+44 SOS	Condition / Operation	Satisfactory	1		6/25/2024
475+87 SS	Condition / Operation	Satisfactory	1		7/30/2024
475+87 SOS	Condition / Operation	Satisfactory	1		6/25/2024
477+33 SS	Condition / Operation	Satisfactory	1		6/25/2024
477+33 SOS	Condition / Operation	Satisfactory	1		6/25/2024
478+79 SS	Condition / Operation	Satisfactory	1		6/25/2024
478+79 SOS	Condition / Operation	Satisfactory	1		6/25/2024
480+26 SS	Condition / Operation	Satisfactory	1		6/25/2024
480+26 SOS	Condition / Operation	Satisfactory	1		6/25/2024
481+72 SS	Condition / Operation	Satisfactory	1		6/25/2024
481+72 SOS	Condition / Operation	Satisfactory	1		6/25/2024
483+18 SS	Condition / Operation	Satisfactory	1		6/25/2024
483+18 SOS	Condition / Operation	Satisfactory	1		6/25/2024
484+64 SS	Condition / Operation	Satisfactory	1		6/25/2024
484+64 SOS	Condition / Operation	Satisfactory	1		6/25/2024
486+10 SS	Condition / Operation	Satisfactory	1		6/25/2024
486+10 SOS	Condition / Operation	Satisfactory	1		6/25/2024
487+56 SS	Condition / Operation	Satisfactory	1		6/25/2024
487+56 SOS	Condition / Operation	Satisfactory	1		6/25/2024
489+03 SS	Condition / Operation	Satisfactory	1		6/25/2024
489+03 SOS	Condition / Operation	Satisfactory	1		6/25/2024
490+49 SS	Condition / Operation	Satisfactory	1		6/25/2024
490+49 SOS	Condition / Operation	Satisfactory	1		6/25/2024
491+95 SS	Condition / Operation	Satisfactory	1		6/25/2024
491+95 SOS	Condition / Operation	Satisfactory	1		7/30/2024
493+42 SS	Condition / Operation	Satisfactory	1		6/25/2024
493+42 SOS	Condition / Operation	Satisfactory	1		6/25/2024
494+88 SS	Condition / Operation	Satisfactory	1		6/25/2024
494+88 SOS	Condition / Operation	Satisfactory	1		6/25/2024
494+88 SOS	Condition / Operation	Satisfactory	1		6/25/2024

496+34 SS	Condition / Operation	Satisfactory	1	6/25/2024
496+34 SOS	Condition / Operation	Satisfactory	1	6/25/2024
497+81 SS	Condition / Operation	Satisfactory	1	6/25/2024
497+81 SOS	Condition / Operation	Satisfactory	1	6/25/2024
499+27 SS	Condition / Operation	Satisfactory	1	6/25/2024
499+27 SOS	Condition / Operation	Satisfactory	1	6/25/2024
500+73 SS	Condition / Operation	Satisfactory	1	6/25/2024
500+73 SOS	Condition / Operation	Satisfactory	1	7/30/2024
502+19 SS	Condition / Operation	Satisfactory	1	6/25/2024
502+19 SOS	Condition / Operation	Satisfactory	1	6/25/2024
503+66 SS	Condition / Operation	Satisfactory	1	6/25/2024
503+66 SOS	Condition / Operation	Satisfactory	1	6/25/2024
505+12 SS	Condition / Operation	Satisfactory	1	6/25/2024
505+12 SOS	Condition / Operation	Satisfactory	1	6/25/2024
506+67 SS	Condition / Operation	Satisfactory	1	6/25/2024
506+67 SOS	Condition / Operation	Satisfactory	1	6/25/2024
508+04 SS	Condition / Operation	Satisfactory	1	6/25/2024
508+04 SOS	Condition / Operation	Satisfactory	1	6/25/2024
509+37 SS	Condition / Operation	Satisfactory	1	6/25/2024
509+37 SOS	Condition / Operation	Satisfactory	1	6/25/2024
510+70 SS	Condition / Operation	Satisfactory	1	6/25/2024
510+70 SOS	Condition / Operation	Satisfactory	1	6/25/2024
512+17 SS	Condition / Operation	Satisfactory	1	6/25/2024
512+17 SOS	Condition / Operation	Satisfactory	1	6/25/2024
513+63 SS	Condition / Operation	Satisfactory	1	6/25/2024
513+63 SOS	Condition / Operation	Satisfactory	1	6/25/2024
515+09 SS	Condition / Operation	Satisfactory	1	6/25/2024
515+09 SOS	Condition / Operation	Satisfactory	1	6/25/2024
516+55 SS	Condition / Operation	Satisfactory	1	6/25/2024
516+55 SOS	Condition / Operation	Satisfactory	1	6/25/2024
518+01 SS	Condition / Operation	Satisfactory	1	6/25/2024

Chesapeake Tunnel - FLS

		518+01 SOS	Condition / Operation	Satisfactory	1		6/25/2024
		519+47 SS	Condition / Operation	Satisfactory	1		6/25/2024
		519+47 SOS	Condition / Operation	Satisfactory	1		6/25/2024
		520+94 SS	Condition / Operation	Satisfactory	1		6/25/2024
		520+94 SOS	Condition / Operation	Satisfactory	1		6/25/2024
		522+50 SS	Condition / Operation	Satisfactory	1		6/25/2024
		522+50 SOS	Condition / Operation	Satisfactory	1		6/25/2024
10700 Fire Protection System	Ground Water Storage Tank #2	Height transducer	Condition / Operation	Faulty, not reading correcity.	N/A		9/17/2024
10800 Tunnel Operations and Security System	Door Access Security	VB 3 Exterior Doors	Condition / Operation				
		3 Island Portal - Sidewalk Side	Condition / Operation				
		3 Island Portal - Side Opp. Sidewalk	Condition / Operation				
		VB 4 Exterior Doors	Condition / Operation				
		4 Island Portal - Sidewalk Side	Condition / Operation				
		4 Island Portal - Side Opp. Sidewalk	Condition / Operation				
	SCADA - Telephones	Emergency Window Information	Operation				





13. Tunnel Major Preventative Maintenance Records

1 | 1

Ventilation Systems - Chesapeake Tunnel

VEII			<u> nesapeake runr</u>		
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date of Maintenance MM/DD/YYYY
1	10200 Ventilation System	Damper BSB1, BSB2 and BSB3,BSE1, BSE 2, BSE3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	
		Damper BSB1, BSB2 and BSB3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all blower chains drive components inspected and greased.	8/6/2024
		Damper BSE1, BSE2, BSE3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all exhaust chains drive components inspected and greased.	8/8/2024
2	10200 Ventilation System	Damper BNB1, BNB2, BNB3, BNE1, BNE 2, BNE 3		Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	
		Damper BNB1, BNB2, BNB3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all chains drive components inspected and greased.	8/13/2024
		Damper BNE1, BNE 2, BNE 3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all chains drive components inspected and greased.	8/15/2024

Ventilation Systems - Chesapeake Tunnel

	SNTI Element		hesapeake Tuni	Preventative Maintenance and/or	Date of
Line Item			Item Name, Description or Location	Repair Performed	Maintenance MM/DD/YYYY
3	10200 Ventilation System	CO Monitor	North Exhaust Duct, South Exhaust Duct		
4	10201 Fans	Fan BSB 1, BSB2, BSB3, BSE 1, BSE 2, BSE 3	Fan Motor, Motor Controller, MC E-Stop	Fans operated for a 48 hr rotation. Checked by maintainer daily. Any issues inspected and addressed by oppropriate E/M personnel.	
		Fan BSB 1, BSB2, BSB3, BSE 1, BSE 2, BSE 3	Fan Motor, Motor Controller, MC E-Stop	MC E-stop exercised yearly	8/12/2024
5	10201 Fans	Fan BSB 1, BSB2, BSB3, BSE 1, BSE 2, BSE 3	Fan Shaft, Bearing Pedestals Bearings, Drive Chains, Fan Housing, Fan Wheel	Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	
		Fan BSB 1, BSB2, BSB3	Three island blower bearing pedestals	Scraped and painted	7/15-19/24

Ventilation Systems - Chesapeake Tunnel

Line Item	SNTI Element	Component	Itam Nama Deceription or	Preventative Maintenance and/or Repair Performed	Date of Maintenance MM/DD/YYYY

Drainage and Pumps - Chesapeake Tunnel

Dian	nage and i	airips Ci	iesapeake run								
Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY						
SOUTH	SOUTH VENTILATION BUILDING #3 - CHESAPEAKE TUNNEL										
1	10301 Drainage and Pumping System	3 Island Portal Pump Room	Main Sump Pump BSD-1, BSD 2, BSD 3 Pumps, Motors, Valves	Checked daily by Maintainer for packing leaks. Inspected weekly by tunnel mechanic for operability, greased, and adjustments made to packing as needed.							
				Exercised and greased all valves in pump room / Greased all pumps	1/23/2024						
2	10301 Drainage and Pumping System	Chesapeake Tunnel Low Point Pump Station (LPPS)	Drainage Pump BMD1 and BMD 2 - Pumps, Motors, Valaves	Monitored by tunnel equipment operator daily via camera system for leaks. Inspected weekly by tunnel mechanic for operability, greased, and adjustments made to packing as needed.							
				Exercised and greased all valves in pump room / Greased all pumps	1/22/2024						

Drainage and Pumps - Chesapeake Tunnel

Diai	Drainage and Pumps - Chesapeake Tunner							
Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
3	10301 Drainage and Pumping System	4 Island Portal Pump Room	Main Sump Pump BND-1, BND 2, BND 3 Pumps, Motors, Valves	Checked daily by Maintainer for packing leaks. Inspected weekly by tunnel mechanic for operability. greased, and adjustments made to packing as needed during weekly inspection.				
				Exercised and greesed all valves in pump room / Greased all pumps	1/18/2024			

Drainage and Pumps - Chesapeake Tunnel

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
-reem			or Escation	remonited	

Emergency Generators - Chesapeake Tunnel

			s - Chesapeake Tur		711		
Line Item	SNTI Element	Component	Item Name, Description or Location	Output Voltage Acceptable (Y/N)	Thermal Reading ∘F	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
			SOUTH VENTILATION	ON BUILDING #	3 - CHESAPEAK	E TUNNEL	
1	10400 Emergency Generator System	3 Island Emerg. Generator	Generator, Fuel Pump, Supply Air Louvers, Day Fuel Tank, Exhaust Manifold, ATS, UPS	Υ	103 Before start up, 175 running temp	System start up and run test	9/6/2023
				Υ	103 / 176	System start up and run test	10/14/2023
				Y	100 / 175	System start up and run test	11/6/2023
				Υ	112 / 177	System start up and run test	1/2/2024
				Υ	105 / 174	System start up and run test	2/15/2024
				Υ	101 / 178	System start up and run test	3/12/2024
				Υ	104 / 180	System start up and run test	4/9/2024
				Υ	105 / 172	System start up and run test	5/1/2024
				Υ	103 / 176	System start up and run test	6/4/2024
				Υ	101 / 172	Oil and filter change / System start up and run test	7/23/2024

Emergency Generators - Chesapeake Tunnel

Line Item	SNTI Element	Component	Item Name, Description or Location	Output Voltage Acceptable (Y/N)	Thermal Reading °F	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
			NORTH VENTILATI	ON BUILDING #	4 - CHESADEAK	TETLINNEL	
19	10400 Emergency Generator System	4 Island Emerg. Generator	Generator, Fuel Pump, Supply Air Louvers, Day Fuel Tank, Exhaust Manifold, ATS, UPS,	Υ	100 / 176	System start up and run test	9/6/2023
				Υ	102 / 179	System start up and run test	10/14/2024
				Υ	107 / 176	System start up and run test	11/6/2023
				Υ	102 / 178	System start up and run test	12/18/2024
				Υ	112 / 177	System start up and run test	1/2/2024
				Υ	104 / 174	System start up and run test	2/15/2024
				Υ	100 / 178	System start up and run test	3/12/2024
				Y	102 / 181	System start up and run test	4/9/2024

Emergency Generators - Chesapeake Tunnel

Emergency denerators - Chesapeake runner								
Line Item	SNTI Element	Component	Item Name, Description or Location	Output Voltage Acceptable (Y/N)	Thermal Reading °F	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY	
				Υ	113' / 179'	System start up and run test	5/1/2024	
				Υ	116' / 177'	System start up and run test	6/4/2024	
				Υ	114' / 175'	Oil and filter change / system start up and run test	7/22/2024	

<u>Power Distribution - Chesapeake Tunnel</u>

Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY						
	NORTH VENTILATION BUILDING #4 - CHESAPEAKE TUNNEL										
		#4 island Power Distribution System	#4 Island 5th Floor	All components are checked daily by the equipment maintainers for operation, noise and temperature. Any issues are adressed by the Electrician							
1	10500 Electrical Distribution System	MB FDR NB23	Medium Voltage Switchgear								
2		FB SB14	Medium Voltage Switchgear								
3		FB XFMR #2	Medium Voltage Switchgear								
4	10500 Electrical Distribution System	TIE BREAKER	Medium Voltage Switchgear								
5		MB FDR NB13	Medium Voltage Switchgear								
6		FB NB14	Medium Voltage Switchgear								
7		FB XFMR #1	Medium Voltage Switchgear								
8		TIE BREAKER	Medium Voltage Switchgear								
9	10500 Electrical Distribution System	LVSG Enclosure	600 V Switchboard								
10	10500 Electrical Distribution System	Trans #1	1000KVA Transformer #1								
11		Trans #2	1000KVA Transformer #2								
12	10500 Electrical Distribution System	House Bus PB 1	Distribution Panel								

<u>Power Distribution - Chesapeake Tunnel</u>

	CI DISCIII	00.0.0	ciicsapcake		
Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
13		House Bus PB 2	Distribution Panel		
14		Essential Service PB	Distribution Panel		
15		Emergency Systems	Diesel Generator Switchboard		
16		HT1	House Transformer #1 - 150KVA 3PH 60HZ		
17		HT2	House Transformer #2 - 150KVA 3PH 60HZ		
18		Outlets/ Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1		
19	10500 Electrical Distribution System	Outlets/ Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2		
20		Raceways/ Overhead Wiring	General Overhead Raceways and Wiring Chases within Electrical Floor		
21	10500 Electrical Distribution System	Disconnect Switches	Main Power Transformer		
		so	UTH VENTILATION BU	JILDING #3 - CHESAPEAKE TUNNEL	
		#3 Island Power Distribution System	#3 Island 5th Floor	All components are checked daily by the equipment maintainers for operation, noise and temperature. Any issues are adressed by the Electrician	
22	10500 Electrical Distribution System	MB FDR NB12	Medium Voltage Switchgear		
23		FB NB23	Medium Voltage Switchgear		
24		FB XFMR #2	Medium Voltage Switchgear		

<u>Power Distribution - Chesapeake Tunnel</u>

Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
25	10500 Electrical Distribution System	MB M1 FDR NB22	Medium Voltage Switchgear		
26		MB M1 FDR NB12	Medium Voltage Switchgear		
27		FB NB13	Medium Voltage Switchgear		
28		FB XFMR #1	Medium Voltage Switchgear		
29		MB M2 FDR SB12	Medium Voltage Switchgear		
30	10500 Electrical Distribution System	LVSG Enclosure	600 V Switchboard		
31	10500 Electrical Distribution System	Trans #1	1000KVA Transformer #1		
32		Trans #2	1000KVA Transformer #2		
33	10500 Electrical Distribution System	House Bus PB 1	Distribution Panel		
34		House Bus PB 2	Distribution Panel		
35		Essential Service PB	Distribution Panel		
36		Emergency Systems	Diesel Generator Switchboard		
37		HT1	House Transformer #1 - 150KVA 3PH 60HZ		
38		HT2	House Transformer #2 - 150KVA 3PH 60HZ		

<u>Power Distribution - Chesapeake Tunnel</u>

Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
39	10500 Electrical Distribution System	Outlets/ Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1		
40		Outlets/ Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2		
41	10500 Electrical Distribution System	Damper BSE3	General Overhead Raceways and Wiring Chases within Electrical Floor		

<u>ı uılı</u>	difficiency - Chesapeake ruffilei						
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY		
1	10600 Tunnel Lighting System	Lights	LED	Inspection and repair of all anchors and repair of lighting North and Southbound lanes	08/5-6/24		
2							
3	10600 Tunnel Lighting System	Lenses	Light Fixture Cover/Lenses	Inspection and repair North and Southbound lanes	08/5-6/24		
4	10600 Tunnel Lighting System	Conduit	Conduits connecting Fixtures	Inspection and repair North and Southbound lanes	08/5-6/24		
5	10600 Tunnel Lighting System	Wireways	Lighting wireways powering fixtures	Inspection and repair North and Southbound lanes	08/5-6/24		
6	10600 Tunnel Lighting System	PLC	Programmable Logic Controllers for Tunnel Lighting Sensors	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
7							
8	10600 Tunnel Lighting System	Luminance Meter	Lumaniance Meter Located near the top of the open approach directed at the portal opening	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
9	10600 Tunnel Lighting System	LDP3-1	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
10							
	·	·	·				

IuIII	Tunnel Lighting - Chesapeake Tunnel							
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
11		LDP3-2	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
12								
13		LDP3-3	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
14								
15		LDP3-4	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
16								
17	10600 Tunnel Lighting System	LDP4-1	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
18								
19		LDP4-2	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
20								
21		LDP4-3	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
22								

IuIII	runner Lighting - Chesapeake Tunner							
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
23		LDP4-4	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
24								
25	10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Thimble Shoals Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
26								
27		CBW-2	112.5KVA Dry Type Transformer for LDP2 (Chesapeake Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
28								
29		CBE-3	112.5KVA Dry Type Transformer for LDP3 (Thimble Shoals Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
30								
31		CBE-4	150KVA Dry Type Transformer for LDP4 (Chesapeake Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
32								
33	10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Chesapeake Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
34								

IuIII	unner Lighting - Chesapeake Tunner					
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY	
35		CBW-2	112.5KVA Dry Type Transformer for LDP2 (Chesapeake Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel		
36						
37		CBE-3	112.5KVA Dry Type Transformer for LDP3 (Chesapeake Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel		
38						
39		CBE-4	150KVA Dry Type Transformer for LDP4 (Chesapeake Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel		
40						
41	10601 Tunnel Lighting Fixture	Housing	Lighting Fixture Housing	Inspection of all fixture housings in chesapeake tunnel/ replacing as required.	08/5-6/24	
42	10601 Tunnel Lighting Fixture	Connections	Connection to the Tunnel Lining	Inspection of all lighting connections in chesapeake tunnel/ replacing as required.	08/5-6/24	

Fire and Life Safety - Chesaneake Tunnel

Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
1	10700 Fire Protection System	Hose Valve	470+05		
2			471+54		
3			473+00		
4			474+50		
5			475+93		
6			477+39		
7			478+85		
8			480+31		
9	10700 Fire Protection System	Hose Valve	481+77		
10			483+24		
11			484+70		
12			486+16		
13			487+62		
14			489+08		
15			490+55		
16			492+01		
17			493+47		

Fire and Life Safety - Chesaneake Tunnel Item Name, Description or **Preventative Maintenance and/or Repair Date Inspected** Line Component **SNTI Element** MM/DD/YY **Performed** Item Location 494+94 18 19 496+40 10700 Fire 497+86 20 Hose Valve **Protection System**

Fire	ira and Lifa Safaty - Chasanaaka Tunnal							
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
35			519+53					
36			521+00					
37			522+41					
38								
39								
40	10700 Fire Protection System	Tunnel Niche Fire Extiguisher	See Operations Inspection Sheets					
79	10800 Tunnel Operations and Security System	Door Access Security	VB 3 Exterior Doors					
80			3 Island Portal - Sidewalk Side					
81			3 Island Portal - Side Opp. Sidewalk					
82			VB 4 Exterior Doors					
83			4 Island Portal - Sidewalk Side					
84			4 Island Portal - Side Opp. Sidewalk					
85		SCADA - ??????????????????????????Telephones	Emergency Window Information					

Ventilation Systems - Chesapeake Tunnel

		•	chesapeake ram		
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date of Maintenance MM/DD/YYYY
1	10200 Ventilation System	Damper TSB1, TSB2 and TSB3,TSE1, TSE 2, TSE3	Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	
			Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all blower chains drive components inspected and greased.	7/16/2024
			Service Damper Door, Chains, Motors,Drive Rods and Bearings, by Mechanics	all exhaust chains drive components inspected and greased.	7/17/2024
		Fan TNB 1, TNB2, TNB3, TNE 1, TNE 2, TNE 3	Fan Motor, Motor Controller, MC E-Stop	MC E-stop exercised yearly	8/12/2024
2	10200 Ventilation System	Damper TNB1, TNB2, TNB3, TNE1, TNE 2, TNE 3	Service Damper Door, Chains, Motors, Drive Rods and Bearings, by Mechanics	Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	
3	10200 Ventilation System	CO Monitor	North Exhaust Duct, South Exhaust Duct	Monitored by operator, issues reported to electrician and repaired promply	
	1	ı			

Ventilation Systems - Chesapeake Tunnel

veni			Chesapeake Tunr		
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date of Maintenance MM/DD/YYYY
4	10201 Fans	Fan TSB 1, TSB2, TSB3, TSE 1, TSE 2, TSE 3	Fan Motor, Motor Controller, MC E-Stop	MC E-stop exercised yearly	8/12/2024
5	10201 Fans	Fan TSB 1, TSB2, TSB3, TSE 1, TSE 2, TSE 3	Fan Shaft, Bearing Pedestals Bearings, Drive Chains, Fan Housing, Fan Wheel	Checked daily by equipment maintainers. inspected weekly by tunnel mechanics, repairs made as required.	

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
SOUTH	VENTILATION B	UILDING #3 - CHE	SAPEAKE TUNNEL		
1	10301 Drainage and Pumping System	1 Island Portal Pump Room	Main Sump Pump TSD-1, TSD 2, TSD 3 Pumps, Motors, Valves (Weekly for operability, greased, adjustments & packing as needed during weekly)	Checked daily by Maintainer for packing leaks. Inspected weekly by tunnel mechanic for operability, greased, and adjustments made to packing as needed.	
				Exercised and greased all valves in pump room / Greased all pumps	1/18/2024

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
54					
55	10301 Drainage and Pumping System	Thimble Tunnel Low Point Pump Station (LPPS)	Drainage Pump TMD1 and TMD 2 - Pumps, Motors, Valaves	Inspected weekly by tunnel mechanic for operability, greased, and adjustments made to packing as needed.	
			TMD-1	Rebuilt pump with new bearings and shaft. Reinstalled in Thimble LPPS.	12/12-14/2023
			TMD-1	Replaced drive motor	12/19/2023
				Exercised and greased all valves in pump room / Greased all pumps	1/22/2024
1					

Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
71	10301 Drainage and Pumping System	2 Island Portal Pump Room	Main Sump Pump TND-1, TND 2, TND 3 Pumps, Motors, Valves (Weekly for operability, greased, adjustments & packing as needed during weekly)	Checked daily by Maintainer for packing leaks. Inspected weekly by tunnel mechanic for operability, greased, and adjustments made to packing as needed.	
				Exercised and greased all valves in pump room / Greased all pumps	1/24/2024

		amps chesapeake ranner				
Line Item	SNTI Element	Locaion	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY	

Emergency Generators - Chesapeake Tunnel

Line Item	SNTI Element	Component	Item Name, Description or Location	Output Voltage Acceptable (Y/N)	Thermal Reading ∘F	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
			SOUTH VENTILA	I ATION BUILDING	I G #1 THIMBLE T	UNNEL	
1	10400 Emergency Generator System	1 Island Emerg. Generator	Generator, Fuel Pump, Supply Air Louvers, Day Fuel Tank, Exhaust Manifold, ATS, UPS	Y	85 before start up / 172 running temp	System start up and run test	9/6/2023
				Υ	83 / 175	System start up and run test	10/12/2023
				Y	107 / 176	System start up and run test	11/6/2023
				Y	112 / 177	System start up and run test	1/2/2024
				Y	109 / 174	System start up and run test	2/15/2024
				Y	106 / 177	System start up and run test	3/12/2024
				Y	104 / 175	System start up and run test	4/9/2024
				Y	113 / 168	System start up and run test	5/1/2024
				Υ	107 / 176	System start up and run test	6/4/2024
				Υ	102 / 177	Oil and filter change / System start up and run test	7/22/2024

Emergency Generators - Chesapeake Tunnel Output Voltage Thermal Line Item Name, Description or **Preventative Maintenance and/or Repair Date Inspected SNTI Element** Component Acceptable Reading Item MM/DD/YY Performed Location (Y/N) **NORTH VENTILATION BUILDING #4 - CHESAPEAKE TUNNEL** 10400 110 before start Generator, Fuel Pump, Supply Air 2 Island Emerg. Emergency Υ 9/6/2023 19 Louvers, Day Fuel Tank, Exhaust up / 174 running System start up and run test Generator Generator Manifold, ATS, UPS temp System Υ 114 / 175 10/12/2023 System start up and run test Υ 109 /174 System start up and run test 11/6/2023 Υ 106 / 174 System start up and run test 1/2/2024 Υ 102 / 175 System start up and run test 2/15/2024 Υ 104 / 177 System start up and run test 3/12/2024 Υ 101 / 178 System start up and run test 4/9/2024 6/4/2024 Υ 112 / 174 System start up and run test Oil and filter change / System 108 / 177 7/22/2024 Υ start up and run test

Emergency Generators - Chesapeake Tunnel

<u>Eme</u>	mergency Generators - Chesapeake Tunnel									
Line Item	SNTI Element	Component	Item Name, Description or Location	Output Voltage Acceptable (Y/N)	Thermal Reading ∘F	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			

Power Distribution - Chesapeake Tunnel

Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
		ı		BUILDING #2 - THIMBLE TUNNEL	
		#2 island Power Distribution System	#2 Island 5th Floor	All components are checked daily by the equipment maintainers for operation, noise and temperature. Any issues are adressed by the Electrician	
1	10500 Electrical Distribution System	MB FDR NB21	Medium Voltage Switchgear	Replaced in service following reconditioning service	12/11/2023
2		FB SB12	Medium Voltage Switchgear		
3		FB XFMR #2	Medium Voltage Switchgear	Failed on 1/25/24	1/25/2024
4	10500 Electrical Distribution System	TIE BREAKER SB12	Medium Voltage Switchgear		
5		MB FDR NB13	Medium Voltage Switchgear		
6		FB NB14	Medium Voltage Switchgear		
7		FB XFMR #1	Medium Voltage Switchgear		
8		TIE BREAKER NB11	Medium Voltage Switchgear		
9	10500 Electrical Distribution System	LVSG Enclosure	600 V Switchboard		
10	10500 Electrical Distribution System	Trans #1	1000KVA Transformer #1		
11		Trans #2	1000KVA Transformer #2		
12	10500 Electrical Distribution System	House Bus PB 1	Distribution Panel		
13		House Bus PB 2	Distribution Panel		
14		Essential Service PB	Distribution Panel		
15		Emergency Systems	Diesel Generator Switchboard		

Power Distribution - Chesapeake Tunnel

		Jacion -	Chesapeake	i diliici	
Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
16		HT1	House Transformer #1 - 150KVA 3PH 60HZ		
17		HT2	House Transformer #2 - 150KVA 3PH 60HZ		
18		Outlets/ Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1		
19	10500 Electrical Distribution System	Outlets/ Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2		
20		Raceways/ Overhead Wiring	General Overhead Raceways and Wiring Chases within Electrical Floor		
21	10500 Electrical Distribution System	Disconnect Switches	Main Power Transformer		
		S	OUTH VENTILATION	BUILDING #1 - THIMBLE TUNNEL	
		#1 island Power Distribution System	#1 Island 5th Floor	All components are checked daily by the equipment maintainers for operation, noise and temperature. Any issues are adressed by the Electrician	
22	10500 Electrical Distribution System	MB FDR NB10	Medium Voltage Switchgear		
23		FB NB21	Medium Voltage Switchgear	Replaced in service following reconditioning service	12/11/2024
24		FB XFMR #2	Medium Voltage Switchgear		
25	10500 Electrical Distribution System	MB M1 FDR NB22	Medium Voltage Switchgear		
26		MB M1 FDR NB12	Medium Voltage Switchgear		
27		FB NB13	Medium Voltage Switchgear		
28		FB XFMR #1	Medium Voltage Switchgear		
29		MB M2 FDR SB12	Medium Voltage Switchgear		
30	10500 Electrical Distribution System	LVSG Enclosure	600 V Switchboard		

Power Distribution - Chesapeake Tunnel

Line Item	SNTI Element	Component/ Label	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
31	10500 Electrical Distribution System	Trans #1	1000KVA Transformer #1		
32		Trans #2	1000KVA Transformer #2		
33	10500 Electrical Distribution System	House Bus PB 1	Distribution Panel		
34		House Bus PB 2	Distribution Panel		
35		Essential Service PB	Distribution Panel		
36		Emergency Systems	Diesel Generator Switchboard		
37		HT1	House Transformer #1 - 150KVA 3PH 60HZ		
38		HT2	House Transformer #2 - 150KVA 3PH 60HZ		
39	10500 Electrical Distribution System	Outlets/ Receptacles	Control Room/Bathroom No. 1/Locker Room No. 1		
40		Outlets/ Receptacles	Kitchen/Bathroom No. 2/Locker Room No. 2		
41	10500 Electrical Distribution System	Damper BSE3	General Overhead Raceways and Wiring Chases within Electrical Floor		
42	10500 Electrical Distribution System	Disconnect Switches	Main Power Transformer		

Tam	Tunnel Lighting - Chesapeake Tunnel							
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
1	10600 Tunnel Lighting System	Lights	LED	Inspection and repair of all anchors and repair of lighting North and Southbound lanes	08/6-7/24			
2								
3	10600 Tunnel Lighting System	Lenses	Light Fixture Cover/Lenses	Inspection and repair North and Southbound lanes	08/6-7/24			
4	10600 Tunnel Lighting System	Conduit	Conduits connecting Fixtures	Inspection and repair North and Southbound lanes	08/6-7/24			
5	10600 Tunnel Lighting System	Wireways	Lighting wireways powering fixtures	Inspection and repair North and Southbound lanes	08/6-7/24			
6	10600 Tunnel Lighting System	PLC	Programmable Logic Controllers for Tunnel Lighting Sensors	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
7								
8	10600 Tunnel Lighting System	Luminance Meter	Lumaniance Meter Located near the top of the open approach directed at the portal opening	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
9	10600 Tunnel Lighting System	LDP3-1	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
10								
11		LDP3-2	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
12								
13		LDP3-3	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
14								
15		LDP3-4	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel				
16								

Tunnei Lighting - Chesapeake Tunnei							
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY		
17	10600 Tunnel Lighting System	LDP4-1	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
18							
19		LDP4-2	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
20							
21		LDP4-3	Lighting Distribution Panel - Substation 3 (Chesapeake Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
22							
23		LDP4-4	Lighting Distribution Panel - Substation 3 (Thimble Tunnel)	Checked daily by equipment maintainers for operation. Issues repaired by appropriate E/M personnel			
24							
25	10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Thimble Shoals Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel			
26							
27		CBW-2	112.5KVA Dry Type Transformer for LDP2 (Chesapeake Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel			
28							
29		CBE-3	112.5KVA Dry Type Transformer for LDP3 (Thimble Shoals Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel			
30							
31		CBE-4	150KVA Dry Type Transformer for LDP4 (Thimble Tunnel) - Substation 3	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel			
32							

Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY			
33	10600 Tunnel Lighting System	CBW-1	112.5KVA Dry Type Transformer for LDP1 (Thimble Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
34								
35		CBW-2	112.5KVA Dry Type Transformer for LDP2 (Thimble Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
36								
37		CBE-3	112.5KVA Dry Type Transformer for LDP3 (Thimble Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
38								
39		CBE-4	150KVA Dry Type Transformer for LDP4 (Thimble Tunnel) - Substation 4	Checked daily by equipment maintainers for noise and temperature. Issues repaired by appropriate E/M personnel				
40	10601 Tunnel Lighting Fixture	Connections	Connection to the Tunnel Lining	Inspection of all lighting connections in chesapeake tunnel/ replacing as required.	08/6-7/24			
41	10601 Tunnel Lighting Fixture	Housing	Lighting Fixture Housing	Inspection of all fixture housings in chesapeake tunnel/replacing as required.	08/6-7/24			

Fire and Life Safety - Chesapeake Tunnel

Line			nesapeake lunne Item Name, Description or	Preventative Maintenance and/or Repair	Date Inspected
Item	SNTI Element	Component	Location	Performed	MM/DD/YY
1	10700 Fire Protection System	Hose Valve	470+05		
2			471+54		
3			473+00		
4			474+50		
5			475+93		
6			477+39		
7			478+85		
8			480+31		
9	10700 Fire Protection System	Hose Valve	481+77		
10			483+24		
11			484+70		
12			486+16		
13			487+62		
14			489+08		
15			490+55		
16			492+01		
17			493+47		

Fire and Life Safety - Chesapeake Tunnel

Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
18			494+94		
19			496+40		
20	10700 Fire Protection System	Hose Valve	497+86		
21			499+32		
22			500+79		
23			502+25		
24			503+72		
25			505+18		
26			506+63		
27			508+10		
28			509+43		
29			510+76		
30			512+23		
31	10700 Fire Protection System	Hose Valve	513+68		
32			515+15		
33			516+61		
34			518+07		

Fire and Life Safety - Chesapeake Tunnel

		arety c	nesapeake runne		Databassatad
Line Item	SNTI Element	Component	Item Name, Description or Location	Preventative Maintenance and/or Repair Performed	Date Inspected MM/DD/YY
35			519+53		
36			521+00		
37			522+41		
38					
39					
40	10700 Fire Protection System	Tunnel Niche Fire Extiguisher	See Operations Inspection Sheets		
79	10800 Tunnel Operations and Security System	Door Access Security	VB 3 Exterior Doors		
80			3 Island Portal - Sidewalk Side		
81			3 Island Portal - Side Opp. Sidewalk		
82			VB 4 Exterior Doors		
83			4 Island Portal - Sidewalk Side		
84			4 Island Portal - Side Opp. Sidewalk		
85		SCADA - ?????????????? ?Telephones	Emergency Window Information		

CBBTD FROM #120 OPERATIONS DIV (Revised 9/2022)

Date: July 30, 2024

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS

NORTH PLAZA CONTROL BUILDING

	NOF	TITI PLAZ	A CONTROL B	DILDING		
LOCATION	TYPE	TATION NO	SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE CHANGED
LOBBY			٧		6/11/2024	
LOBBY			٧		6/11/2024	
KITCHEN			٧		6/11/2024	
BOILER ROOM			V		6/11/2024	
CONTOL ROOM			٧		6/11/2024	
SOUTH	PLAZA C	ONTROL	& SOUTH MAI	NTENANCE BUIL	DING	
LOBBY			٧		6/10/2024	
CONTROL ROOM			٧		6/10/2024	
KITCHEN			٧		6/10/2024	
BOILER ROOM				٧	6/10/2024	6/11/2024
MAINTENANCE GARAGE			٧		6/10/2024	
MAINTENANCE GARAGE			٧		6/10/2024	
MAINTENANCE GARAGE			٧		6/10/2024	
		ADMINIS	TRATION BUI	LDING		
Lobby/ Receptionist Closet			٧		6/11/2024	
Exective Wing Hallway			٧		6/11/2024	
Main Hallway-next to Marketing Office			٧		6/11/2024	
Finance Wing - next to Directors Office			٧		6/11/2024	
Finance Wing - Cathy Bell's Office			٧		6/11/2024	
Finance Wing - end of hall way			٧		6/11/2024	
Boiler Room			٧		6/11/2024	
Maintenance Wing- next to entry door			٧		6/11/2024	

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

Maurice Giddens/

TO: CHIEF OF POLICE

Date: July 30, 2024

RE: INSPECTION OF FIRE EXTINGUISHERS/ MAINTENANCE AREA NORTH PLAZA

LOCATION	TYPE	STATION NO.	SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE CHANGED
MAIN OFFICE			٧		6/10/2024	
BREEZE WAY			٧		6/10/2024	
AUTO SHOP			٧		6/10/2024	
AUTO SHOP			V		6/10/2024	
AUTO SHOP			V		6/10/2024	
GAS PUMPS			V		6/10/2024	
DIESEL PUMPS			٧		6/10/2024	
MIS OFFICE			٧		6/10/2024	
MIS OFFICE			٧		6/10/2024	,
MIS WORK BAY			٧		6/10/2024	1
MIS WORK BAY			٧		6/10/2024	
ELECTRICAL BUILDING			٧		6/10/2024	
FAR END STORAGE BAY	,		٧		6/10/2024	

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

Operations Employee Name

Maurice Giddens

CBBTD FROM #120

OPERATIONS DIV (Revised 9/2022)

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS

LOCATION	TYPE	STATION NO.	MBER 1 ISLAN SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE CHANGED
	ITPE	STATION NO.		UNSATISFACTORY		DATE CHANGED
4TH FLOOR GARAGE			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
3RD FLOOR			V		6/12/2024	
3RD FLOOR			V	,	6/12/2024	
3RD FLOOR			٧		6/12/2024	
2ND FLOOR			V		6/12/2024	
2ND FLOOR			٧		6/12/2024	
1ST FLOOR			٧		6/12/2024	
		NU	MBER 2 ISLAN	D		
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			٧		6/12/2024	
5TH FLOOR			V		6/12/2024	
3RD FLOOR			٧		6/12/2024	
3RD FLOOR			٧		6/12/2024	
2ND FLOOR			٧		6/12/2024	
2ND FLOOR			٧		6/12/2024	
2ND FLOOR			٧		6/12/2024	
1ST FLOOR			٧		6/12/2024	
						,

Date: JULY 30, 2024

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

Operations Employee Name

Maurice Giddens/

Date: JULY 30, 2024

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS

LOCATION	TYPE	STATION NO.	MBER 3 ISLAN SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE CHANGED
3 Island Garage			٧		6/11/2024	
Fifth Floor			V		6/11/2024	
Fifth Floor			V		6/11/2024	
Fifth Floor			V		6/11/2024	
Fifth Floor			V		6/11/2024	
Third Floor			V		6/11/2024	
Third Floor			٧		6/11/2024	
Second Floor			V		6/11/2024	
Second Floor			V		6/11/2024	
Second Floor			٧		6/11/2024	
First Floor			٧		6/11/2024	
		NUI	MBER 4 ISLAN	D		
4 Island Garage			٧		6/12/2024	
Fifth Floor			٧		6/12/2024	
Fifth Floor			٧		6/12/2024	
Fifth Floor			٧		6/12/2024	
Fifth Floor			٧		6/12/2024	
Third Floor			V		6/12/2024	
Third Floor			٧		6/12/2024	
Third Floor			٧		6/12/2024	
Second Floor			٧		6/12/2024	
Second Floor			٧		6/12/2024	
Second Floor			٧		6/12/2024	
First Floor			٧		6/12/2024	

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

Operations Employee Name

Maurice Giddens

CBBTD FROM #120 OPERATIONS DIV (Revised 9/2022)

Date: July 30, 2024

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS/THIMBLE TUNNEL SHEET

LOCATION	Northbound/Southbound	TYPE	STATION NO.		UNSATISFACTORY	DATE INSPECTED	DATE CHANGED
Thimble Tunnel	Northbound		189+92	V		6/27/2024	
Thimble Tunnel	Northbound		191+43	٧		6/27/2024	
Thimble Tunnel	Northbound		192+89	٧		6/27/2024	
Thimble Tunnel	Northbound		194+37	٧		6/27/2024	
Thimble Tunnel	Northbound		195+83	٧		6/27/2024	
Thimble Tunnel	Northbound		197+30	٧		6/27/2024	
Thimble Tunnel	Northbound		198+76	√		6/27/2024	
Thimble Tunnel	Northbound		200+24	٧		6/27/2024	
Thimble Tunnel	Northbound		201+70	√		6/27/2024	
Thimble Tunnel	Northbound		203+17	٧		6/27/2024	
Thimble Tunnel	Northbound		204+64	٧		6/27/2024	
Thimble Tunnel	Northbound		206+11	٧		6/27/2024	
Thimble Tunnel	Northbound	***************************************	207+58	٧		6/27/2024	
Thimble Tunnel	Northbound		209+05	٧		6/27/2024	
Thimble Tunnel	Northbound		210+52	٧		6/27/2024	
Thimble Tunnel	Northbound		211+96	٧		6/27/2024	
Thimble Tunnel	Northbound		213+40	٧		6/27/2024	
Thimble Tunnel	Northbound		214+84	٧		6/27/2024	· · · · · · · · · · · · · · · · · · ·
Thimble Tunnel	Northbound		216+28	٧		6/27/2024	
Thimble Tunnel	Northbound		217+76	٧		6/27/2024	
Thimble Tunnel	Northbound		219+16	٧		6/27/2024	
Thimble Tunnel	Northbound		220+60	٧		6/27/2024	
Thimble Tunnel	Northbound		222+04	٧		6/27/2024	
Thimble Tunnel	Northbound		223+48	V		6/27/2024	
Thimble Tunnel	Northbound		224+92	٧		6/27/2024	
Thimble Tunnel	Northbound		226+39	٧		6/27/2024	
Thimble Tunnel	Northbound		227+85	٧		6/27/2024	
Thimble Tunnel	Northbound		229+32	٧		6/27/2024	
Thimble Tunnel	Northbound		230+79	٧		6/27/2024	
Thimble Tunnel	Northbound		232+26	٧		6/27/2024	
Thimble Tunnel	Northbound		233+73	٧		6/27/2024	
Thimble Tunnel	Northbound		235+20	٧		6/27/2024	
Thimble Tunnel	Northbound		236+66	V		6/27/2024	
Thimble Tunnel	Northbound		238+14	٧		6/27/2024	
Thimble Tunnel	Northbound		239+60	٧		6/27/2024	
Thimble Tunnel	Northbound		241+07	٧		6/27/2024	
Thimble Tunnel	Northbound	 	242+54	٧		6/27/2024	,
Thimble Tunnel	Northbound		244+00	V	†	6/27/2024	
Thimble Tunnel	Northbound		245+58	٧		6/27/2024	
Timible fulliel	ITOTATIOUTIU		273130	V		0/2//2024	
· · · · · · · · · · · · · · · · · · ·			1				
	<u> </u>						

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

TO: CHIEF OF POLICE

Date: July 30, 2024

RE: INSPECTION OF FIRE EXTINGUISHERS/THIMBLE TUNNEL SHEET

LOCATION	Northbound/Southbound	TYPE STATION NO.	SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE REPLACED
Thimble Tunnel	Southbound	189+92	٧		6/11/2024	
Thimble Tunnel	Southbound	191+43	√		6/11/2024	
Thimble Tunnel	Southbound	192+89	٧		6/11/2024	
Thimble Tunnel	Southbound	194+37	٧		6/11/2024	
Thimble Tunnel	Southbound	195+83	٧		6/11/2024	
Thimble Tunnel	Southbound	197+30	٧		6/11/2024	
Thimble Tunnel	Southbound	198+76	٧		6/11/2024	
Thimble Tunnel	Southbound	200+24		٧	6/11/2024	7/25/2024
Thimble Tunnel	Southbound	201+70	٧		6/11/2024	
Thimble Tunnel	Southbound	203+17	٧		6/11/2024	
Thimble Tunnel	Southbound	204+64	٧		6/11/2024	
Thimble Tunnel	Southbound	206+11	٧		6/11/2024	
Thimble Tunnel	Southbound	207+58	٧		6/11/2024	
Thimble Tunnel	Southbound	209+05	٧		6/11/2024	
Thimble Tunnel	Southbound	210+52	٧		6/11/2024	
Thimble Tunnel	Southbound	211+96	٧		6/11/2024	
Thimble Tunnel	Southbound	213+40	٧		6/11/2024	
Thimble Tunnel	Southbound	214+84	٧		6/11/2024	
Thimble Tunnel	Southbound	216+28			6/11/2024	
Thimble Tunnel	Southbound	217+76	٧		6/11/2024	
Thimble Tunnel	Southbound	219+16	٧		6/11/2024	
Thimble Tunnel	Southbound	220+60	٧		6/11/2024	
Thimble Tunnel	Southbound	222+04	٧		6/11/2024	
Thimble Tunnel	Southbound	223+48	٧		6/11/2024	
Thimble Tunnel	Southbound	224+92	٧		6/11/2024	
Thimble Tunnel	Southbound	226+39	٧		6/11/2024	
Thimble Tunnel	Southbound	227+85	٧		6/11/2024	
Thimble Tunnel	Southbound	229+32	٧		6/11/2024	
Thimble Tunnel	Southbound	230+79	٧		6/11/2024	
Thimble Tunnel	Southbound	232+26	٧		6/11/2024	
Thimble Tunnel	Southbound	233+73	٧		6/11/2024	
Thimble Tunnel	Southbound	235+20		٧	6/11/2024	7/25/2024
Thimble Tunnel	Southbound	236+66	٧		6/11/2024	
Thimble Tunnel	Southbound	238+14	٧		6/11/2024	
Thimble Tunnel	Southbound	239+60	٧		6/11/2024	
Thimble Tunnel	Southbound	241+07	٧		6/11/2024	
Thimble Tunnel	Southbound	242+54	٧		6/11/2024	
Thimble Tunnel	Southbound	244+00	٧		6/11/2024	
Thimble Tunnel	Southbound	245+58	٧		6/11/2024	
L	<u> </u>	L		1		<u> </u>

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS/CHESAPEAKE TUNNEL SHEET

LOCATION	Northbound/Southbound	TYPE	STATION NO.	SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE REPLACED
Chesapeake Tunnel	Northbound		469+99		٧	6/25/2024	7/30/2024
Chesapeake Tunnel	Northbound		471+48	٧		6/25/2024	
Chesapeake Tunnel	Northbound		472+95		٧	6/25/2024	7/30/2024
Chesapeake Tunnel	Northbound		474+44	٧	-	6/25/2024	
Chesapeake Tunnel	Northbound		475+87		٧	6/25/2024	7/30/2024
Chesapeake Tunnel	Northbound		477+33	٧		6/25/2024	
Chesapeake Tunnel	Northbound		478+79	٧		6/25/2024	
Chesapeake Tunnel	Northbound		480+26	٧		6/25/2024	
Chesapeake Tunnel	Northbound		481+72	٧		6/25/2024	
Chesapeake Tunnel	Northbound		483+18	٧		6/25/2024	
Chesapeake Tunnel	Northbound		484+64	٧		6/25/2024	
Chesapeake Tunnel	Northbound		486+10	٧		6/25/2024	
Chesapeake Tunnel	Northbound		487+56	٧		6/25/2024	
Chesapeake Tunnel	Northbound		489+03	٧		6/25/2024	
Chesapeake Tunnel	Northbound		490+49	٧		6/25/2024	
Chesapeake Tunnel	Northbound		491+95	٧		6/25/2024	
Chesapeake Tunnel	Northbound		493+42	٧		6/25/2024	
Chesapeake Tunnel	Northbound		494+88	٧		6/25/2024	
Chesapeake Tunnel	Northbound		496+34	٧		6/25/2024	
Chesapeake Tunnel	Northbound		497+81	٧		6/25/2024	
Chesapeake Tunnel	Northbound		499+27	٧		6/25/2024	
Chesapeake Tunnel	Northbound		500+73	٧		6/25/2024	
Chesapeake Tunnel	Northbound		502+19	٧		6/25/2024	
Chesapeake Tunnel	Northbound		503+66	٧	·	6/25/2024	
Chesapeake Tunnel	Northbound		505+12	٧		6/25/2024	
Chesapeake Tunnel	Northbound		506+57	٧		6/25/2024	
Chesapeake Tunnel	Northbound		508+04	٧		6/25/2024	
Chesapeake Tunnel	Northbound		509+37	٧		6/25/2024	
Chesapeake Tunnel	Northbound		510+70	٧		6/25/2024	
Chesapeake Tunnel	Northbound		512+17	V		6/25/2024	
Chesapeake Tunnel	Northbound		513+63	٧		6/25/2024	A
Chesapeake Tunnel	Northbound		515+09	٧		6/25/2024	
Chesapeake Tunnel	Northbound		516+55	٧		6/25/2024	
Chesapeake Tunnel	Northbound		518+01	٧		6/25/2024	
Chesapeake Tunnel	Northbound		519+47	٧		6/25/2024	
Chesapeake Tunnel	Northbound		520+94	V		6/25/2024	
Chesapeake Tunnel	Northbound		522+50	V		6/25/2024	

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.

CBBTD FROM #120 OPERATIONS DIV (Revised 9/2022)

TO: CHIEF OF POLICE

RE: INSPECTION OF FIRE EXTINGUISHERS/CHESAPEAKE TUNNEL SHEET

LOCATION	Northbound/Southbound	TYPE	STATION NO.	SATISFACTORY	UNSATISFACTORY	DATE INSPECTED	DATE REPLACED
Chesapeake Tunnel	Southbound		469+99	٧		6/11/2024	
Chesapeake Tunnel	Southbound		471+48	V		6/11/2024	
Chesapeake Tunnel	Southbound		472+95	٧		6/11/2024	
Chesapeake Tunnel	Southbound		474+44	٧		6/11/2024	
Chesapeake Tunnel	Southbound		475+87	V		6/11/2024	
Chesapeake Tunnel	Southbound		477+33	٧		6/11/2024	
Chesapeake Tunnel	Southbound		478+79	٧		6/11/2024	
Chesapeake Tunnel	Southbound		480+26	V		6/11/2024	
Chesapeake Tunnel	Southbound		481+72	٧		6/11/2024	
Chesapeake Tunnel	Southbound		483+18	√		6/11/2024	
Chesapeake Tunnel	Southbound		484+64	٧		6/11/2024	
Chesapeake Tunnel	Southbound		486+10	٧		6/11/2024	
Chesapeake Tunnel	Southbound		487+56	٧		6/11/2024	
Chesapeake Tunnel	Southbound		489+03	٧		6/11/2024	
Chesapeake Tunnel	Southbound		490+49	٧		6/11/2024	
Chesapeake Tunnel	Southbound		491+95		٧	6/11/2024	7/30/2024
Chesapeake Tunnel	Southbound		493+42	٧		6/11/2024	
Chesapeake Tunnel	Southbound		494+88	٧		6/11/2024	
Chesapeake Tunnel	Southbound		496+34	٧		6/11/2024	
Chesapeake Tunnel	Southbound		497+81	٧		6/11/2024	
Chesapeake Tunnel	Southbound		499+27	٧		6/11/2024	
Chesapeake Tunnel	Southbound		500+73		٧	6/11/2024	7/30/2024
Chesapeake Tunnel	Southbound		502+19	٧		6/11/2024	
Chesapeake Tunnel	Southbound		503+66	٧		6/11/2024	
Chesapeake Tunnel	Southbound		505+12	٧		6/11/2024	
Chesapeake Tunnel	Southbound		506+57	٧		6/11/2024	
Chesapeake Tunnel	Southbound		508+04	٧		6/11/2024	
Chesapeake Tunnel	Southbound		509+37	٧		6/11/2024	
Chesapeake Tunnel	Southbound		510+70	٧		6/11/2024	
Chesapeake Tunnel	Southbound		512+17	٧		6/11/2024	
Chesapeake Tunnel	Southbound		513+63	٧		6/11/2024	
Chesapeake Tunnel	Southbound		515+09	٧		6/11/2024	
Chesapeake Tunnel	Southbound		516+55	٧		6/11/2024	
Chesapeake Tunnel	Southbound		518+01	٧		6/11/2024	
Chesapeake Tunnel	Southbound		519+47	٧		6/11/2024	
Chesapeake Tunnel	Southbound		520+94	٧		6/11/2024	
Chesapeake Tunnel	Southbound		522+50	٧		6/11/2024	

Date: JULY 30, 2024

This report is to be submitted quarterly (90) days. Any unsatisfactory conditions are explained on reverse side.





14. Special Testing Documents

Colorado | Florida | Illinois | Louisiana | Michigan | Missouri | New Jersey | New York | North Carolina | Pennsylvania | Texas | Washington, DC | West Virginia

Test Report

DATE: 12/27/2024

TO: Chesapeake Bay Bridge and Tunnel

FROM: Alexander Waardenburg, P.E. (M&M)

RE: Ventilation Fan Bearing Vibration and Temperature Test Results

		•									
Ver	Ventilation Fan Bearing Vibration and Temperature Test Results										
Location		Mo	otor			Fan Shaft	Bearings				
	Front Mo	otor Side (DE)	Rear Mot	or Side (NDE)	Fixed	Bearing	Floatii	ng Bearing			
Fan ID	Vibration	Temperature	Vibration	Temperature	Vibration	Temperature	Vibration	Temperature			
BSB1	NM	NM	NM	NM	NM	NM	NM	NM			
BSB2	NM	NM	NM	NM	NM	NM	NM	NM			
BSB3	Normal	Normal	Normal	Normal	Normal	Normal	Normal	Normal			
BSE1	Normal	Normal	Normal	Normal	Normal	Normal	Normal	Normal			
BSE2	Normal	Normal	Normal	Normal	Normal	Normal	Normal	Normal			
BSE3	NM	NM	NM	NM	NM	NM	NM	NM			
BNB1	NM	NM	NM	NM	NM	NM	NM	NM			
BNB2	NM	NM	NM	NM	NM	NM	NM	NM			
BNB3	Normal	Normal	Normal	Normal	Normal	Normal	Normal	Normal			
BNE1	NM	NM	NM	NM	NM	NM	NM	NM			
	İ	1	İ	1	1	İ	İ	i			

Normal

Normal

Normal

Normal

Chesapeake Channel Tunnel

BNE3 Key:

BNE2

Normal - Continue to monitor.

Normal

Normal

Alarming - Determine the cause of vibration and make correction.

Normal

Normal

Hammering - Machinery is undergoing damage. Continued operation will produce early failure.

Normal

Normal

Notes:

DE - Driving End NDE - Nondriving End NM - Not Measured

Bearing Vibration Measurements were performed at Operating Speeds 1, 2, 3, and 4 $\,$



Normal

Normal

Normal

Normal

Normal

Normal

Colorado | Florida | Illinois | Louisiana | Michigan | Missouri | New Jersey | New York | North Carolina | Pennsylvania | Texas | Washington, DC | West Virginia

Test Report

DATE: 12/27/2024

TO: Chesapeake Bay Bridge and Tunnel

FROM: Kyle Gable, P.E. (M&M)

RE: Tunnel Lighting Measurement Results

Chesapeake Channel Tunnel										
	Lighting Measurement Results Center of Center of									
Location	Southbound Lane	Centerline	Northbound Lane							
Station 469+00	35.24	32.51	31.46							
Station 478+45	35.54	33.69	32.9							
Station 484+22	32.59	34.04	35.14							
Station 495+60	31.62	37.67	38.53							
Station 501+90	38.74	35.25	36.4							
Station 513+31	32.15	33.56	33.57							
Station 522+54	33.24	31.03	27.97							
<u> </u>										

Notes:

Measurements are in foot-candles

Measurements acquired using an LM-50 illumination testing device



NATIONAL ELEVATOR INSPECTION SERVICE A Bureauveritas Company



VERITAS
Elevator No.
In the building located at Tonne / Tolard #/ Has been granted a
The second granical a
CERTIFICATE OF OPERATION
Maximum Capacity <u> </u>
Maximum Speed 100 Ft. per Min. 3-31-2035
Inspector's Signature Lic. No. Tecologo
This permit shall be conspicuously posted on, near
or visible from each entry to the elevator.
NATIONAL ELEVATOR INSPECTION SERVICE A Bureauveritas Company
A Bureauveritas Company BUREAU VERITAS
A Bureauveritas Company BUREAU VERITAS Elevator No.
A Bureauveritas Company BUREAU VERITAS
A Bureauveritas Company BUREAU VERITAS Elevator No. 2 In the building located at /www Island + 2 Has been granted a
A Bureauveritas Company BUREAU VERITAS Elevator No
A Bureauveritas Company BUREAU VERITAS Elevator No. 2 In the building located at /www Island + 2 Has been granted a
Elevator No
Elevator No. 2 In the building located at 15 and + 2 Has been granted a CERTIFICATE OF OPERATION Maximum Capacity 2000 Pounds Expiration Date

This permit shall be conspicuously posted on, near or visible from each entry to the elevator.

NEIS-C/O (4/13)

NATIONAL ELEVATOR INSPECTION SERVICE A Bureauveritas Company



Elevator No. 3
In the building located at
CERTIFICATE OF OPERATION
Maximum Capacity 2000 Pounds Expiration Date Maximum Speed 100 Ft. per Min. Inspector's Signature factor Lic. No. F001040
This permit shall be conspicuously posted on, near or visible from each entry to the elevator.
NATIONAL ELEVATOR INSPECTION SERVICE A Bureauveritas Company Elevator No. 4 In the building located at 40 Has been granted a CERTIFICATE OF OPERATION
Maximum Capacity 2000 Pounds Maximum Speed 100 Ft. per Min. Inspector's Signature Lic. No. Fool 640

This permit shall be conspicuously posted on, near or visible from each entry to the elevator.

NEIS-C/O (4/13)





15. Electronic Submittals

The following supplemental items will be submitted electronically to the District with the Final Report:

- Inspection Database: Includes majority of field notes from the annual inspection and the routine inspections performed on the trestles
- Underwater Inspection Forms (Level I & II) of Trestle CSB